



**PLANNING COMMISSION
MEETING AGENDA
REGULAR MEETING**

500 W. Big Beaver
Troy, MI 48084
(248) 524-3364
www.troymi.gov
planning@troymi.gov

Donald Edmunds, Chair, Philip Sanzica, Vice Chair
Ollie Apahidean, Karen Crusse, Michael W. Hutson, Tom Krent
Padma Kuppa and John J. Tagle

August 11, 2015

7:00 P.M.

Council Chambers

1. ROLL CALL
2. APPROVAL OF AGENDA
3. MINUTES – July 28, 2015
4. PUBLIC COMMENT – For Items Not on the Agenda
5. TUCKER STREET BARRICADE

PRELIMINARY SITE PLAN REVIEW

6. PRELIMINARY SITE PLAN REVIEW (File Number SP 1009) – Proposed Sedona Taphouse Restaurant, South side of Big Beaver, East of Livernois (198 E Big Beaver), Section 27, Currently Zoned BB (Big Beaver) District

OTHER BUSINESS

7. PUBLIC COMMENT – Items on Current Agenda
8. PLANNING COMMISSION COMMENT

ADJOURN

NOTICE: People with disabilities needing accommodations for effective participation in this meeting should contact the City Clerk by e-mail at clerk@troymi.gov or by calling (248) 524-3317 at least two working days in advance of the meeting. An attempt will be made to make reasonable accommodations.

Chair Edmunds called the Regular meeting of the Troy City Planning Commission to order at 7:00 p.m. on July 28, 2015 in the Council Board Room of the Troy City Hall.

1. ROLL CALL

Present:

Ollie Apahidean
Karen Crusse
Donald Edmunds
Michael W. Hutson
Tom Krent
Padma Kuppa
John J. Tagle

Absent:

Philip Sanzica

Also Present:

R. Brent Savidant, Planning Director
Ben Carlisle, Carlisle/Wortman Associates, Inc.
Allan Motzny, Assistant City Attorney
Kathy L. Czarnecki, Recording Secretary

2. APPROVAL OF AGENDA

Resolution # PC-2015-07-046

Moved by: Tagle
Seconded by: Krent

RESOLVED, To approve the Agenda as prepared.

Yes: All present (7)
Absent: Sanzica

MOTION CARRIED

3. APPROVAL OF MINUTES

Resolution # PC-2015-07-047

Moved by: Apahidean
Seconded by: Kuppa

RESOLVED, To approve the minutes of the July 15, 2015 Regular meeting as submitted.

Yes: All present (7)
Absent: Sanzica

MOTION CARRIED

4. PUBLIC COMMENT – Items not on the Agenda

There was no one present who wished to speak.

5. ZONING BOARD OF APPEALS (ZBA) REPORT

There was no Zoning Board of Appeals meeting in July.

6. DOWNTOWN DEVELOPMENT AUTHORITY (DDA) REPORT

Mr. Savidant reported there was no Downtown Development Authority meeting in July.

7. PLANNING AND ZONING REPORT

Mr. Savidant announced the final Master Plan forum, *Boomers & Shakers*, is scheduled on August 17, 2015 at 7:00 p.m.

PRELIMINARY SITE PLAN REVIEWS

8. PRELIMINARY SITE PLAN REVIEW (File Number SP 1008) – Proposed Virginia Tile Showroom, East side of Crooks, North of Maple (1820 Crooks), Section 28, Currently Zoned MR (Maple Road) District

Mr. Carlisle reviewed the Preliminary Site Plan application for Virginia Tile Showroom. He recommended approval with the conditions as identified in his report dated July 21, 2015.

Victor Saroki, project architect, and William Stephenson of Virginia Tile Company were present.

Mr. Saroki circulated a materials board. He addressed:

- State-of-art flagship store.
- Parking.
- Cross access easements.
- Overhead power lines.

Mr. Stephenson said the proposed showroom would replace the existing design center showroom and would function as a wholesale distributor offering select retail services.

Chair Edmunds opened the floor for public comment. There was no one present; the floor was closed for public comment.

Resolution # PC-2015-07-048

Moved by: Hutson

Seconded by: Krent

RESOLVED, That Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Virginia Tile Showroom, located on the east side of Crooks and north of Maple (1820 Crooks), Section 28, within the MR (Maple Road) District, be granted, subject to the following:

1. Demonstrate shared parking compatibility by providing additional information regarding hours of operation and number of employees and provide parking reciprocal easement.
2. Provide additional information regarding anticipated delivery truck size.
3. Provide all cross-access agreements prior to Final approval.
4. Widen Crooks sidewalk to eight (8) feet.
5. Reduce lighting level along the northern property line.

Yes: All present (7)

Absent: Sanzica

MOTION CARRIED

9. PRELIMINARY SITE PLAN REVIEW (File Number SP 1009) – Proposed Sedona Taphouse Restaurant, South side of Big Beaver, East of Livernois (198 E Big Beaver), Section 27, Currently Zoned BB (Big Beaver) District

Mr. Carlisle reviewed the Preliminary Site Plan application. He addressed the site access, pedestrian and vehicular circulation, lighting plan, trash enclosure screening and landscaping. Mr. Carlisle recommended approval with the conditions as identified in his report dated July 21, 2015.

Mr. Savidant announced written comments from Ganesh Reddy of Mamta Holdings were distributed to Board members prior to the beginning of tonight's meeting.

Present were Martin Knollenberg, project architect Thomas Strat, Thomas Desmond of Thomas Strat Architects and landscape architect Brian Devlin.

Mr. Strat gave a presentation detailing the architecture of the proposed Sedona Taphouse Restaurant, featuring building materials, building transparency and rooftop outdoor seating.

There was discussion on:

- Vehicular and pedestrian circulation; narrowness of layout, drive aisle.
- Maneuverability of delivery trucks.
- Landscape requirements; hardscaping.
- Valet service.
- Hours of operation.

Chair Edmunds opened the floor for public comment.

Ganesh Reddy of Mamta Holdings, 3270 W. Big Beaver, addressed concerns with the proposed development as relates to the abutting property to the west, 100 E. Big Beaver.

Chair Edmunds closed the floor for public comment.

Discussion followed on:

- Existing cross access easement to the south; reciprocal agreement, use of property no effect on agreement.
- Required parking spaces; based on projected number of seats only, outdoor seating requires no additional parking.
- Deceleration lane; would be determined by County.
- Stormwater management; utilize regional detention or retain underground.

Resolution # PC-2015-07-049

Moved by: Tagle
 Seconded by: Crusse

RESOLVED, That Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Sedona Taphouse Restaurant, located on the south side of Big Beaver and east of Livernois (198 E. Big Beaver), Section 27, within the BB (Big Beaver) District, be postponed until the applicant can respond to comments made this evening, specifically addressing conditions outlined in the proposed Resolution.

Yes: All present (7)
 Absent: Sanzica

MOTION CARRIED

CONDITIONAL REZONING REQUEST

10. **PUBLIC HEARING - CONDITIONAL REZONING APPLICATION (File Number CR 013)**
 – Proposed Amber Studios and Lofts, East side of Livernois between Vermont and Birchwood, Section 21, From O (Office) District to MR (Maple Road) District

Mr. Carlisle reviewed the proposed Conditional Rezoning application. He addressed the conditions offered by the applicant, parking, landscaping, applicant’s intent to vacate the 18 foot alley and the height of light poles.

Mr. Carlisle recommended approval of the proposed Conditional Rezoning application for the reasons identified in his report dated March 13, 2015 and subject to the conditions as identified in his report dated March 13, 2015.

Present were Dennis Cowan of Plunkett Cooney, Jerome Amber of Amber Properties Company and Justin Wieber of Stantec.

Mr. Cowan addressed the re-design of the building since last presented to the Board. He agreed conditions identified in the Planning Consultant report would be adhered to with one exception. The applicant does not want to eliminate the two parking spaces, as recommended.

Mr. Cowan addressed their intent and timeline to vacate the 18-foot alley. He said a meeting to discuss the proposed development was held with property owners who were given notice by the City of the public hearing, of which nine attended.

Mr. Wieber gave a detailed overview of the building architectural features.

Mr. Amber addressed snow removal and screening of the terraces. He circulated photographs and building material samples.

Mr. Savidant reported the department received numerous written responses to the public hearing notices which were distributed to the Planning Commission prior to the beginning of tonight's meeting.

PUBLIC HEARING OPENED

Denise Whitmer, 76 Vermont, spoke in opposition. She addressed concerns with traffic, cut-through traffic, noise, screening and privacy.

Ted LaVanaway, 6952 Dublin Fair, spoke in support.

PUBLIC HEARING CLOSED

Discussion followed on:

- Defined height of light pole.
- Trash enclosure materials.
- Zoning requirements for trash enclosures.

Resolution # PC-2015-07-050

Moved by: Tagle

Seconded by: Krent

RESOLVED, That the Planning Commission hereby recommends to the City Council that the O to MR Conditional Rezoning request, which incorporates Preliminary Site Plan Approval, as per Section 16.04 of the City of Troy Zoning Ordinance, located on the east side of Livernois, between Vermont and Birchwood, within Section 21, being approximately 0.5 acres in size, be granted, for the following reasons:

1. The request complies with the Master Plan.
2. The Form-Based District would permit greater flexibility in use and development of the property.
3. The abundance of Office District property in the City has been well documented.
4. The rezoning would be compatible with surrounding zoning and land use.
5. The site can be adequately served with municipal water and sewer.

BE IT FURTHER RESOLVED, That the Planning Commission recommends the following site plan design considerations:

1. Obtain alley vacation prior to Final approval.
2. Provide one (1) additional barrier free parking space.
3. Remove the single space directly adjacent to the north side of the building.
4. Remove one (1) parking space along the western property line and add five (5) additional feet to each greenbelt along Vermont and Birchwood.
5. Address berming and landscaping within north and south greenbelt.
6. Purchase additional 9-foot of alley or if additional 9-foot of alley cannot be purchased, address screening and landscape deficiencies within eastern greenbelt.
7. Trash enclosure screening will be with poured concrete with gates similar in construction and appearance as the screening on the upper terrace level, metal slats and wood, and to comply with enclosure requirements.
8. Light pole height shall be maximum 15 feet high to mitigate impact upon adjacent single-family properties.

Yes: All present (7)
 Absent: Sanzica

MOTION CARRIED

Chair Edmunds opened the floor for one additional public comment.

Randy Whitmer, 76 Vermont, spoke in opposition. He said if a vote was taken, the City would find the residents along Vermont and Birchwood are opposed to the proposed development.

Mr. Carlisle informed the audience that the recommending body on the proposed development is City Council and those residents within the required 300 foot radius would be notified of the scheduled date and time of the City Council public hearing.

OTHER BUSINESS

11. PLANNED UNIT DEVELOPMENT APPLICATION (PUD 014) – Proposed Stonecrest Planned Unit Development, East side of Livernois between Big Beaver and Wattles, Section 22, Currently Zoned R-1E (One Family Residential) District

Mr. Hutson asked to be recused from discussion on this item because of his business relationship with Thomas Sawyer, the attorney representing the applicant.

The Board accepted Mr. Hutson's request to be recused. Mr. Hutson exited the meeting.

Mr. Carlisle reviewed the proposed Planned Unit Development application. He addressed the applicant's intent to partner with the City in an effort to increase the development potential of the site and assist the City with developing the City's dog park and trailhead.

There was discussion on:

- Concept of the development; partnership.
- Building design, layout.
 - Flip building, parking, detention 180 degrees
 - Single story near road; drop off at rear entrance
 - Reduce scale of building and hard pavement along road
 - Existing green visually appealing on right-of-way
 - Exclusivity of building; create privacy in front
- Dog park stand-alone vs partnership project.
- Conceptual dog park plans, overlay.
- Stormwater management; detention as feature.
- Property in floodplain; undevelopable, unbuildable.
- Shared parking.
- Access point.

Mark Pomerence, Vice President of Development of North Point Development, introduced Thomas Sawyer of Hutson, Sawyer, Rupp & Schroeder law firm and David Hunter of Professional Engineering Associates. Mr. Pomerence addressed:

- Company profile.
- Assisted living services.
- Building design, elevation; not a cookie-cutter development.
- Proposed improvements, amenities.
- \$50,000 assistance for dog park, trailhead, other improvements.
- Detention basin as amenity.
- Approximately \$12 million development.
- Traffic; lowest traffic-generated use.
- Desire to have building visibility from street.

Chair Edmunds opened the floor for public comment. There was no one present; the floor was closed for public comment.

Mr. Hutson returned to the meeting.

12. ZONING ORDINANCE TEXT AMENDMENT (File Number ZOTA 248) – Woodland Protection

Mr. Savidant briefly reviewed the proposed Zoning Ordinance Text Amendment, noting the proposed text is consistent to what was last discussed among the Board. Mr. Savidant said with a consensus of the members, he would circulate the proposed Zoning Ordinance Text Amendment internally for review and bring it back to the Planning Commission to schedule a Public Hearing.

Chair Edmunds opened the floor for public comment.

Present to speak were:

- Omar Shouhayib of Choice Development, 4254 Beach Road.
- Dan McLeish, McLeish Building, 650 E. Big Beaver.

Chair Edmunds closed the floor for public comment.

It was the consensus of the members to circulate the proposed amendment for internal review.

13. PUBLIC COMMENT – Items on Current Agenda

There was no one present who wished to speak.

14. PLANNING COMMISSION COMMENT

There were general Planning Commission comments.

Mr. Savidant reported that City Council, in its consideration of the proposed Zoning Ordinance Text Amendment relating to Oil and Gas Extraction, scheduled a joint meeting with the Planning Commission on September 14, 2015 at 6:00 p.m. City Council intent is to have direct communication with the Planning Commission on the matter. Mr. Savidant said representatives from Michigan Department of Environmental Quality (MDEQ), Associated Petroleum Industries of Michigan and Oakland County Water Resources would be present at the meeting.

The Regular meeting of the Planning Commission adjourned at 10:45 p.m.

Respectfully submitted,

Donald Edmunds, Chair

Kathy L. Czarnecki, Recording Secretary

G:\Planning Commission Minutes\2015 PC Minutes\Draft\2015 07 28 Regular Meeting_Draft.doc

Date: August 6, 2015
To: Planning Commission
From: R. Brent Savidant, Planning Director
Subject: REMOVAL OF TUCKER STREET BARRICADE

This item was initiated by James and Dorothy Konarske, Troy residents who live at 2237 Drake, who submitted a request to remove the barricade on Tucker Street, east of John R Road. This item will be forwarded to City Council for consideration. A recommendation from the Planning Commission is sought to assist City Council in this issue.

Establishment of Tucker Street Barricade

Final Preliminary Plat Approval was granted for Long Lake Meadows Subdivision by City Council on September 14, 1987 (Resolution #87-1086). In addition to approving 91 residential lots, City Council approved a vehicular barricade on Tucker Street:

BE IT FURTHER RESOLVED, That before removal of the barrier to and from Tucker Street, said street shall be paved, a Public Hearing is to be scheduled and notice of the Public Hearing is to be sent to all property owners on Tucker Street.

The barricade is located approximately 370 west of Long Lake Meadows Subdivisions and still stands today (see attached photos). The project file indicates that on February 10, 1987 the Planning Commission recommended a barricade on Tucker “until future development occurs in the Tucker Street area”. This recommendation appears to stem from residents’ opposition to the Tucker Street connection.

Approval of Hunters Park 2 Site Condominium

On March 24, 2015 the Planning Commission granted Preliminary Site Condominium Approval for Hunters Park 2 Site Condominium, a 25-unit single-family detached development located on the east side of John R between Tucker and Mayflower.

Some residents, including James and Dorothy Konarske, stated their opposition to the project at the Planning Commission meeting, based primarily on the proposed vehicular connection with Drake. This connection eliminated the “dead end” status of Drake and provided a more direct vehicular connection with John R Road.

The vehicular connection was requested by planning, engineering, and public safety staff for the following reasons:

1. A more convenient route through residential streets to Wass Elementary is preferred over a route that uses a major road.
2. Interconnected streets are the preferred alternative for emergency vehicle access, as requested by the Troy Fire Department.

3. Providing interconnected neighborhoods is a policy of the city.
4. The connection to Drake would provide another access point for the existing subdivisions to the east out to John R rather than forcing the rather circuitous route that is currently in place. This could reduce traffic to Long Lake from the Saffron approach.
5. A direct connection to Drake could also facilitate better operations for other “services” such as school buses, garbage collection, police patrol, snow plowing, mail services and others while reducing travel time and travel distance servicing this section of the City.
6. Tucker is barricaded at the end of the concrete section, just west of Standish, so there is no connection from the subdivisions to the east out to John R along Tucker.
7. Drake is a public road as will be the new roads in Hunters Park 2. City policy is to provide connected public streets.

Request to Remove Tucker Street Barricade

James and Dorothy Konarske submitted a formal request to remove the barricade on Tucker. In their request, the Konarske’s point out an inconsistency related to the City’s street connectivity policy. Specifically, Tucker Street was barricaded in 1987 which eliminated a vehicular connection to John R Road and reduced interconnectivity.

City Council has the authority to pass a resolution rescinding the requirements that the barrier on Tucker Street remain in place.

Traffic Committee Consideration

Traffic Committee considered this item on July 15, 2015. Notices were sent to properties in the SE quarter of Section 13 (322 addresses).

A petition was submitted opposed to the removal of the barricade and was signed by sixty-two (62) residents in the immediate area. Emails in opposition to removing the barricade were received from twenty-five (25) residents. Emails supporting removal were received from seven (7) residents. A majority of residents spoke in opposition to removing the barricade, while a minority spoke in favor of removing the barricade.

A motion was made at the conclusion of public comment to “*Leave the barricade in place and encourage an Emergency Vehicle Address (EVA)*”. This motion ended in a tie vote (3-3).

A summary of the meeting and draft minutes from the meeting are attached.

Recommendations

The Planning Department sought input on this matter from various departments and organizations. The following is a summary of their comments:

Troy Police Department - Police Chief

“The Troy Police Department is in support of removing any barricade, in particular the Tucker Street barricade, that impedes, or otherwise delays the response of any emergency responder. Often the need for a timely response is crucial in emergency situations”.

Troy Fire Department – Fire Chief

“The fire department is in support of removing any barricade, in particular the Tucker Street barricade, that impedes, or otherwise delays the response of emergency responders, including fire, police, and EMS. In the case of the Tucker Street barricade, the fire department has experienced several incidents where access was delayed due to the barricade. Anytime there are street segments with the same name that are not contiguous, there is the potential for delays in emergency response. Typically these are caused by callers providing inexact locations when they contact dispatch”.

Troy Fire Department – Fire Station 5 Captain

“From a Station 5 response perspective, removal of the barrier would not only assist in responding to Tucker addresses that are on the east side of the barrier, but would also give a shorter route into that subdivision for addresses on Standish, Radcliffe, Saffron, etc”.

OHM, City Traffic Consultant

“We support the removal of the barricade along Tucker Street for many of the same reasons the City supported the connection with Drake. This opening of Tucker will provide an even more direct route to John R than the connection with Drake. Here are some reasons to open Tucker:

1. It will provide a more convenient route through residential streets to Wass Elementary, which is preferred over a route that uses a major road.
2. Interconnected streets are the preferred alternative for connected emergency vehicle access.
3. Providing interconnected neighborhoods is a policy of the city.
4. The connection to Tucker would provide another access point for the existing subdivisions to the east out to John R rather than forcing the rather circuitous route that is currently in place.
5. Opening up Tucker could also facilitate better operations for other “services” such as school buses, garbage collection, police patrol, snow plowing, mail services and others while reducing travel time and travel distance servicing this section of the City.”

Troy School District Transportation Liaison

“This should have no impact on our transportation routes. We don’t plan on changing routes with the barrier down”.

City Policy on Street Interconnectivity

The Master Plan is the City’s policy document that relates to land use and development. The following are excerpts from the City of Troy Master Plan:

Page 12: Transportation Troy is a complex place that contains diverse neighborhoods, business districts, industrial and educational campuses, and a wide variety of roads, from freeways to neighborhood streets. These ingredients are in place and complement one another to make up the City of Troy. To sustain the positive relationship between land uses and street characters, linking and connecting the City through multiple methods is critical.

Page 46: 10. Provide a supporting street system and circulation system; interconnected street and circulation systems better support alternative forms of transportation.

Page 71: Land Use Planning and Transportation; to develop compact complete land use patterns where a variety of uses are mixed to increase alternatives to automobile travel. Strategies include contiguous development patterns, parking plans, street design and traffic rules, trip reduction measures, and stakeholder participation.

Page 88: Elements of Great Streets and Neighborhoods Great Streets • Connect smoothly with the rest of the street network.

Page 155: Provide a supporting street system and circulation system—Well-planned communities with a supporting network of local and collector streets, unified property access and circulation systems are better able to accommodate development.

Summary

City Management seeks a recommendation from the Planning Commission on whether to remove the barricade on Tucker Street. City Council has the authority to pass a resolution rescinding the requirements that the barrier on Tucker Street remain in place. Options available to City Council related to this item include the following:

1. Do nothing (barricade remains).
2. Remove barricade, gravel road remains unpaved.
3. Remove barricade, pave gravel road.
4. Remove barricade, replace with Emergency Vehicle Access (EVA).

Attachments:

1. Email from City Manager Brian Kischnick to City Council/Planning Commission.
2. Traffic Committee item including:
 - a. Map of Area
 - b. Map of gravel roads and barricades in Troy
 - c. Photos of barricade taken in May, 2015.
 - d. City Council resolution from September 14, 1987.
 - e. Planning Commission agenda Item for March 24, 2015 meeting.
 - f. Request from James and Dorothy Konarske to eliminate Tucker Street barricade.
 - g. Public input received on or before July 14, 2015.
3. Summary of July 15, 2015 Traffic Committee meeting.
4. Minutes (draft) from July 15, 2015 Traffic Committee meeting.
5. Public input received after July 14, 2015.

Brent Savidant

From: Brian M Kischnick
Sent: Thursday, August 06, 2015 2:36 PM
To: Brent Savidant
Subject: FW: EVA's, Barricades and Tucker Street

Brian M. Kischnick | City Manager

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Cell: 989.233.7335 | bkischnick@troymi.gov  

From: Brian M Kischnick
Sent: Thursday, August 6, 2015 2:20 PM
To: City Council Email <CityCouncilEmail@troymi.gov>
Cc: Brian M Kischnick <B.Kischnick@troymi.gov>
Subject: EVA's, Barricades and Tucker Street

Mayor and Council members:

I have received many forwarded emails from City Council members that were received from Tucker Street residents regarding the barricade there as well as the process. I appreciate the emails since I have not received one email, phone call or communication from Tucker Street residents (nor Drake for that matter). I personally attended the Traffic Committee meeting to hear the comments firsthand. The commentary will be used to assist me in drafting a recommendation to the City Council.

I believe in due process for all residents and issues related to city services. Therefore, I've provided the following information, thinking and justification below regarding the process used for this issue. In addition to evaluating the Tucker street barricade (and the functionality of EVA's Emergency Vehicle Access) I am taking a comprehensive look at all EVA's in the city.

To: Planning Commission:

On August 11, 2015 the Planning Commission will consider the Tucker barricade. The item was considered by the Traffic Committee on July 15, 2015.

I initiated the evaluation of the Tucker barricade because it came up during the March 24, 2015 Planning Commission meeting, during consideration of the Hunters Park 2 Site Condominium. Furthermore, there is a Planning Commission resolution recommending the Tucker barricade from 1987 and a City Council resolution establishing the barricade on Tucker from 1987. It is therefore important to give due process to the issue regardless of its lack of connection to the new site condominium to the north. Additionally, there is no Emergency Vehicle Access (EVA) on Tucker. It is simply a barricade, which is not a good situation for public safety response. This is not only my opinion but is also the opinion of both the Fire Department and Police Department. While there has been much conversation regarding EVA's, to my knowledge Captain Bob Redmond and I are the only ones to actually take a police car through one. So I know about EVA's firsthand. We will show the video of the test at the City Council meeting where this will be on the agenda.

I could go on, but suffice it to say this is why we have a Traffic Committee, Planning Commission and City Council. So, it is my opinion that this issue has standing, is germane to a larger public safety issue and is going through a legitimate and appropriate vetting process.

As an aside, the fact that the Traffic Committee voted 3 to 3 on the issue of removal indicates to me that we are doing the right thing. Due process will be the fundamental framework for making a proper and informed decision.

Brian M. Kischnick | City Manager _____

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Cell: 989.233.7335 | bkischnick@troymi.gov  



CITY COUNCIL INFORMATION

Date: July 16, 2015

To: Brian Kischnick, City Manager

From: Mark F. Miller, Director of Economic and Community Development
Steven J. Vandette, City Engineer
William J. Huotari, Deputy City Engineer/Traffic Engineer

Subject: Additional Information on Tucker Street Barricade

Attached is information for City Council on the Tucker Street barricade issue addressed at the July 15, 2015 Traffic Committee meeting. The same item will be included on the August 11, 2015 Planning Commission. A complete report with minutes of both meetings and supporting documentation will be provided to City Council at their meeting of August 24, 2015.

Included as information is:

1. Agenda item from the July 15, 2015 Traffic Committee meeting
2. Emails received
3. A map delineating locations of emails received, noting support or opposition to removing the barricade
4. Residents in attendance at the meeting that signed in
5. Petition submitted at the meeting signed by sixty-two (62) residents in the immediate area
6. Fire Department Memo
7. Typical Emergency Vehicle Access (EVA)



TRAFFIC COMMITTEE REPORT

June 24, 2015

TO: Traffic Committee

FROM: Bill Huotari, Deputy City Engineer/ Traffic Engineer

SUBJECT: Request to Discuss Interconnection
Tucker, John R to Standish

Background:

A request to remove the barricade on Tucker Street, east of John R, was made by James and Dorothy Konarske of 2237 Drake. The Konarske's point out an inconsistency related to the City's street connectivity policy. Specifically, Tucker Street was barricaded in 1987, which eliminated a vehicular connection to John R Road and reduced interconnectivity.

The newly approved Hunters Park 2 Site Condominium development, north of Tucker, includes a vehicular connection to Drake. This connection eliminated the "dead end" status of Drake and provided a more direct vehicular connection with John R.

The request to remove the barricade will be forwarded to City Council for consideration after a recommendation is made by the Traffic Committee as well as the Planning Commission.

A memo is provided from Planning Director, Brent Savidant, which discusses the history of the barricade as well as providing input from various departments and organizations.

Date: June 30, 2015

To: Planning Commission
Traffic Committee

From: R. Brent Savidant, Planning Director

Subject: REMOVAL OF TUCKER STREET BARRICADE

This item was initiated by James and Dorothy Konarske, Troy residents who live at 2237 Drake, who submitted a request to remove the barricade on Tucker Street, east of John R Road. This item will be forwarded to City Council for consideration. A recommendation from the Planning Commission and Traffic Committee are sought to assist City Council in this issue.

Establishment of Tucker Street Barricade

Final Preliminary Plat Approval was granted for Long Lake Meadows Subdivision by City Council on September 14, 1987 (Resolution #87-1086). In addition to approving 91 residential lots, City Council approved a vehicular barricade on Tucker Street:

BE IT FURTHER RESOLVED, That before removal of the barrier to and from Tucker Street, said street shall be paved, a Public Hearing is to be scheduled and notice of the Public Hearing is to be sent to all property owners on Tucker Street.

The barricade is located approximately 370 west of Long Lake Meadows Subdivisions and still stands today (see attached photos). The project file indicates that on February 10, 1987 the Planning Commission recommended a barricade on Tucker “until future development occurs in the Tucker Street area”. This recommendation appears to stem from residents’ opposition to the Tucker Street connection.

Approval of Hunters Park 2 Site Condominium

On March 24, 2015 the Planning Commission granted Preliminary Site Condominium Approval for Hunters Park 2 Site Condominium, a 25-unit single-family detached development located on the east side of John R between Tucker and Mayflower.

Some residents, including James and Dorothy Konarske, stated their opposition to the project at the Planning Commission, based primarily on the proposed vehicular connection with Drake. This connection eliminated the “dead end” status of Drake and provided a more direct vehicular connection with John R Road.

The vehicular connection was requested by planning, engineering, and public safety staff for the following reasons:

1. A more convenient route through residential streets to Wass Elementary is preferred over a route that uses a major road.

2. Interconnected streets are the preferred alternative for emergency vehicle access, as requested by the Troy Fire Department.
3. Providing interconnected neighborhoods is a policy of the city.
4. The connection to Drake would provide another access point for the existing subdivisions to the east out to John R rather than forcing the rather circuitous route that is currently in place. This could reduce traffic to Long Lake from the Saffron approach.
5. A direct connection to Drake could also facilitate better operations for other “services” such as school buses, garbage collection, police patrol, snow plowing, mail services and others while reducing travel time and travel distance servicing this section of the City.
6. Tucker is barricaded at the end of the concrete section, just west of Standish, so there is no connection from the subdivisions to the east out to John R along Tucker.
7. Drake is a public road as will be the new roads in Hunters Park 2. City policy is to provide connected public streets.

Request to Remove Tucker Street Barricade

James and Dorothy Konarske submitted a formal request to remove the barricade on Tucker. In their request, the Konarske’s point out an inconsistency related to the City’s street connectivity policy. Specifically, Tucker Street was barricaded in 1987 which eliminated a vehicular connection to John R Road and reduced interconnectivity.

City Council has the authority to pass a resolution rescinding the requirements that the barrier on Tucker Street remain in place.

Recommendations

The Planning Department sought input on this matter from various departments and organizations. The following is a summary of their comments:

Troy Police Department - Police Chief

“The Troy Police Department is in support of removing any barricade, in particular the Tucker Street barricade, that impedes, or otherwise delays the response of any emergency responder. Often the need for a timely response is crucial in emergency situations”.

Troy Fire Department – Fire Chief

“The fire department is in support of removing any barricade, in particular the Tucker Street barricade, that impedes, or otherwise delays the response of emergency responders, including fire, police, and EMS. In the case of the Tucker Street barricade, the fire department has experienced several incidents where access was delayed due to the barricade. Anytime there are street segments with the same name that are not contiguous, there is the potential for delays in emergency response. Typically these are caused by callers providing inexact locations when they contact dispatch”.

Troy Fire Department – Fire Station 5 Captain

“From a Station 5 response perspective, removal of the barrier would not only assist in responding to Tucker addresses that are on the east side of the barrier, but would also give a shorter route into that subdivision for addresses on Standish, Radcliffe, Saffron, etc”.

OHM, City Traffic Consultant

“We support the removal of the barricade along Tucker Street for many of the same reasons the City supported the connection with Drake. This opening of Tucker will provide an even more direct route to John R than the connection with Drake. Here are some reasons to open Tucker:

1. It will provide a more convenient route through residential streets to Wass Elementary, which is preferred over a route that uses a major road.
2. Interconnected streets are the preferred alternative for connected emergency vehicle access.
3. Providing interconnected neighborhoods is a policy of the city.
4. The connection to Tucker would provide another access point for the existing subdivisions to the east out to John R rather than forcing the rather circuitous route that is currently in place.
5. Opening up Tucker could also facilitate better operations for other “services” such as school buses, garbage collection, police patrol, snow plowing, mail services and others while reducing travel time and travel distance servicing this section of the City.”

Troy School District Transportation Liaison

“This should have no impact on our transportation routes. We don’t plan on changing routes with the barrier down”.

City Policy on Street Interconnectivity

The Master Plan is the City’s policy document that relates to land use and development. The following are excerpts from the City of Troy Master Plan:

Page 12: Transportation Troy is a complex place that contains diverse neighborhoods, business districts, industrial and educational campuses, and a wide variety of roads, from freeways to neighborhood streets. These ingredients are in place and complement one another to make up the City of Troy. To sustain the positive relationship between land uses and street characters, linking and connecting the City through multiple methods is critical.

Page 46: 10. Provide a supporting street system and circulation system; interconnected street and circulation systems better support alternative forms of transportation.

Page 71: Land Use Planning and Transportation; to develop compact complete land use patterns where a variety of uses are mixed to increase alternatives to automobile travel. Strategies include contiguous development patterns, parking plans, street design and traffic rules, trip reduction measures, and stakeholder participation.

Page 88: Elements of Great Streets and Neighborhoods Great Streets • Connect smoothly with the rest of the street network.

Page 155: Provide a supporting street system and circulation system—Well-planned communities with a supporting network of local and collector streets, unified property access and circulation systems are better able to accommodate development.

Summary

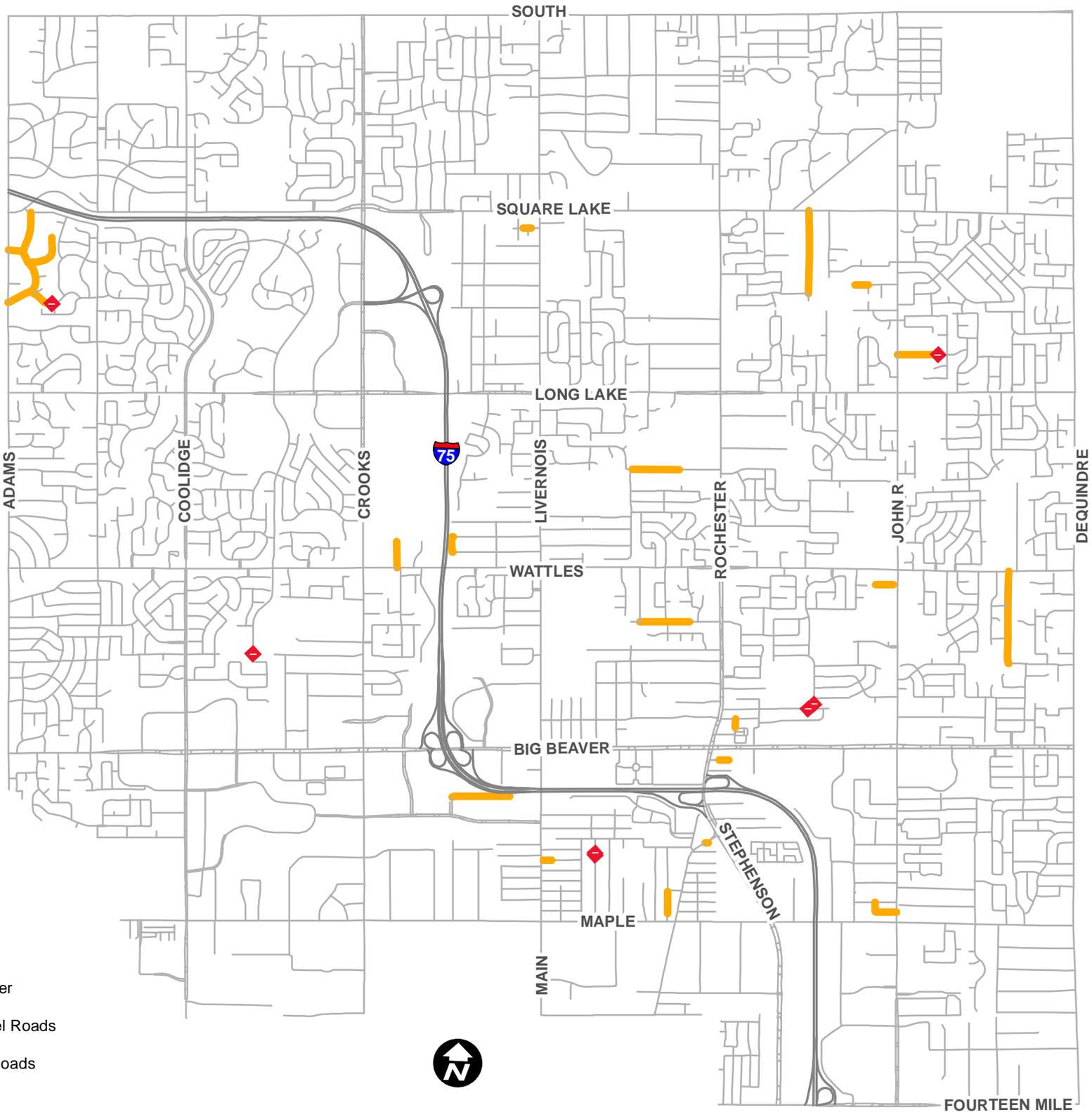
City Council has the authority to pass a resolution rescinding the requirements that the barrier on Tucker Street remain in place. City Management seeks a recommendation from the Planning Commission and Traffic Committee on whether to remove the barricade on Tucker Street. Options available to City Council related to this item include the following:

1. Do nothing (barricade remains).
2. Remove barricade, gravel road remains unpaved.
3. Remove barricade, pave gravel road.

Estimated costs and methods for funding will be determined at a later date and provided for City Council.

Attachments:

1. Map of Area
2. Map of gravel roads and barricades in Troy
3. Photos of barricade taken in May, 2015.
4. City Council resolution from September 14, 1987.
5. Planning Commission agenda Item for March 24, 2015 meeting.
6. Request from James and Dorothy Konarske to eliminate Tucker Street barricade.



◆ Road Barrier

— Troy Gravel Roads

— All Other Roads



FOURTEEN MILE



ROAD
ENDS

FINAL PRELIMINARY PLAT APPROVAL - LONG LAKE MEADOWS SUBDIVISION - NORTH SIDE C-18
OF LONG LAKE, EAST OF JOHN R - SECTION 12

Resolution #87-1086
Moved by Pallotta
Supported by Liebrecht

9/14/87

RESOLVED, That final approval of the preliminary plat for Long Lake Meadows Subdivision, located on the north side of Long Lake, east of John R, in Section 12, be granted after cash, irrevocable bank letter of credit, or certified check has been filed with the Clerk in the amount of \$1,002,430.50 to insure construction of all required improvements within a period of two years from date of final approval of the Preliminary Plat; also after receipt of a cash payment of \$78,770.50 for sidewalk closures, water main testing and chlorination, sidewalks-retention basin, street name and traffic signs, street island improvements, landscaping and screen plantings - non-access greenbelt easements, landscaping and screen planting - retention basin, seeding, mulching and topsoil - Long Lake Road, Engineering review and inspection fees - 4.7%, soil erosion and sedimentation control permit, testing services, walkway and split rail fence - retention basin; and

BE IT FURTHER RESOLVED, That the Mayor and City Clerk are hereby authorized to execute the Subdivision Agreement, a copy of which shall be attached to the original minutes of this meeting.

BE IT FURTHER RESOLVED, That before removal of the barrier to and from Tucker Street, said street shall be paved, a Public Hearing is to be scheduled and notice of the Public Hearing is to be sent to all property owners on Tucker Street.

Yeas: All-7

- 5 -

DATE: March 5, 2015

TO: Planning Commission

FROM: R. Brent Savidant, Planning Director

SUBJECT: PRELIMINARY SITE PLAN REVIEW – Proposed Hunters Park 2 Site Condominium, 25 units/lots, East side of John R between Tucker and Mayflower, Section 12, Currently Zoned R-1C (One Family Residential) District

The petitioner Mondrian Properties Hunters Park LLC submitted the above referenced Preliminary Site Plan Approval application for a 25-unit site condominium. The property is currently zoned R-1C (One Family Residential) District. The Planning Commission is responsible for granting Preliminary Site Plan Approval for site condominium applications.

The attached report prepared by Carlisle/Wortman Associates, Inc. (CWA), the City's Planning Consultant, summarizes the project. CWA prepared the report with input from various City departments including Planning, Engineering, Public Works and Fire. City Management supports the findings of fact contained in the report and recommends approval of the project, as noted.

Attachments:

1. Maps
2. Report prepared by Carlisle/Wortman Associates, Inc.

cc: Applicant
File/ Hunters Park 2 Site Condominium

G:\SUBDIVISIONS & SITE CONDOS\Hunters Park 2 Sec 12\PC Memo 03 24 2015.docx

PROPOSED RESOLUTION

PRELIMINARY SITE PLAN REVIEW – Proposed Hunters Park 2 Site Condominium, 25 units/lots, East side of John R between Tucker and Mayflower, Section 12, Currently Zoned R-1C (One Family Residential) District

Resolution # PC-2015-03-

Moved by:

Seconded by:

RESOLVED, That Preliminary Site Condominium Approval, pursuant to Article 8 and Section 10.02 of the Zoning Ordinance, as requested for Hunters Park 2 Site Condominium, 25 units/lots, East side of John R between Tucker and Mayflower, Section 12, Currently Zoned R-1C (One Family Residential) District, be granted, subject to the following:

1. Provide a midblock pedestrian crossing along the newly created Kingston Drive.
2. Construct the connection to Drake Road as shown in Site Plan, Sheet P-3.
3. Add one (1) additional tree along John R. Road.

_____) or

(denied, for the following reasons: _____) or

(postponed, for the following reasons: _____)

Yes:

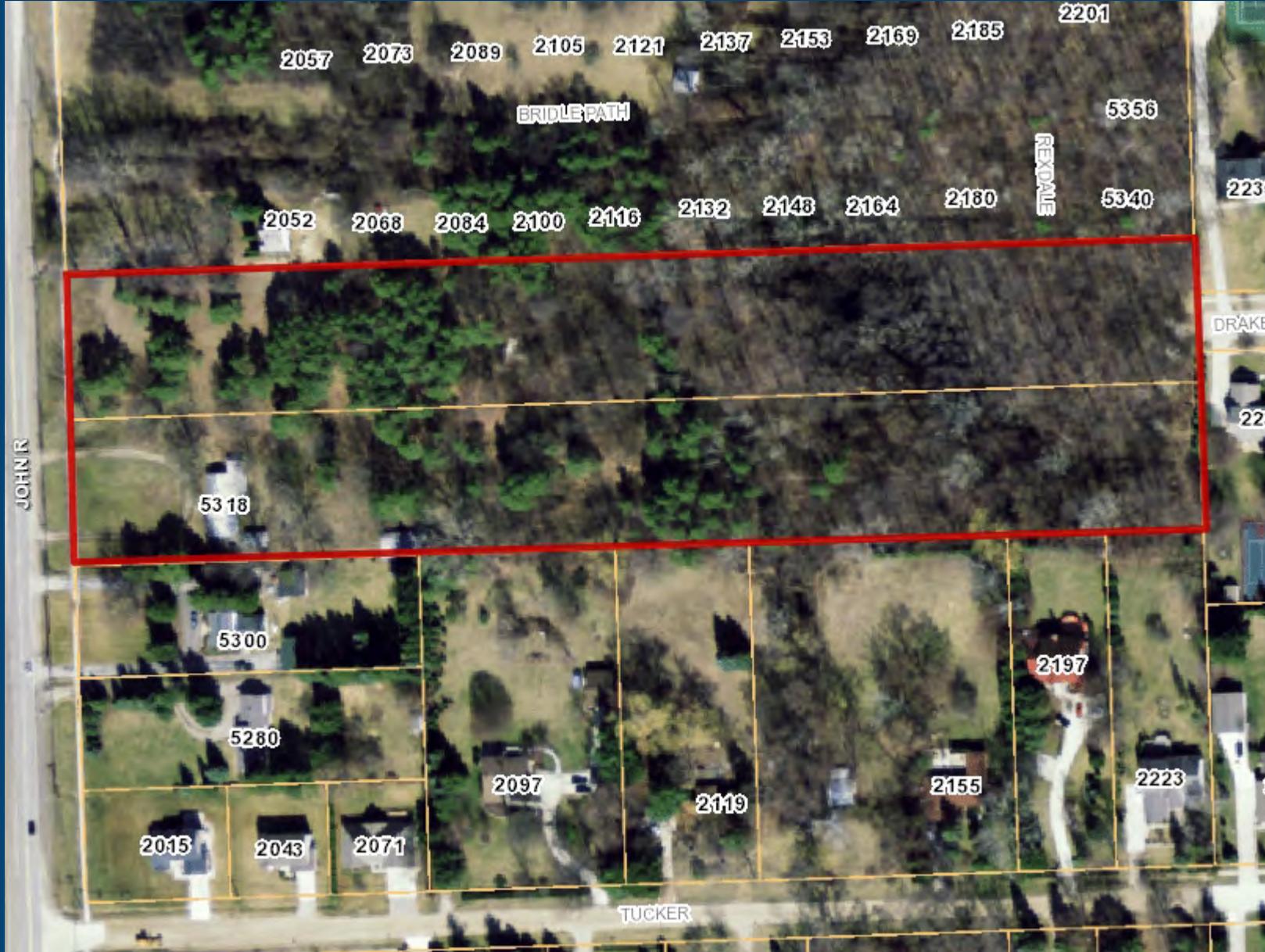
No:

Absent:

MOTION CARRIED / FAILED

HUNTERS PARK 2 SITE CONDOMINIUM

City of Troy Planning Department



Legend:

- Aerial
- Red: Band_1
- Green: Band_2
- Blue: Band_3

417 0 208 417 Feet

Scale 1:2,500

Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Printed: 2/12/2015

HUNTERS PARK 2 SITE CONDOMINIUM

City of Troy Planning Department



Legend:

Form Based Zoning 2

- (PUD) Planned Unit Development
- (CF) Community Facilities District
- (EP) Environmental Protection District
- (BB) Big Beaver Road
- (MR) Maple Road
- (NN) Neighborhood Nodes (A-U)
- (CB) Community Business
- (GB) General Business
- (IB) Integrated Industrial Business District
- (O) Office Building District
- (OM) Office Mixed Use
- (P) Vehicular Parking District
- (R-1A) One Family Residential District
- (R-1B) One Family Residential District
- (R-1C) One Family Residential District
- (R-1D) One Family Residential District
- (R-1E) One Family Residential District
- (RT) One Family Attached Residential District
- (MF) Multi-Family Residential
- (MHP) Manufactured Housing
- (UR) Urban Residential
- (RC) Research Center District
- (PV) Planned Vehicle Sales

Aerial

- Red: Band_1
- Green: Band_2
- Blue: Band_3

467 0 233 467 Feet

Scale 1:2,800



605 S. Main Street, Ste. 1
Ann Arbor, MI 48104

(734) 662-2200
(734) 662-1935 Fax

Date: March 12, 2015

Site Condominium Plan For City of Troy, Michigan

PROJECT INFORMATION

Applicant:	Mondrian Properties
Project Name:	Hunters Park 2 Site Condominium
Plan Date:	March 4, 2015
Location:	East side John R Road, between 18 Mile Road and E. Square Lake Road.
Zoning:	R1-C, One-Family Residential District
Action Requested:	Preliminary Site Condominium Approval
Required Information:	Noted

PROJECT AND SITE DESCRIPTION

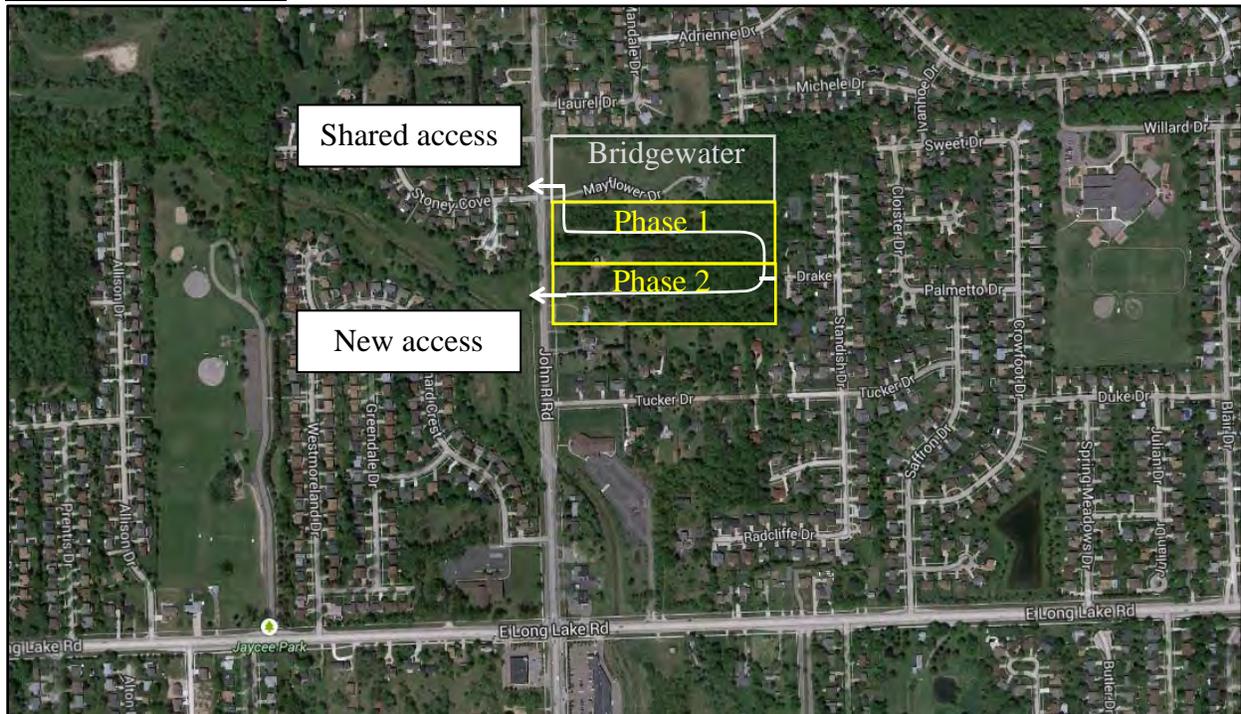
We are in receipt of a site condominium application which includes a preliminary site plan, topographic survey, grading plan, utility plan, tree preservation plan and tree inventory, elevations and floor plans. This project is the second phase of the Hunters Park site condominium development. Phase 2 is proposed for a 7.92 acre site immediately south of the phase 1 development.

The applicant has submitted two (2) plans for Planning Commission consideration. The first plan, as shown on sheet P-3, requests approval for 24 lots single family detached site condominium units. The second plan, labeled "alternative site plan," requests approval for 25 single family detached site condominium units. The only difference between the two plans is that site plan 1, Sheet P-3, includes a vehicular connection to Drake Road. The vehicular connection was requested by planning, engineering, and public safety staff.

The Hunters Park development will have 45 or 46 lots in total, including phase 1 and 2. Access to all units will be via a new residential street off of John R Road and Mayflower Road to the north, and Drake to the east.

The phase 2 subject property is currently improved with one (1) single family home, which will be demolished. The site has significant tree cover and a large wetland area. The site is zoned R-1C and the proposed site condominium use is permitted by-right.

Location of subject site:



Size of subject property:

Phase 2 is 7.92 acres in area.

Current use of subject property:

The subject property has one (1) existing single family home.

Proposed use of subject site:

The proposed use is single family residential site condominium.

Current Zoning:

The property is currently zoned R-1C, One Family Residential District.

Surrounding Property Details:

<u>Direction</u>	<u>Zoning</u>	<u>Use</u>
North	R-1C, One-family Residential District	Single-family homes
South	R-1C, One-family Residential District	Single-family homes
East	R-1C, One-family Residential District	Single-family homes

West	R-1C, One-family Residential District	Single-family homes, Open space
------	---------------------------------------	---------------------------------

SITE ARRANGEMENT, ACCESS, AND CIRCULATION

The applicant is applying the lot size averaging option, permitted and regulated by Section 10.01. The lot range in size between 9,477 to 20,238 square feet and the average lot size is 10,964 square feet. The proposed lots are regular in shape, allow for adequate setbacks, and permit sufficient space for the homes and ingress and egress for each unit.

The project is phase 2 of the existing Hunters Park site condominium. Phase 2 will provide an additional access drive off John R. Road and also connect to the shared access with the Bridgewater site condominium development through Hunters Park Phase 1. The development will be served by a new public road, which runs perpendicular to John R. Road and connects to Rexdale Drive from Phase 1. The new road will have a sixty (60) foot right-of-way with 5-foot sidewalks on both sides of the road. The applicant has provided 5-foot sidewalks in the development, however we recommend that the applicant provide a midblock pedestrian crossing along the newly created Kingston Drive.

As an infill development project, Hunters Park should be connected to the existing residential fabric. As a result, the applicant should also connect to the residential neighborhood to east via the Drake Road stub street. The vehicular connection was requested by planning, engineering, and public safety staff for the following reasons:

1. Hunters Park 2 (as well as Hunters Park and Bridgewater) will be part of the attendance area for Wass Elementary so a more convenient route through residential streets to the school is preferred rather than a route out to a major road.
2. Interconnected streets are the preferred alternative for connected emergency vehicle access, as requested by the Troy Fire Department.
3. Providing interconnected neighborhoods are policy of the city. Rather than creating "island" neighborhoods.
4. The connection to Drake would provide another access point for the existing subdivisions to the east out to John R rather than forcing the rather circuitous route that is currently in place. This could reduce traffic to Long Lake from the Saffron approach.
5. A direct connection to Drake could also facilitate better operations for other "services" such as school buses, garbage collection, police patrol, snow plowing, mail services and others while reducing travel time and travel distance servicing this section of the City.
6. Tucker is barricaded at the end of the concrete section, just west of Standish, so there is no connection from the subdivisions to the east out to John R along Tucker. This barricade was placed by resolution until such a time as Tucker is paved.
7. Drake is a public road as will be the new roads in Hunters Park 2. City policy is to provide connected public streets.

Though a full vehicular connection is recommended, if a full vehicular connection is not provided, the fire department recommends that a minimum a dedicated pedestrian and emergency vehicle connection be made.

Items to be Addressed: 1). Provide a midblock pedestrian crossing along the newly created Kingston Drive; and 2). Construct the connection to Drake Road as shown in Site Plan, Sheet P-3.

AREA, WIDTH, HEIGHT, SETBACKS

Required and Provided Dimensions:

Table 4.06.C establishes the requirements for the R-1C District. The requirement and the proposed dimensions are as follows:

	<u>Required</u>	<u>Provided</u>	<u>Compliance</u>
Minimum Lot Area*	10,500 sq ft	Sheet P-3: 10,964 (Ave) 9,957 sq ft (Min) Alternative: 11,302 (Ave) 9,477 sq ft (Min)	Complies
Minimum Lot Width	85 ft	76.5 ft (min), Avg exceeds 85 ft	Complies
<u>Setbacks</u>			
Front	30 ft	30 ft	Complies
Side (Least)	10 ft	10 ft	Complies
Side (Total)	20 ft	20 ft	Complies
Rear	40 ft	40 ft	Complies
Maximum Building Height	30 ft, 2.5 story	20'-1/4", 2 story	Complies
Minimum Floor Area per Unit	1,200 sq ft	1,800	Complies
Maximum Lot Coverage	30%	19%	Complies

*The lot size average option has been applied and Section 10.01 standards have been met.

The applicant has meet all R-1C bulk requirements.

Items to be Addressed: None

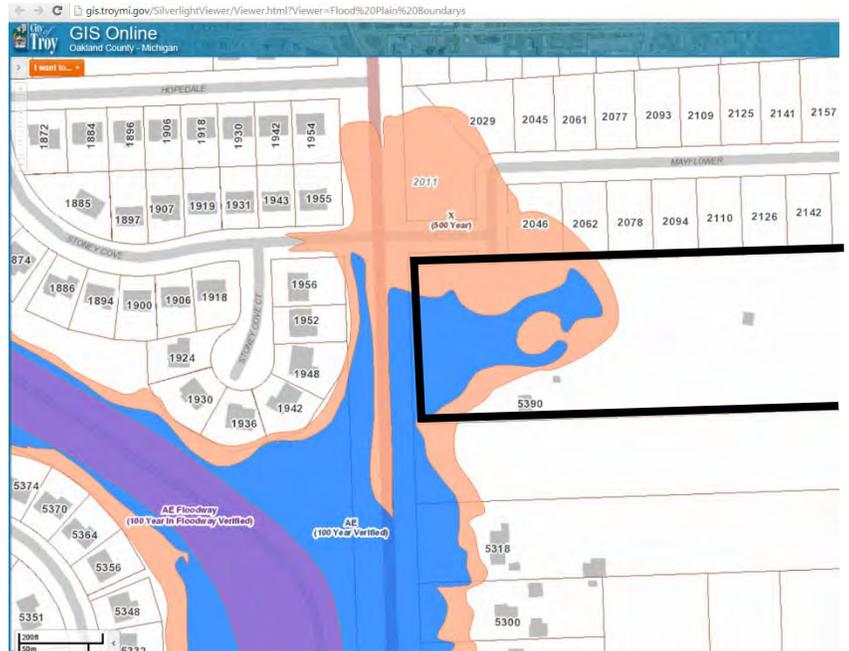
NATURAL RESOURCES

Topography - The grading plan shows a gradual slope down from east to west. The detention basin for Hunters Park is located in the southwest corner of phase 1.

Woodlands – The site has significant tree cover. The applicant has tagged over 650 trees on the site, primarily American Elm, Scotch Pine, Wild Black Cherry, and Silver Maple. Most of the trees are between 5 and 10 inches DBH and at least half are in good condition. The applicant does not provide the exact number, however sheet P-6 indicates that most of the site will be clear-cut with some areas to be subject to selective clearing by builders.

The applicant is encouraged to selectively clear trees in order to preserve trees particularly along proposed new lots and existing development, and in non-building envelopes of new lots.

Wetlands/Flood Plain - The front/westerly portion of the site is located within the 100 year flood plain. An application is being made to FEMA for a LOMA. The LOMA will be required for final site plans submittal. Additionally, there is a 1.04 acre non-regulated wetland located in the central portion of the site.



Items to be Addressed: The applicant is encouraged to selectively clear trees in order to preserve trees particularly along proposed new lots and existing development, and in non-building envelopes of new lots.

LANDSCAPING

The Landscape Plan includes a mixture of evergreen and deciduous trees. All proposed species fall within Troy regulations and are not prohibited. Site condominium and subdivision landscaping are regulated by Section 13.02.F.2.

	<u>Required</u>	<u>Provided</u>	<u>Compliance</u>
John R Road Screening Alt 2	1 tree per 10 lineal feet (300 lf =30 tree)	29 trees	Add (1) additional tree.
Proposed Kingston Drive Internal Street	1 tree per 50 lineal feet (2,278 lf = 5 trees)	46 trees	Complies

The applicant has provided landscaping at the entrance. Plantings are 78% native plants and include a variety of species.

Access drives should not be subtracted from the lineal dimension used to determine the minimum number of trees for greenbelts or internal streets. The applicant should provide landscaping based on the 300 foot frontage along John R Road.

Items to be Addressed: Add one (1) additional tree along John R. Road.

SUBMITTAL REQUIREMENTS

Section 10.02 sets forth the intent and standards for site condominium projects.

1. Intent: The intent of this Section is to regulate site condominium projects to ensure compliance with this Ordinance and other applicable standards of the City, to provide procedures and standards for review and approval or disapproval of such developments, and to insure that each project will be consistent and compatible with other developments in the community.

The proposed site condominium project is consistent and compatible with other developments in the community, and more importantly adjacent properties. The proposed development meets the intent of the Site Condominium section of the ordinance.

Section 10.02.E. regulates physical improvements associated with condominium projects. It requires the following:

*1. Principal access and circulation through a site condominium shall be provided by public streets constructed to City standards, within sixty (60) foot wide rights-of-way. Secondary access and circulation through such developments, on which some of the residential parcels may have their sole frontage, may be provided by twenty-eight (28) foot wide streets constructed to City public street standards, within forty (40) foot private easements for public access. **The applicant has provided a 60-foot wide public right-of-way. All lots front on the 60-foot right-of-way.***

*2. Principal access to site condominium of five (5) acres or less in area may be provided by way of twenty-eight (28) foot wide streets constructed to City public street standards, within forty (40) foot private easements for public access, when in the opinion of the City Council the property configuration is such that the provision of conforming dwelling unit parcels is impractical. **Not applicable.***

*3. All entrances to major or secondary thoroughfares shall include deceleration, acceleration and passing lanes as required by Engineering Standards of the City of Troy. **The applicant has provided deceleration and acceleration lanes at the entrance to the proposed Kingston Drive along John R Road.***

*4. Sidewalks shall be constructed, in accordance with City Standards, across the frontage of all dwelling unit parcels. Utilities shall be placed within street rights-of-way, or within easements approved as to size and location by the City Engineer. **Satisfied.***

*5. All shall be served by public water, sanitary sewer, storm sewer and detention/retention systems constructed to City standards, at the expense of the developer. Easements over these systems shall be conveyed and recorded before occupancy permits are issued for dwelling units. **The applicant has proposed full utilities, but all proposed configurations and easements are subject to approval by the City engineering department.***

As noted above, all condominium projects are subject to Section 8.05.A.7, which establishes the requirements for a preliminary site plan submittal. Three additional requirements are specifically identified for residential projects. The three additional requirements, identified in 8.05.A.7.o, include:

Hunters Park 2
March 12, 2015

i. Calculation of the dwelling unit density allowable and a statement of the number of dwelling units, by type, to be provided. **The dwelling calculation is compliant with R1-C regulations.**

ii. Topography on site and fifty (50) feet beyond, drawn at two (2) foot contour intervals, with existing drainage courses, flood plains, wetlands, and tree stands indicated. **Satisfied.**

iii. The typical floor plans and elevations of the proposed buildings, with building height(s). **Satisfied.**

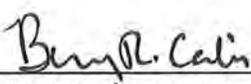
Items to be Addressed: *Noted above.*

RECOMMENDATIONS

We recommend preliminary Site Plan approval of the Hunters Park Phase 2 site condominium development given the following conditions:

1. Provide a midblock pedestrian crossing along the newly created Kingston Drive.
2. Construct the connection to Drake Road as shown in Site Plan, Sheet P-3.
3. The applicant is encouraged to selectively clear trees in order to preserve trees particularly along proposed new lots and existing development, and in non-building envelopes of new lots.
4. Add one (1) additional tree along John R. Road.





CARLISLE/WORTMAN ASSOC., INC.
Benjamin R. Carlisle, LEED AP, AICP

225-1426

cc:

BENCHMARKS
(GPS DERIVED - NAVD88)

BM#305
SET BENCH THE WEST FACE OF UTILITY POLE, 2ND UTILITY POLE SOUTH OF MAYFLOWER, ±33' EAST OF THE CENTERLINE OF JOHN R ROAD & ±27' WEST OF THE NORTHWEST PARCEL CORNER OF LOT 25.
ELEV. - 657.41

BM#306
SET BENCH THE NORTH FACE OF UTILITY POLE, NORTH OF #5300 NORTH DRIVE, ±32' EAST OF THE CENTERLINE OF JOHN R ROAD & ±28' WEST OF THE NORTHWEST PARCEL CORNER OF LOT 24.
ELEV. - 655.61

LEGAL DESCRIPTION - AS SURVEYED; (PHASE 1)
(Per Professional Engineering Associates, Inc.)

Lots 26 and 27 of Eyster's John R Acres, a subdivision of part of the Southwest 1/4 of Section 12, T.2N., R.11E., City of Troy, Oakland County, Michigan, as recorded in Liber 55 of Plats, Page 53, Oakland County Records.

Being more particularly described as:
Commencing at the West 1/4 Corner of said Section 12; thence along the east-west quarter line of Section 12, N88°02'09"E, 60.04 feet to a point on the east right-of-way line of John R. Road (60 foot half width); thence along said east right-of-way line S00°02'00"W, 341.19 feet to the northwest corner of Lot 27 of the aforementioned Eyster's John R Acres subdivision and the POINT OF BEGINNING;
thence N89°59'21"E, 1150.05 feet (platted as EAST, 1150 feet) along the north line of Lot 27 to the northeast corner of Lot 27; thence S00°00'25"E, 299.92 feet (platted as S00°02'W, 300 feet) along the east line of Lot 27 and the east line of Lot 26 of Eyster's John R Acres to the southeast corner of Lot 26; thence S89°59'21"W, 1150.26 feet (platted as WEST, 1150 feet) along the south line of said Lot 26 to the southwest corner of Lot 26 and the aforementioned east right-of-way line of John R. Road; thence along said east right-of-way line N00°02'00"E, 299.92 feet (platted as 300 feet) to the POINT OF BEGINNING; containing 7.92 acres of land, more or less.

LEGAL DESCRIPTION - AS SURVEYED; (PHASE 2)
(Per Professional Engineering Associates, Inc.)

Lots 24 and 25 of Eyster's John R Acres, a subdivision of part of the Southwest 1/4 of Section 12, T.2N., R.11E., City of Troy, Oakland County, Michigan, as recorded in Liber 55 of Plats, Page 53, Oakland County Records.

Being more particularly described as:
Commencing at the West 1/4 Corner of said Section 12; thence along the east-west quarter line of Section 12, N88°02'09"E, 60.04 feet to a point on the east right-of-way line of John R. Road (60 foot half width); thence along said east right-of-way line S00°02'00"W, 641.11 feet to the northwest corner of Lot 25 of the aforementioned Eyster's John R Acres subdivision and the POINT OF BEGINNING;
thence N89°59'21"E, 1150.26 feet (platted as EAST, 1150 feet) along the north line of said Lot 25 to the northeast corner of Lot 25; thence S00°00'25"E, 299.92 feet (platted as S00°02'W, 300 feet) along the east line of Lot 25 and the east line of Lot 24 of Eyster's John R Acres to the southeast corner of Lot 24; thence S89°59'21"W, 1150.47 feet (platted as WEST, 1150 feet) along the south line of said Lot 24 to the southwest corner of Lot 24 and the aforementioned east right-of-way line of John R. Road; thence along said east right-of-way line N00°02'00"E, 299.92 feet (platted as 300 feet) to the POINT OF BEGINNING; containing 7.92 acres of land, more or less.

LEGAL DESCRIPTION - AS SURVEYED; (OVERALL)
(Per Professional Engineering Associates, Inc.)

Lots 24, 25, 26 and 27 of Eyster's John R Acres, a subdivision of part of the Southwest 1/4 of Section 12, T.2N., R.11E., City of Troy, Oakland County, Michigan, as recorded in Liber 55 of Plats, Page 53, Oakland County Records.

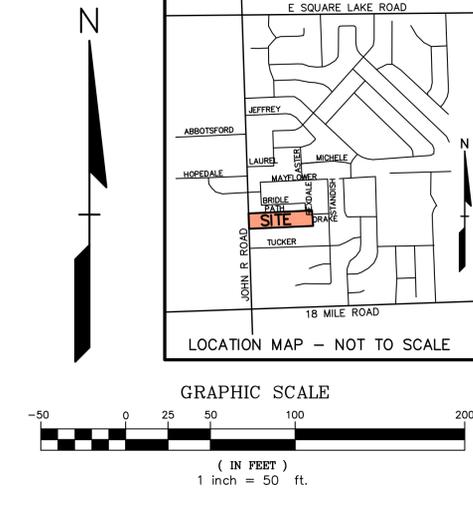
Being more particularly described as:
Commencing at the West 1/4 Corner of said Section 12; thence along the east-west quarter line of Section 12, N88°02'09"E, 60.04 feet to a point on the east right-of-way line of John R. Road (60 foot half width); thence along said east right-of-way line S00°02'00"W, 341.19 feet to the northwest corner of Lot 27 of the aforementioned Eyster's John R Acres subdivision and the POINT OF BEGINNING;
thence N89°59'21"E, 1150.05 feet (platted as EAST, 1150 feet) along the north line of Lot 27 to the northeast corner of Lot 27; thence S00°00'25"E, 599.84 feet (platted as S00°02'W, 600 feet) along the east line of the aforementioned Lots 24 thru 27, inclusive, of Eyster's John R Acres to the southeast corner of Lot 24; thence S89°59'21"W, 1150.47 feet (platted as WEST, 1150 feet) along the south line of said Lot 24 to the southwest corner of Lot 24 and the aforementioned east right-of-way line of John R. Road; thence along said east right-of-way line N00°02'00"E, 599.84 feet (platted as 600 feet) to the POINT OF BEGINNING; containing 15.84 acres of land, more or less.

LEGEND

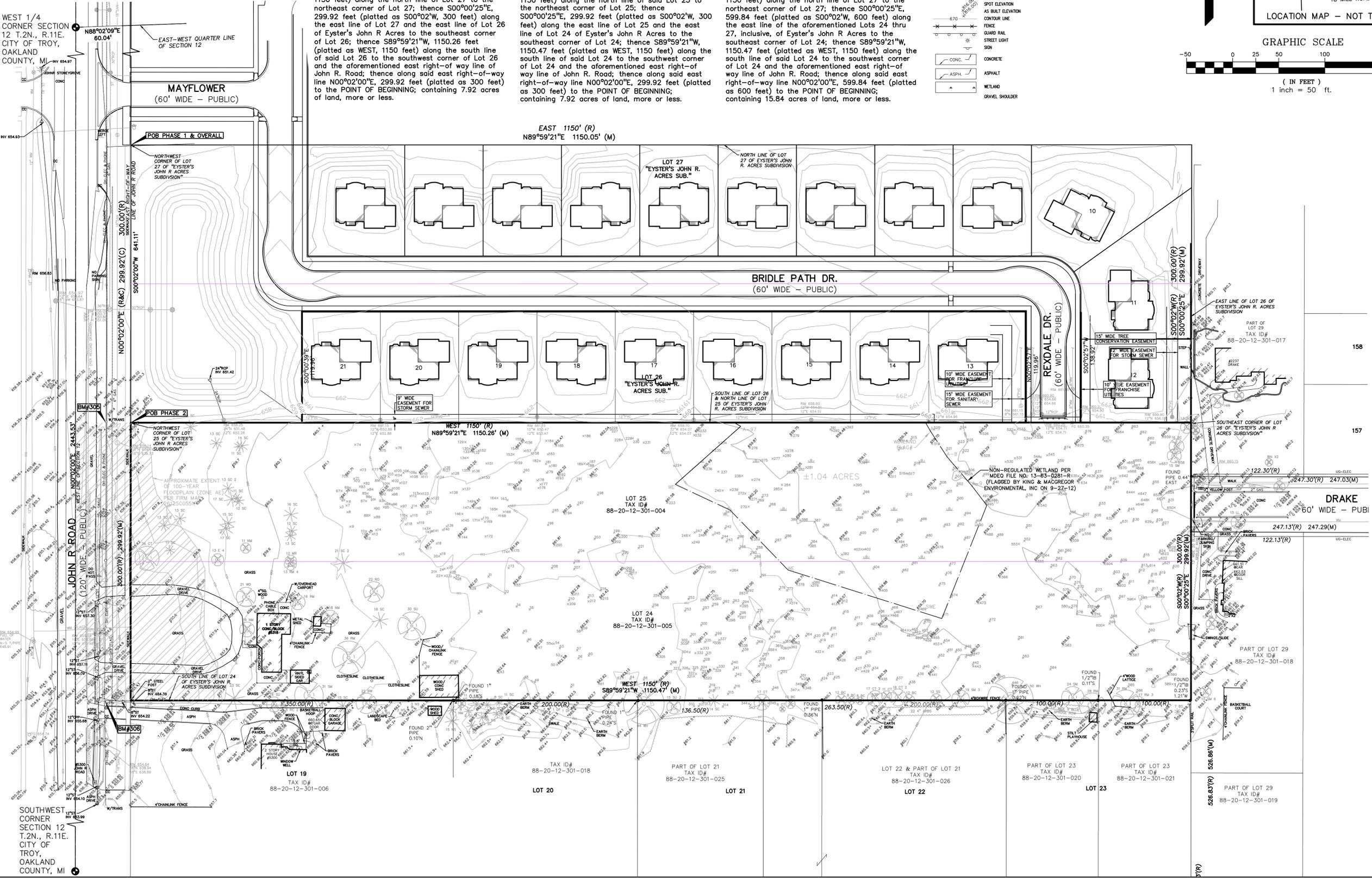
- IRON FOUND
- IRON SET
- ⊗ NAIL FOUND
- ⊗ NAIL & CAP SET
- ⊗ BRASS PLUG SET
- ⊗ MONUMENT FOUND
- ⊗ MONUMENT SET
- ⊗ SEC. CORNER FOUND
- ⊗ RECORDED
- ⊗ MEASURED
- ⊗ CALCULATED

EXISTING

- OH-ELEC—V—V— ELEC. PHONE OR CABLE TV OH. LINE, POLE & GUY WIRE
- UG-CATV— CATV UNDERGROUND CABLE TV, CATV PRESTAL
- UG-TEL— TEL. U.G. CABLE, PEDESTAL & MANHOLE
- UG-ELEC— ELEC. U.G. CABLE, MANHOLE, METER & MANDOLE
- GAS— GAS MAIN, VALVE & GAS LINE MARKER
- WATER— WATER MAIN, HYD. GATE VALVE, TAPPING SLEEVE & VALVE
- SEWER— SANITARY SEWER, CLEANOUT & MANHOLE
- STORM— STORM SEWER, CLEANOUT & MANHOLE
- COMB— COMBINED SEWER & MANHOLE
- CATCH— CATCH BASIN
- INLET— INLET (NO INCLUDING LINES)
- YARD— YARD DRAIN (2" DIA. & SMALLER)
- POST— POST INDICATOR VALVE
- WATER— WATER VALVE, 80#/MINUT VALVE BOX, SERVICE SHUTOFF
- MIXED— MIXED, TRANSFORMER, BRIGATOR CONTROL VALVE
- UNDEF— UNDEFINED STRUCTURE
- SPOT— SPOT ELEVATION
- AS BUILT— AS BUILT ELEVATION
- CONTOUR— CONTOUR LINE
- GUARD— GUARD RAIL
- STREET— STREET LIGHT
- SIGN— SIGN
- CONC— CONCRETE
- ASPH— ASPHALT
- WET— WETLAND
- GRAV— GRAVEL SHOULDER



NO.	BY	CHK	DESCRIPTION	DATE



CAUTION!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO WARRANTY IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING FOR DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE LOANED TO YOU FOR YOUR USE ONLY AND ARE NOT TO BE REPRODUCED, COPIED, OR USED IN ANY MANNER WITHOUT THE WRITTEN CONSENT OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY RESERVED TO PROFESSIONAL ENGINEERING ASSOCIATES, INC.

CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR SHALL BE REQUIRED TO ASSUME SOLE RESPONSIBILITY FOR THE SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL BE MADE TO APPLY TO ALL CONTRACTORS AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL ENGINEER FROM ANY AND ALL LIABILITY, LOSS, OR DAMAGES OF WORK ON THIS PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL ENGINEER.

3 FULL WORKING DAYS BEFORE YOU DIG CALL 811

Know what's below
Call before you dig

MISS DIG System, Inc.
1-800-482-7171 www.missdig.net

PEA

PROFESSIONAL ENGINEERING ASSOCIATES
2430 Rochester Ct. Suite 100
Troy, MI 48063-1872
Phone: (248) 689-9090
Fax: (248) 689-1044
website: www.peainc.com

MONDRIAN PROPERTIES HUNTERS PARK LLC
50216 SCHOENHERR
SHELBY TWP., MI 48315

TOPOGRAPHIC SURVEY
HUNTERS PARK 2
PART OF THE SW 1/4 OF SECTION 12, T. 2N., R. 11E., CITY OF TROY, OAKLAND COUNTY, MICHIGAN

DES: TMK DN TMK SUR DEC P.M.
JBT

ORIGINAL ISSUE DATE: MARCH 4, 2015
PEA JOB NO. 2014-249
SCALE: 1" = 50'
DRAWING NUMBER:
P-2

WEST 1/4 CORNER SECTION 12 T.2N., R.11E. CITY OF TROY, OAKLAND COUNTY, MI

MAYFLOWER (60' WIDE - PUBLIC)

POB PHASE 1 & OVERALL

NORTHWEST CORNER OF LOT 27 OF EYSTER'S JOHN R. ACRES SUBDIVISION

NOTE: DEVELOPER CURRENTLY IN PROCESS OF SUBMITTING APPLICATION FOR LETTER OF MAP AMENDMENT (LOMA) TO FEMA BASED ON CURRENT TOPOGRAPHICAL SURVEY DATA.

BRIDLE PATH DR. (60' WIDE - PUBLIC)

PROPOSED KINGSTON DR. (60' WIDE - PUBLIC)

REXDALE DR. (60' WIDE - PUBLIC)

PARK

DRAKE (60' WIDE - PUBLIC)

DRAKE (60' WIDE - PUBLIC)

SOUTHWEST CORNER SECTION 12 T.2N., R.11E. CITY OF TROY, OAKLAND COUNTY, MI

LEGEND

● IRON FOUND	⊗ BRASS PLUG SET	⊙ SEC. CORNER FOUND
⊗ IRON SET	⊙ MONUMENT FOUND	⊙ RECORDED
⊗ NAIL FOUND	⊙ MONUMENT SET	⊙ MEASURED
⊗ NAIL & CAP SET		⊙ CALCULATED

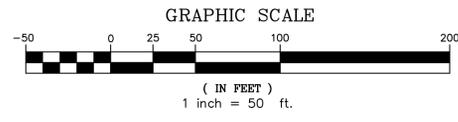
EXISTING

- OH-ELEC-: ELEC. PHONE OR CABLE TV OH. LINE, POLE & GUY WIRE
- UG-CATV-: UNDERGROUND CABLE TV, CATV PEDESTAL
- UG-PHONE-: TELEPHONE U.G. CABLE, PEDESTAL & MANHOLE
- UG-ELEC-: ELECTRIC U.G. CABLE, MANHOLE, METER & HANDHOLE
- GAS-: GAS MAIN, VALVE & GAS LINE MARKER
- WATER-: WATERMAIN, HYD. GATE VALVE, TAPPING SLEEVE & VALVE
- SANITARY-: SANITARY SEWER, CLEANOUT & MANHOLE
- STORM-: STORM SEWER, CLEANOUT & MANHOLE
- COMBINED-: COMBINED SEWER & MANHOLE
- CATCH BASIN
- INLET (NO INCOMING LINES)
- YARD DRAIN (2" DIA. & SMALLER)
- POST INDICATOR VALVE
- WATER VALVE, BSA/HYDRANT VALVE BOX, SERVICE SHUTOFF
- MALBOX, TRANSFORMER, BRIGATION CONTROL VALVE
- UNIDENTIFIED STRUCTURE

PROPOSED

- SPOT ELEVATION
- AS BUILT ELEVATION
- CONTOUR LINE
- FENCE
- GUARD RAIL
- STREET LIGHT
- SIGN
- CONCRETE
- ASPHALT
- WETLAND
- GRAVEL SHOULDER

Graphic Scale: 1 inch = 50 ft.



NO.	BY	CHK	DESCRIPTION	DATE

CAUTION!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT UTILITY LOCATIONS AND DEPTHS PRIOR TO THE START OF CONSTRUCTION.

THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE LOANED TO YOU FOR YOUR PROJECT ONLY AND NOT TO BE REPRODUCED, COPIED, OR USED IN ANY MANNER, OR USED FOR ANY OTHER PROJECT WITHOUT THE WRITTEN CONSENT OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY RESERVED TO PROFESSIONAL ENGINEERING ASSOCIATES, INC.

CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR SHALL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL LIABILITY FROM ANY AND ALL LIABILITY, FEES, OR DAMAGES OF WORK ON THIS PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL.

3 FULL WORKING DAYS BEFORE YOU DIG CALL 811

Know what's below
Call before you dig

MISS DIG System, Inc.
1-800-482-7171 www.missdig.net

PEA

PROFESSIONAL ENGINEERING ASSOCIATES
2430 Rochester Ct. Suite 100
Troy, MI 48063-1872
Phone: (248) 689-9090
Fax: (248) 689-1044
website: www.peainc.com

MONDRIAN PROPERTIES HUNTERS PARK LLC
50216 SCHOENHERR SHELBY TWP., MI 48315

PRELIMINARY GRADING PLAN HUNTERS PARK 2
PART OF THE SW 1/4 OF SECTION 12, T. 2N., R. 11E. CITY OF TROY, OAKLAND COUNTY, MICHIGAN

DES.	TMK	DN	TMK	SUR	DEC.	P.M.	JBT

ORIGINAL ISSUE DATE: MARCH 4, 2015
PEA JOB NO. 2014-249
SCALE: 1" = 50'
DRAWING NUMBER:
P-4

WEST 1/4
CORNER SECTION
12 T.2N., R.11E.
CITY OF TROY,
OAKLAND
COUNTY, MI

SOUTHWEST
CORNER
SECTION 12
T.2N., R.11E.
CITY OF
TROY,
OAKLAND
COUNTY, MI

MAYFLOWER
(60' WIDE - PUBLIC)

BRIDLE PATH DR.
(60' WIDE - PUBLIC)

REXDALE DR.
(60' WIDE - PUBLIC)

PARK

DRAKE
(60' WIDE - PUBLIC)

DRAKE
60' WIDE - PUBLI

PROPOSED KINGSTON DR.
(60' WIDE - PUBLIC)

PHASE 1 DETENTION AREA
BASIN SIZED TO
ACCOMMODATE
PHASE 1 AND PHASE 2

PHASE 1 REAR YARD
STORM SIZED TO PHASE 2

10' WIDE EASEMENT
FOR FRANCHISE
UTILITIES

15' WIDE TREE
CONSERVATION EASEMENT

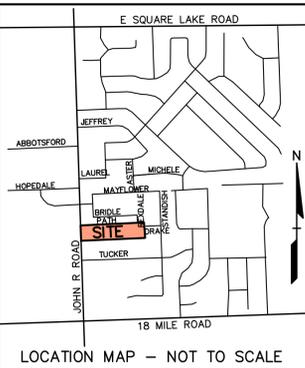
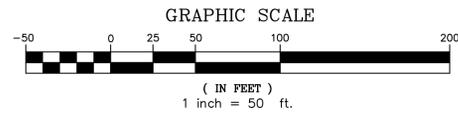
10' WIDE EASEMENT
FOR STORM SEWER

10' WIDE EASEMENT
FOR FRANCHISE
UTILITIES

LEGEND

● IRON FOUND	⊗ BRASS PLUG SET	⊙ SEC. CORNER FOUND
⊗ IRON SET	⊙ MONUMENT FOUND	⊗ RECORDED
⊙ NAIL FOUND	⊗ MONUMENT SET	⊗ MEASURED
⊗ NAIL & CAP SET		⊗ CALCULATED

—OH-ELEC—	ELEC. PHONE OR CABLE TV OH. LINE, POLE & GUY WIRE
—UG-CATV—	UNDERGROUND CABLE TV, CATV PEDESTAL
—UG-PHON—	TELEPHONE U.G. CABLE, PEDESTAL & MANHOLE
—UG-ELEC—	ELECTRIC U.G. CABLE, MANHOLE, METER & HANDHOLE
—GAS—	GAS MAIN, VALVE & GAS LINE MARKER
—WATER—	WATERMAIN, HYD. GATE VALVE, TAPPING SLEEVE & VALVE
—SANITARY—	SANITARY SEWER, CLEANOUT & MANHOLE
—STORM—	STORM SEWER, CLEANOUT & MANHOLE
—COMBINED—	COMBINED SEWER & MANHOLE
—CATCH—	CATCH BASIN
—INLET—	INLET (NO INCOMING LINES)
—YARD—	YARD DRAIN (2" DIA. & SMALLER)
—POST—	POST INDICATOR VALVE
—WATER—	WATER VALVE, BSA/HYDRANT VALVE BOX, SERVICE SHUTOFF
—MTRX—	MALTRX, TRANSFORMER, BRIGATOR CONTROL VALVE
—UNID—	UNIDENTIFIED STRUCTURE
—SPOT—	SPOT ELEVATION
—AS BUILT—	AS BUILT ELEVATION
—CONTOUR—	CONTOUR LINE
—FENCE—	FENCE
—RAIL—	GUARD RAIL
—STREET—	STREET LIGHT
—SIGN—	SIGN
—CONC.—	CONCRETE
—ASPH.—	ASPHALT
—WETLAND—	WETLAND
—GRAVEL—	GRAVEL SHOULDER



NO.	BY	CHK	DESCRIPTION	DATE

CAUTION!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE CORRECTNESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING FOR DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE LOANED TO YOU FOR YOUR USE ONLY AND ARE NOT TO BE REPRODUCED, COPIED, OR USED IN ANY MANNER WITHOUT THE WRITTEN CONSENT OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY RESERVED BY PROFESSIONAL ENGINEERING ASSOCIATES, INC.

CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR SHALL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR ALL CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS AND CONSTRUCTION CONTRACTOR PARTIES AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL PARTIES FROM ANY AND ALL LIABILITY, LOSS OR ALLEGES IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL.

3 FULL WORKING DAYS BEFORE YOU DIG CALL 811

Know what's below
Call before you dig
MISS DIG System, Inc.
1-800-482-7171 www.missdig.net

PEA

PROFESSIONAL ENGINEERING ASSOCIATES
2430 Rochester Ct. Suite 100
Troy, MI 48063-1872
Phone: (248) 689-9090
Fax: (248) 689-1044
website: www.peainc.com

MONDRIAN PROPERTIES HUNTERS PARK LLC
50216 SCHOENHERR
SHELBY TWP., MI 48315

**PRELIMINARY UTILITY PLAN
HUNTERS PARK 2**
PART OF THE SW 1/4 OF SECTION 12, T. 2N, R. 11E,
CITY OF TROY, OAKLAND COUNTY, MICHIGAN

DES.	TMK	DN	TMK	SUJR	DEC	P.M.

ST:\PROJECTS\2014\249\249-0000\HUNTERS PARK 2 - 811\Drawings\Title\01-17-14-000.dwg

ORIGINAL
ISSUE DATE: MARCH 4, 2015

PEA JOB NO. 2014-249

SCALE: 1" = 50'

DRAWING NUMBER:
P-5

WEST 1/4
CORNER SECTION
12 T.2N., R.11E.
CITY OF TROY,
OAKLAND
COUNTY, MI

MAYFLOWER
(60' WIDE - PUBLIC)

BRIDLE PATH DR.
(60' WIDE - PUBLIC)

REXDALE DR.
(60' WIDE - PUBLIC)

JOHN R. ROAD
(120' WIDE - PUBLIC)

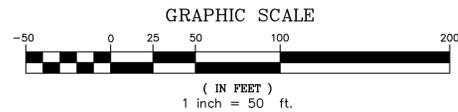
DRAKE
60' WIDE - PUBLIC

SOUTHWEST
CORNER SECTION 12
T.2N., R.11E.
CITY OF
TROY,
OAKLAND
COUNTY, MI

— AREA TO BE CLEARCUT

NOTE: SOME AREAS SUBJECT TO
SELECTIVE CLEARING BY BUILDERS.

- LEGEND**
- IRON FOUND
 - ⊗ IRON SET
 - NAIL FOUND
 - ⊗ NAIL & CAP SET
 - ⊗ BRASS PLUG SET
 - ⊗ MONUMENT FOUND
 - ⊗ MONUMENT SET
 - SEC. CORNER FOUND
 - ⊗ RECORDED
 - ⊗ MEASURED
 - CALCULATED
- EXISTING**
- OH-ELEC- Δ - Δ - Δ - ELEC. PHONE OR CABLE TV OH. LINE, POLE & GUY WIRE
 - UG-CATV- \square - \square - UNDERGROUND CABLE TV, CATV PEDESTAL
 - UG-PHONE- \square - \square - TELEPHONE U.G. CABLE, PEDESTAL & MANHOLE
 - UG-ELEC- \square - \square - \square - ELECTRIC U.G. CABLE, MANHOLE, METER & HANDHOLE
 - \square - \square - GAS MAIN, VALVE & GAS LINE MARKER
 - \square - \square - WATERMAIN, HYD. GATE VALVE, TAPPING SLEEVE & VALVE
 - \square - \square - SANITARY SEWER, CLEANOUT & MANHOLE
 - \square - \square - STORM SEWER, CLEANOUT & MANHOLE
 - \square - \square - COMBINED SEWER & MANHOLE
 - CATCH BASIN
 - INLET (NO INCOMING LINES)
 - YARD DRAIN (2" DIA. & SMALLER)
 - POST INDICATOR VALVE
 - WATER VALVE, BSA/HYDRANT VALVE BOX, SERVICE SHUTOFF
 - MALBOX, TRANSFORMER, BRIGATOR CONTROL VALVE
 - UNIDENTIFIED STRUCTURE
 - SPOT ELEVATION
 - AS BUILT ELEVATION
 - CONTOUR LINE
 - FENCE
 - GUARD RAIL
 - STREET LIGHT
 - SIGN
- CONCRETE**
- ASPHALT**
- WETLAND**
- GRAVEL SHOULDER**



NO.	BY	CHK	DESCRIPTION	DATE

CAUTION!
THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE EXCLUSIVELY RESPONSIBLE FOR DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE LOANED TO YOU FOR THE PROJECT ONLY AND ARE NOT TO BE REPRODUCED, COPIED, OR USED IN ANY MANNER OR BY ANY PERSON OR ENTITY WITHOUT THE WRITTEN CONSENT OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY RESERVED. © 2015 PROFESSIONAL ENGINEERING ASSOCIATES, INC.

CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR SHALL BE REQUIRED TO ASSUME FULL AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT. NO LIABILITY OF ANY PERSON AND PROPERTY THAT THIS REQUIREMENT SHALL BE MADE TO APPLY TO ANY CONTRACTOR WHO IS NOT LIMITED TO NORMAL WORKING HOURS AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND.

INDUSTRY AND HOLD DESIGN PROFESSIONAL LIABILITY FROM ANY AND ALL LIABILITY ARISING OR WORK ON THIS PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL.

3 FULL WORKING DAYS BEFORE YOU DIG CALL 811

Know what's below
Call before you dig
MISS DIG System, Inc.
1-800-482-7171 www.missdig.net



PROFESSIONAL ENGINEERING ASSOCIATES
2430 Rochester Ct. Suite 100
Troy, MI 48063-1872
Phone: (248) 689-9090
Fax: (248) 689-1044
website: www.peainc.com

MONDRIAN PROPERTIES HUNTERS PARK LLC
50216 SCHOENHERR
SHELBY TWP., MI 48315

PRELIMINARY TREE PRESERVATION PLAN
HUNTERS PARK 2
PART OF THE SW 1/4 OF SECTION 12, T. 2N., R. 11E.
CITY OF TROY, OAKLAND COUNTY, MICHIGAN

DES.	TMK	DN	TMK	DN	SUR.	DEC.	P.M.

PROJECT: 2014-249 (REV. 03-2015)

ORIGINAL
ISSUE DATE: MARCH 4, 2015

PEA JOB NO. 2014-249

SCALE: 1" = 50'

DRAWING NUMBER:
P-6

TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT	TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT	TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT	TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT	TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT				
1	SC	10	Scotch Pine	Pinus sylvestris	FAIR		100	SC	6	Scotch Pine	Pinus sylvestris	POOR		199	E	4	American Elm	Ulmus americana	FAIR		298	WO	5	White Oak	Quercus alba	GOOD												
2	SC	10	Scotch Pine	Pinus sylvestris	FAIR		101	SC	8	Scotch Pine	Pinus sylvestris	FAIR		200	BC	9	Wild Black Cherry	Prunus serotina	FAIR		299	BC	4	Wild Black Cherry	Prunus serotina	GOOD												
3	SC	6	Scotch Pine	Pinus sylvestris	GOOD		102	SC	6	Scotch Pine	Pinus sylvestris	FAIR		201	LOST TAG						300	BC	6	Wild Black Cherry	Prunus serotina	GOOD												
4	E	7	American Elm	Ulmus americana	GOOD		103	SC	8	Scotch Pine	Pinus sylvestris	FAIR		202	BC	5	Wild Black Cherry	Prunus serotina	POOR		301	BC	4	Wild Black Cherry	Prunus serotina	GOOD												
5	SC	10	Scotch Pine	Pinus sylvestris	POOR		104	SC	8	Scotch Pine	Pinus sylvestris	FAIR		203	E	4	American Elm	Ulmus americana	GOOD		302	SC	6	Scotch Pine	Pinus sylvestris	FAIR												
6	SC	5	Scotch Pine	Pinus sylvestris	POOR		105	SC	9	Scotch Pine	Pinus sylvestris	GOOD		204	E	6	American Elm	Ulmus americana	FAIR		303	BC	5	Wild Black Cherry	Prunus serotina	GOOD												
7	SC	8	Scotch Pine	Pinus sylvestris	FAIR		106	SC	9	Scotch Pine	Pinus sylvestris	FAIR		205	E	6	American Elm	Ulmus americana	FAIR		304	SC	5	Scotch Pine	Pinus sylvestris	FAIR												
8	SC	8	Scotch Pine	Pinus sylvestris	FAIR		107	SC	6	Scotch Pine	Pinus sylvestris	FAIR	X2	206	WO	4	White Oak	Quercus alba	FAIR		305	SC	7	Scotch Pine	Pinus sylvestris	GOOD												
9	SC	6	Scotch Pine	Pinus sylvestris	FAIR		108	SC	7	Scotch Pine	Pinus sylvestris	FAIR		207	SM	4	Silver Maple	Acer saccharinum	FAIR	X2	306	MR	6	Red Mulberry	Morus rubra	FAIR									X3			
10	SC	7	Scotch Pine	Pinus sylvestris	GOOD		109	SC	5	Scotch Pine	Pinus sylvestris	POOR		208	BC	10	Wild Black Cherry	Prunus serotina	GOOD		307	WO	7	White Oak	Quercus alba	GOOD												
11	SC	10	Scotch Pine	Pinus sylvestris	FAIR		110	SC	8	Scotch Pine	Pinus sylvestris	FAIR	X2	209	SC	5	Scotch Pine	Pinus sylvestris	POOR	X2	308	BC	7	Wild Black Cherry	Prunus serotina	GOOD												
12	SC	10	Scotch Pine	Pinus sylvestris	GOOD		111	SC	5	Scotch Pine	Pinus sylvestris	FAIR		210	SC	5	Scotch Pine	Pinus sylvestris	FAIR		309	SC	6	Scotch Pine	Pinus sylvestris	GOOD												
13	SC	10	Scotch Pine	Pinus sylvestris	GOOD		112	E	8	American Elm	Ulmus americana	GOOD		211	SC	6	Scotch Pine	Pinus sylvestris	FAIR		310	SC	4	Scotch Pine	Pinus sylvestris	POOR												
14	SC	9	Scotch Pine	Pinus sylvestris	FAIR		113	SC	10	Scotch Pine	Pinus sylvestris	FAIR		212	NM	5	Norway Maple	Acer platanoides	FAIR		311	SC	6	Scotch Pine	Pinus sylvestris	GOOD												
15	SC	7	Scotch Pine	Pinus sylvestris	GOOD		114	SC	9	Scotch Pine	Pinus sylvestris	FAIR		213	SC	8	Scotch Pine	Pinus sylvestris	POOR		312	SC	5	Scotch Pine	Pinus sylvestris	FAIR												
16	SC	9	Scotch Pine	Pinus sylvestris	FAIR		115	SC	5	Scotch Pine	Pinus sylvestris	POOR		214	E	4	American Elm	Ulmus americana	GOOD		313	E	5	American Elm	Ulmus americana	GOOD												
17	SC	9	Scotch Pine	Pinus sylvestris	FAIR	X2	116	SC	7	Scotch Pine	Pinus sylvestris	FAIR	X2	215	E	8	American Elm	Ulmus americana	GOOD		314	PN	5	Pin Cherry	Prunus pennsylvanica	GOOD												
18	SC	6	Scotch Pine	Pinus sylvestris	FAIR		117	SC	6	Scotch Pine	Pinus sylvestris	POOR		216	E	9	American Elm	Ulmus americana	GOOD		315	SM	5	Silver Maple	Acer saccharinum	GOOD											X2	
19	SC	8	Scotch Pine	Pinus sylvestris	POOR		118	SC	8	Scotch Pine	Pinus sylvestris	FAIR	X2	217	BC	9	Wild Black Cherry	Prunus serotina	FAIR		316	SM	5	Silver Maple	Acer saccharinum	GOOD											X2	
20	SC	8	Scotch Pine	Pinus sylvestris	POOR		119	NM	4	Norway Maple	Acer platanoides	GOOD		218	BC	7	Wild Black Cherry	Prunus serotina	POOR		317	SC	5	Scotch Pine	Pinus sylvestris	POOR												
21	SC	10	Scotch Pine	Pinus sylvestris	GOOD		120	SC	7	Scotch Pine	Pinus sylvestris	POOR		219	BC	9	Wild Black Cherry	Prunus serotina	GOOD		318	SC	7	Scotch Pine	Pinus sylvestris	FAIR												
22	SC	9	Scotch Pine	Pinus sylvestris	FAIR		121	SC	7	Scotch Pine	Pinus sylvestris	FAIR	X2	220	SM	10	Silver Maple	Acer saccharinum	GOOD		319	SC	10	Scotch Pine	Pinus sylvestris	FAIR												
23	SC	10	Scotch Pine	Pinus sylvestris	FAIR		122	E	8	American Elm	Ulmus americana	GOOD		221	BC	9	Wild Black Cherry	Prunus serotina	POOR		320	BC	7	Wild Black Cherry	Prunus serotina	GOOD												
24	NM	5	Norway Maple	Acer platanoides	GOOD		123	SC	10	Scotch Pine	Pinus sylvestris	FAIR		222	PN	6	Pin Cherry	Prunus pennsylvanica	GOOD		321	SC	5	Scotch Pine	Pinus sylvestris	POOR												
25	SC	9	Scotch Pine	Pinus sylvestris	FAIR		124	SC	8	Scotch Pine	Pinus sylvestris	POOR		223	E	7	American Elm	Ulmus americana	FAIR		322	SC	7	Scotch Pine	Pinus sylvestris	FAIR												
26	SC	8	Scotch Pine	Pinus sylvestris	FAIR		125	SM	5	Silver Maple	Acer saccharinum	GOOD		224	E	4	American Elm	Ulmus americana	GOOD		323	SC	5	Scotch Pine	Pinus sylvestris	POOR												
27	NM	4	Norway Maple	Acer platanoides	GOOD		126	SC	8	Scotch Pine	Pinus sylvestris	POOR		225	EE	9	Siberian Elm	Ulmus pumila	POOR		324	SC	6	Scotch Pine	Pinus sylvestris	FAIR												
28	NM	4	Norway Maple	Acer platanoides	FAIR		127	SC	8	Scotch Pine	Pinus sylvestris	FAIR		226	WO	6	White Oak	Quercus alba	FAIR		325	SC	7	Scotch Pine	Pinus sylvestris	FAIR												
29	BC	5	Wild Black Cherry	Prunus serotina	GOOD		128	SC	9	Scotch Pine	Pinus sylvestris	FAIR		227	E	4	American Elm	Ulmus americana	FAIR		326	SC	10	Scotch Pine	Pinus sylvestris	GOOD												
30	SC	8	Scotch Pine	Pinus sylvestris	POOR		129	SM	5	Silver Maple	Acer saccharinum	GOOD		228	BC	4	Wild Black Cherry	Prunus serotina	FAIR		327	SC	5	Scotch Pine	Pinus sylvestris	FAIR												
31	BC	6	Wild Black Cherry	Prunus serotina	GOOD		130	SC	10	Scotch Pine	Pinus sylvestris	FAIR		229	E	4	American Elm	Ulmus americana	GOOD		328	SC	7	Scotch Pine	Pinus sylvestris	FAIR												
32	SC	10	Scotch Pine	Pinus sylvestris	POOR		131	SC	5	Scotch Pine	Pinus sylvestris	POOR		230	BC	4	Wild Black Cherry	Prunus serotina	FAIR		329	SC	8	Scotch Pine	Pinus sylvestris	POOR												
33	WO	6	White Oak	Quercus alba	GOOD		132	SC	10	Scotch Pine	Pinus sylvestris	FAIR		231	E	5	American Elm	Ulmus americana	GOOD		330	SC	8	Scotch Pine	Pinus sylvestris	FAIR												
34	GA	9	Green Ash	Fraxinus pennsylvanica	GOOD		133	SC	9	Scotch Pine	Pinus sylvestris	FAIR		232	E	8	American Elm	Ulmus americana	GOOD		331	SC	6	Scotch Pine	Pinus sylvestris	FAIR												
35	E	4	American Elm	Ulmus americana	GOOD		134	SC	8	Scotch Pine	Pinus sylvestris	POOR	X2	233	E	7	American Elm	Ulmus americana	GOOD		332	SC	8	Scotch Pine	Pinus sylvestris	GOOD												
36	BC	9	Wild Black Cherry	Prunus serotina	GOOD		135	SC	10	Scotch Pine	Pinus sylvestris	FAIR		234	F	5	American Elm	Ulmus americana	GOOD		333	SC	6	Scotch Pine	Pinus sylvestris	FAIR												
37	SC	6	Scotch Pine	Pinus sylvestris	POOR		136	SC	8	Scotch Pine	Pinus sylvestris	FAIR		235	SM	4	Silver Maple	Acer saccharinum	POOR		334	SC	5	Scotch Pine	Pinus sylvestris	FAIR												
38	GA	9	Green Ash	Fraxinus pennsylvanica	GOOD		137	SC	8	Scotch Pine	Pinus sylvestris	FAIR		236	F	8	American Elm	Ulmus americana	FAIR		335	SC	5	Scotch Pine	Pinus sylvestris	FAIR												
39	E	6	American Elm	Ulmus americana	GOOD		138	SC	7	Scotch Pine	Pinus sylvestris	FAIR	X2	237	E	7	American Elm	Ulmus americana	GOOD		336	SC	5	Scotch Pine	Pinus sylvestris	POOR												
40	BC	5	Wild Black Cherry	Prunus serotina	GOOD		139	BC	5	Wild Black Cherry	Prunus serotina	GOOD		238	E	6	American Elm	Ulmus americana	FAIR	X2	337	SC	10	Scotch Pine	Pinus sylvestris	FAIR												
41	RO	4	Red Oak	Quercus rubra	GOOD		140	SC	8	Scotch Pine	Pinus sylvestris	FAIR		239	SM	6	Silver Maple	Acer saccharinum	GOOD		338	BC	7	Wild Black Cherry	Prunus serotina	GOOD												
42	SC	8	Scotch Pine	Pinus sylvestris	GOOD		141	SC	7	Scotch Pine	Pinus sylvestris	FAIR	X2	240	E	6	American Elm	Ulmus americana	POOR		339	SC	9	Scotch Pine	Pinus sylvestris	FAIR												
43	GA	9	Green Ash	Fraxinus pennsylvanica	FAIR		142	NM	4	Norway Maple	Acer platanoides	GOOD	X2	241	BC	9	Wild Black Cherry	Prunus serotina	POOR	X3	340	BC	6	Wild Black Cherry	Prunus serotina	GOOD												
44	BC	8	Wild Black Cherry	Prunus serotina	GOOD		143	SC	7	Scotch Pine	Pinus sylvestris	FAIR		242	WO	4	White Oak	Quercus alba	FAIR		341	BC	7	Wild Black Cherry	Prunus serotina	POOR												
45	BC	6	Wild Black Cherry	Prunus serotina	GOOD		144	BC	4	Wild Black Cherry	Prunus serotina	GOOD		243	WO	5	White Oak	Quercus alba	GOOD		342	SC	10	Scotch Pine	Pinus sylvestris	FAIR												
46	BC	4	Wild Black Cherry	Prunus serotina	GOOD		145	SC	9	Scotch Pine	Pinus sylvestris	FAIR		244	CT	6	Cottonwood	Populus deltoides	GOOD		343	BC	8	Wild Black Cherry	Prunus serotina	GOOD												
47	WO	6	White Oak	Quercus alba	GOOD		146	SC	8	Scotch Pine	Pinus sylvestris	FAIR		245	CT	9	Cottonwood	Populus deltoides	GOOD		344	SC	8	Scotch Pine	Pinus sylvestris	FAIR												
48	CA	6	Crab Apple	Malus coronaria	FAIR	X2	147	SC	6	Scotch Pine	Pinus sylvestris	FAIR		246	CT	6	Cottonwood	Populus deltoides	GOOD		345	BC	7	Wild Black Cherry	Prunus serotina	GOOD												
49	CA	9	Crab Apple	Malus coronaria	GOOD	X3	148	SC	7	Scotch Pine	Pinus sylvestris	FAIR		247	CT	8	Cottonwood	Populus deltoides	GOOD		346	BC	10	Wild Black Cherry	Prunus serotina	GOOD												
50	E	8	American Elm	Ulmus americana	GOOD		149	SC	7	Scotch Pine	Pinus sylvestris	FAIR		248	PN	6	Pin Cherry	Prunus pennsylvanica	GOOD		347	PB	6	Paper Birch	Betula papyrifera	GOOD												
51	BC	9	Wild Black Cherry	Prunus serotina	GOOD		150	SC	10	Scotch Pine	Pinus sylvestris	FAIR		249	CT	8	Cottonwood	Populus deltoides	GOOD		348	SM	4	Silver Maple	Acer saccharinum	GOOD												
52	BC	6	Wild Black Cherry	Prunus serotina																																		

TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT	TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT
397	RM	8	Red Maple	Acer rubrum	FAIR		496	RO	4	Red Oak	Quercus rubra	FAIR	
398	SM	8	Silver Maple	Acer saccharinum	POOR		497	SM	7	Silver Maple	Acer saccharinum	FAIR	
399	E	4	American Elm	Ulmus americana	GOOD		498	BC	4	Wild Black Cherry	Prunus serotina	FAIR	
400	SM	8	Silver Maple	Acer saccharinum	POOR	X2	499	SM	9	Silver Maple	Acer saccharinum	GOOD	
401	E	4	American Elm	Ulmus americana	GOOD		500	SM	7	Silver Maple	Acer saccharinum	GOOD	X2
402	E	4	American Elm	Ulmus americana	POOR		501	E	4	American Elm	Ulmus americana	GOOD	
403	E	5	American Elm	Ulmus americana	POOR		502	SM	10	Silver Maple	Acer saccharinum	GOOD	
404	SM	4	Silver Maple	Acer saccharinum	GOOD		503	SM	8	Silver Maple	Acer saccharinum	GOOD	
405	PB	9	Paper Birch	Betula papyrifera	GOOD		504	SM	5	Silver Maple	Acer saccharinum	GOOD	
406	PB	7	Paper Birch	Betula papyrifera	GOOD		505	SM	6	Silver Maple	Acer saccharinum	GOOD	
407	RM	8	Red Maple	Acer rubrum	GOOD		506	SM	4	Silver Maple	Acer saccharinum	GOOD	
408	SM	6	Silver Maple	Acer saccharinum	GOOD		507	SM	7	Silver Maple	Acer saccharinum	GOOD	X3
409	SS	6	Sassafras	Sassafras albidum	GOOD		508	SM	6	Silver Maple	Acer saccharinum	GOOD	
410	SC	10	Scotch Pine	Pinus sylvestris	FAIR		509	SM	6	Silver Maple	Acer saccharinum	GOOD	
411	SC	7	Scotch Pine	Pinus sylvestris	POOR		510	RO	6	Red Oak	Quercus rubra	POOR	
412	WO	6	White Oak	Quercus alba	POOR		511	E	5	American Elm	Ulmus americana	GOOD	
413	BC	6	Wild Black Cherry	Prunus serotina	FAIR		512	RO	5	Red Oak	Quercus rubra	GOOD	
414	CT	5	Cottonwood	Populus deltoides	GOOD		513	E	5	American Elm	Ulmus americana	GOOD	
415	BC	4	Wild Black Cherry	Prunus serotina	FAIR		514	SM	10	Silver Maple	Acer saccharinum	POOR	
416	SM	4	Silver Maple	Acer saccharinum	GOOD		515	E	5	American Elm	Ulmus americana	GOOD	
417	SM	5	Silver Maple	Acer saccharinum	GOOD		516	WO	8	White Oak	Quercus alba	GOOD	
418	PB	6	Paper Birch	Betula papyrifera	GOOD	X2	517	E	5	American Elm	Ulmus americana	FAIR	X3
419	PB	5	Paper Birch	Betula papyrifera	GOOD		518	E	4	American Elm	Ulmus americana	GOOD	
420	PB	4	Paper Birch	Betula papyrifera	GOOD	X2	519	SM	7	Silver Maple	Acer saccharinum	FAIR	
421	PB	6	Paper Birch	Betula papyrifera	GOOD		520	SM	7	Silver Maple	Acer saccharinum	GOOD	X4
422	PB	7	Paper Birch	Betula papyrifera	GOOD		521	SM	7	Silver Maple	Acer saccharinum	FAIR	
423	BC	5	Wild Black Cherry	Prunus serotina	GOOD		522	RO	8	Red Oak	Quercus rubra	FAIR	
424	SM	5	Silver Maple	Acer saccharinum	GOOD	X2	523	SM	5	Silver Maple	Acer saccharinum	GOOD	X2
425	BC	8	Wild Black Cherry	Prunus serotina	FAIR		524	SM	9	Silver Maple	Acer saccharinum	GOOD	X2
426	BC	8	Wild Black Cherry	Prunus serotina	GOOD		525	SM	5	Silver Maple	Acer saccharinum	GOOD	
427	BC	5	Wild Black Cherry	Prunus serotina	GOOD		526	SM	7	Silver Maple	Acer saccharinum	GOOD	X3
428	BC	5	Wild Black Cherry	Prunus serotina	GOOD		527	RO	8	Red Oak	Quercus rubra	GOOD	
429	BC	4	Wild Black Cherry	Prunus serotina	GOOD		528	SM	6	Silver Maple	Acer saccharinum	GOOD	
430	RO	4	Red Oak	Quercus rubra	GOOD		529	SM	6	Silver Maple	Acer saccharinum	GOOD	X3
431	BC	4	Wild Black Cherry	Prunus serotina	GOOD		530	SM	7	Silver Maple	Acer saccharinum	GOOD	
432	SM	4	Silver Maple	Acer saccharinum	GOOD		531	SM	6	Silver Maple	Acer saccharinum	POOR	
433	PB	6	Paper Birch	Betula papyrifera	GOOD	X2	532	RO	9	Red Oak	Quercus rubra	FAIR	X2
434	BC	4	Wild Black Cherry	Prunus serotina	FAIR		533	RO	6	Red Oak	Quercus rubra	GOOD	
435	BC	4	Wild Black Cherry	Prunus serotina	GOOD		534	PB	4	Paper Birch	Betula papyrifera	FAIR	
436	CT	5	Cottonwood	Populus deltoides	FAIR		535	PB	5	Paper Birch	Betula papyrifera	GOOD	
437	E	6	American Elm	Ulmus americana	POOR		536	PB	5	Paper Birch	Betula papyrifera	FAIR	
438	PB	4	Paper Birch	Betula papyrifera	GOOD		537	SM	9	Silver Maple	Acer saccharinum	GOOD	X2
439	PB	6	Paper Birch	Betula papyrifera	GOOD		538	SM	5	Silver Maple	Acer saccharinum	FAIR	X3
440	BC	9	Wild Black Cherry	Prunus serotina	FAIR		539	SM	7	Silver Maple	Acer saccharinum	GOOD	X2
441	PB	5	Paper Birch	Betula papyrifera	GOOD		540	BC	9	Wild Black Cherry	Prunus serotina	FAIR	
442	PB	4	Paper Birch	Betula papyrifera	FAIR		541	SM	6	Silver Maple	Acer saccharinum	GOOD	
443	SM	6	Silver Maple	Acer saccharinum	GOOD		542	SM	4	Silver Maple	Acer saccharinum	FAIR	
444	SM	8	Silver Maple	Acer saccharinum	GOOD		543	SM	4	Silver Maple	Acer saccharinum	FAIR	
445	SM	10	Silver Maple	Acer saccharinum	GOOD		544	SM	6	Silver Maple	Acer saccharinum	FAIR	
446	SM	5	Silver Maple	Acer saccharinum	GOOD		545	PB	6	Paper Birch	Betula papyrifera	FAIR	
447	RM	9	Red Maple	Acer rubrum	GOOD		546	BC	8	Wild Black Cherry	Prunus serotina	GOOD	
448	PN	4	Pin Cherry	Prunus pennsylvanica	GOOD		547	SM	5	Silver Maple	Acer saccharinum	FAIR	
449	SM	7	Silver Maple	Acer saccharinum	GOOD	X3	548	E	4	American Elm	Ulmus americana	FAIR	
450	SM	5	Silver Maple	Acer saccharinum	FAIR		549	E	5	American Elm	Ulmus americana	FAIR	
451	SM	9	Silver Maple	Acer saccharinum	GOOD		550	BC	7	Wild Black Cherry	Prunus serotina	FAIR	
452	BC	5	Wild Black Cherry	Prunus serotina	GOOD	X2	551	SM	8	Silver Maple	Acer saccharinum	FAIR	
453	PB	4	Paper Birch	Betula papyrifera	FAIR		552	E	7	American Elm	Ulmus americana	FAIR	
454	SM	7	Silver Maple	Acer saccharinum	GOOD	X2	553	SM	7	Silver Maple	Acer saccharinum	GOOD	X2
455	PB	5	Paper Birch	Betula papyrifera	FAIR		554	SM	10	Silver Maple	Acer saccharinum	GOOD	
456	BC	5	Wild Black Cherry	Prunus serotina	GOOD		555	SM	4	Silver Maple	Acer saccharinum	FAIR	
457	PB	6	Paper Birch	Betula papyrifera	GOOD		556	E	6	American Elm	Ulmus americana	FAIR	
458	BC	4	Wild Black Cherry	Prunus serotina	GOOD		557	RO	7	Red Oak	Quercus rubra	POOR	
459	BC	8	Wild Black Cherry	Prunus serotina	GOOD		558	SM	7	Silver Maple	Acer saccharinum	FAIR	
460	PB	6	Paper Birch	Betula papyrifera	POOR		559	E	8	American Elm	Ulmus americana	GOOD	
461	PB	6	Paper Birch	Betula papyrifera	GOOD		560	E	8	American Elm	Ulmus americana	GOOD	
462	PB	6	Paper Birch	Betula papyrifera	GOOD		561	E	10	American Elm	Ulmus americana	GOOD	
463	PB	5	Paper Birch	Betula papyrifera	GOOD		562	E	6	American Elm	Ulmus americana	POOR	
464	PB	6	Paper Birch	Betula papyrifera	FAIR		563	SS	4	Sassafras	Sassafras albidum	FAIR	
465	PB	4	Paper Birch	Betula papyrifera	GOOD		564	SM	6	Silver Maple	Acer saccharinum	GOOD	X2
466	PB	4	Paper Birch	Betula papyrifera	POOR		565	E	7	American Elm	Ulmus americana	FAIR	
467	PB	7	Paper Birch	Betula papyrifera	GOOD		566	SM	6	Silver Maple	Acer saccharinum	GOOD	X4
468	TH	5	Thornapple/Hawthorne	Cragagac spp.	POOR		567	WO	5	White Oak	Quercus alba	FAIR	
469	PN	4	Pin Cherry	Prunus pennsylvanica	FAIR		568	SM	4	Silver Maple	Acer saccharinum	GOOD	
470	BC	5	Wild Black Cherry	Prunus serotina	FAIR		569	PB	6	Paper Birch	Betula papyrifera	GOOD	X2
471	WO	4	White Oak	Quercus alba	GOOD		570	PB	4	Paper Birch	Betula papyrifera	POOR	
472	E	4	American Elm	Ulmus americana	GOOD		571	PB	4	Paper Birch	Betula papyrifera	FAIR	
473	SM	4	Silver Maple	Acer saccharinum	GOOD		572	PB	5	Paper Birch	Betula papyrifera	GOOD	
474	SM	7	Silver Maple	Acer saccharinum	GOOD		573	SM	4	Silver Maple	Acer saccharinum	GOOD	X2
475	SM	8	Silver Maple	Acer saccharinum	FAIR		574	PB	7	Paper Birch	Betula papyrifera	GOOD	
476	SM	5	Silver Maple	Acer saccharinum	GOOD		575	SM	7	Silver Maple	Acer saccharinum	GOOD	X2
477	E	8	American Elm	Ulmus americana	GOOD		576	SM	5	Silver Maple	Acer saccharinum	GOOD	
478	E	4	American Elm	Ulmus americana	GOOD		577	SM	8	Silver Maple	Acer saccharinum	GOOD	
479	E	9	American Elm	Ulmus americana	GOOD		578	SM	8	Silver Maple	Acer saccharinum	GOOD	
480	E	5	American Elm	Ulmus americana	GOOD		579	E	4	American Elm	Ulmus americana	FAIR	
481	SM	9	Silver Maple	Acer saccharinum	GOOD	X2	580	PB	5	Paper Birch	Betula papyrifera	GOOD	
482	SM	6	Silver Maple	Acer saccharinum	GOOD		581	E	5	American Elm	Ulmus americana	GOOD	
483	F	4	American Elm	Ulmus americana	GOOD		582	SM	5	Silver Maple	Acer saccharinum	GOOD	
484	RM	9	Red Maple	Acer rubrum	GOOD		583	SM	9	Silver Maple	Acer saccharinum	GOOD	
485	F	5	American Elm	Ulmus americana	GOOD		584	SM	7	Silver Maple	Acer saccharinum	GOOD	
486	E	5	American Elm	Ulmus americana	GOOD		585	E	8	American Elm	Ulmus americana	GOOD	
487	SM	5	Silver Maple	Acer saccharinum	FAIR	X2	586	BX	8	Box elder	Acer negundo	FAIR	
488	SM	9	Silver Maple	Acer saccharinum	GOOD	X2	587	SM	4	Silver Maple	Acer saccharinum	POOR	
489	E	4	American Elm	Ulmus americana	GOOD		588	SM	6	Silver Maple	Acer saccharinum	FAIR	
490	SM	7	Silver Maple	Acer saccharinum	FAIR		589	E	7	American Elm	Ulmus americana	FAIR	
491	SM	7	Silver Maple	Acer saccharinum	FAIR		590	SM	4	Silver Maple	Acer saccharinum	FAIR	
492	SM	8	Silver Maple	Acer saccharinum	GOOD		591	SM	7	Silver Maple	Acer saccharinum	FAIR	
493	E	6	American Elm	Ulmus americana	GOOD		592	SM	8	Silver Maple	Acer saccharinum	FAIR	
494	SM	7	Silver Maple	Acer saccharinum	POOR		593	BC	6	Wild Black Cherry	Prunus serotina	POOR	X2
495	BC	5	Wild Black Cherry	Prunus serotina	GOOD		594	SM	7	Silver Maple	Acer saccharinum	GOOD	X3

TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	COND	COMMENT
595	SM	4	Silver Maple	Acer saccharinum	GOOD	
596	PB	8	Paper Birch	Betula papyrifera	GOOD	
597	BC	8	Wild Black Cherry	Prunus serotina	GOOD	
598	PB	6	Paper Birch	Betula papyrifera	POOR	
599	BC	7	Wild Black Cherry	Prunus serotina	FAIR	
600	SM	8	Silver Maple	Acer saccharinum	GOOD	
601	SM	5	Silver Maple	Acer saccharinum	GOOD	X2
602	SM	7	Silver Maple	Acer saccharinum	GOOD	X2
603	PB	5	Paper Birch	Betula papyrifera	GOOD	
604	SM	10	Silver Maple	Acer saccharinum	GOOD	
605	E	5	American Elm	Ulmus americana	POOR	
606	RO	6	Red Oak	Quercus rubra	FAIR	
607	SM	5	Silver Maple	Acer saccharinum	FAIR	
608	RO	5	Red Oak	Quercus rubra	POOR	X2
609	SS	10	Sassafras	Sassafras albidum	GOOD	
610	SM	6	Silver Maple	Acer saccharinum	GOOD	
611	SM	8	Silver Maple	Acer saccharinum	GOOD	X3
612	SM	5	Silver Maple	Acer saccharinum	GOOD	
613	PB	4	Paper Birch	Betula papyrifera	GOOD	
614	PB	7	Paper Birch	Betula papyrifera	GOOD	
615	WO	5	White Oak	Quercus alba	GOOD	
616	SM	7	Silver Maple	Acer saccharinum	GOOD	
617	BC	5	Wild Black Cherry	Prunus serotina	FAIR	
618	SM	7	Silver Maple	Acer saccharinum	GOOD	
619	RO	6	Red Oak	Quercus rubra	POOR	
620	SS	4	Sassafras	Sassafras albidum	POOR	
621	SS	8	Sassafras	Sassafras albidum	POOR	
622	BC	9	Wild Black Cherry	Prunus serotina	GOOD	
623	CT	10	Cottonwood	Populus deltoides	GOOD	
624	SM	7	Silver Maple	Acer saccharinum	GOOD	
625	WO	6	White Oak	Quercus alba	FAIR	
626	SM	8	Silver Maple	Acer saccharinum	GOOD	
627	BC	7	Wild Black Cherry	Prunus serotina	POOR	X2
628	WO	7	White Oak	Quercus alba	FAIR	
629	WO	4	White Oak	Quercus alba	GOOD	
630	WO	4	White Oak	Quercus alba	GOOD	X3
631	SM	7	Silver Maple	Acer saccharinum	GOOD	
632	WO	5	White Oak	Quercus alba	GOOD	
633	SM	10	Silver Maple	Acer saccharinum	GOOD	
634	SM	6	Silver Maple	Acer saccharinum	GOOD	X2
635	RO	7	Red Oak	Quercus rubra	GOOD	
636	SM	6	Silver Maple	Acer saccharinum	FAIR	
637	BC	7	Wild Black Cherry	Prunus serotina		

THE IDEAS AND DESIGN CONCEPTS EXPRESSED HEREIN AND THE GRAPHICALLY DISPLAYED ARRANGEMENT OF THEIR COMPONENTS REPRESENTED BY THIS DRAWING HAVE BEEN DEVELOPED FOR THE EXCLUSIVE USE OF THE SPECIFIED PROJECT AND ARE THE SOLE PROPERTY OF JMP Design & Build, Inc. INCORPORATED. A CONVEYANCE OR DISCLOSURE OF THE IDEAS DESIGN CONCEPTS OR USE OF ANY GRAPHICALLY DISPLAYED ARRANGEMENTS OF THE COMPONENTS SHALL BE AT THE DISCRETION AND ONLY THROUGH THE EXPRESSED WRITTEN CONSENT OF JMP Design & Build, Inc. ALL MATERIALS HEREIN ARE PROPRIETARY AND COPYRIGHTED

PROJECT

LANDEN RESERVE

BUILDER



MONDRIAN PROPERTIES
 50215 Schoenherr Road
 Shelby Township, MI 48315

PRELIMINARY
 CONSTRUCTION
 PERMIT

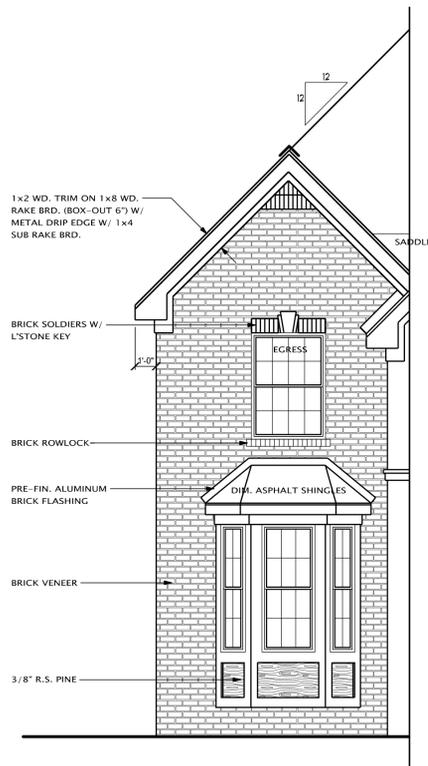
SHEET TITLE:
OPTIONAL ELEVATIONS

DATE: 05-05-2014
 DRAWN BY: C.J.B.

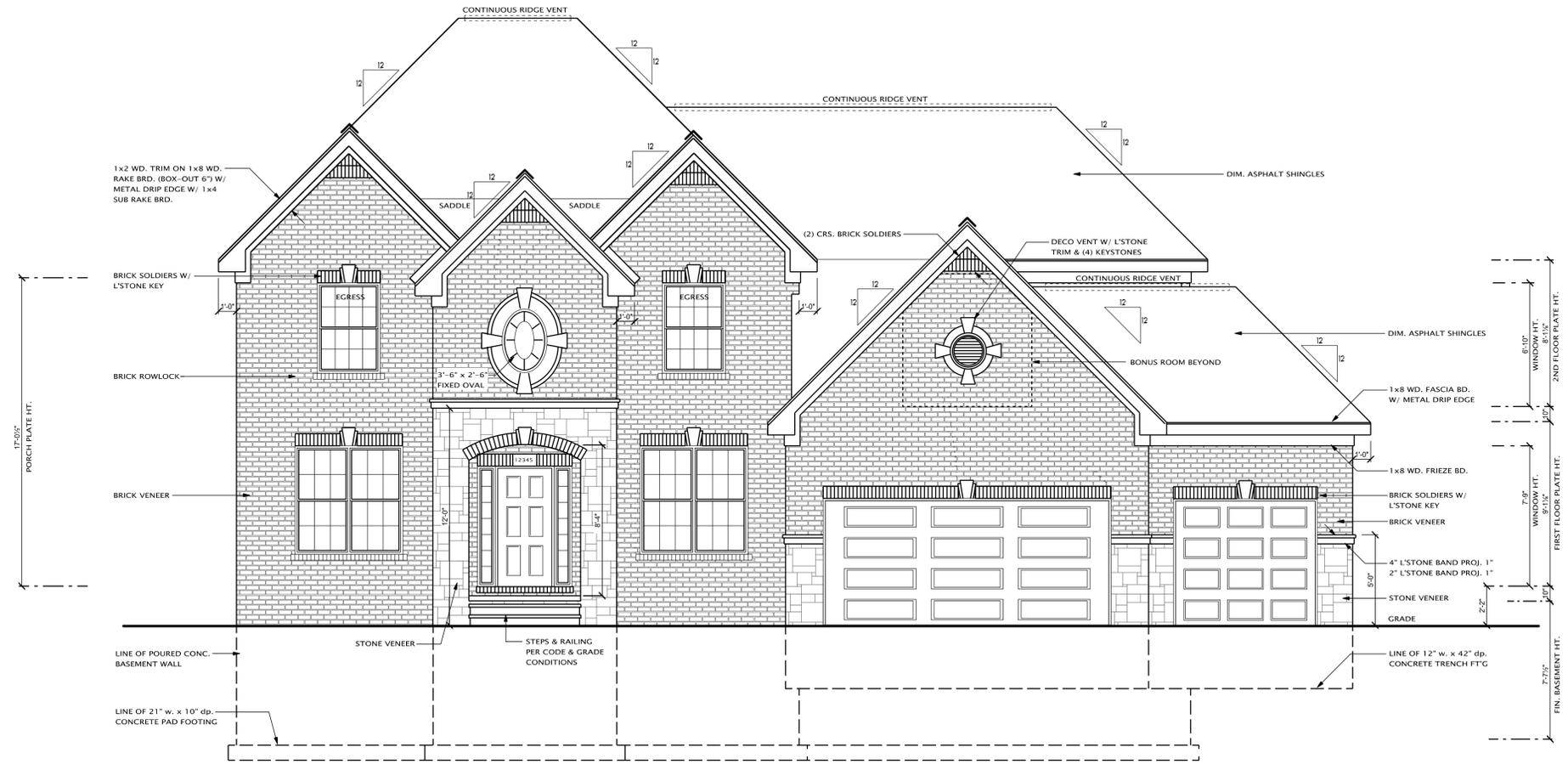
CHECKED BY:
 J.M.P.

JOB NUMBER:
 14 - 148

SHEET NUMBER:
 A-5

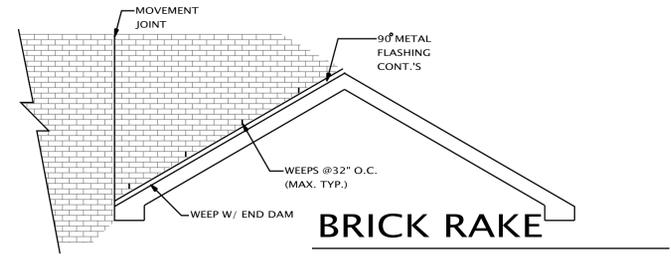


OPT. BAY WINDOW



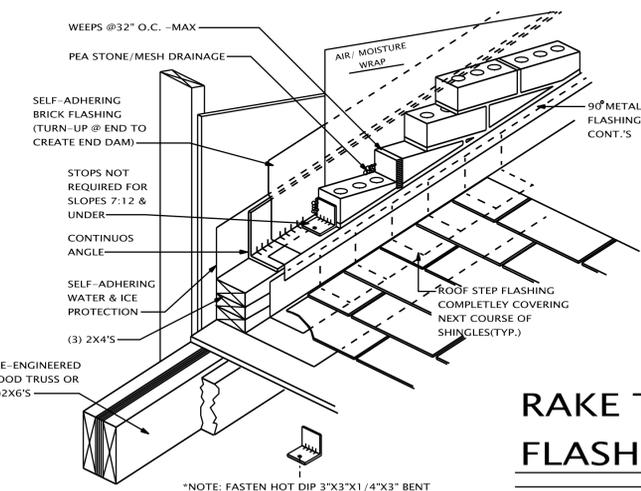
FRONT ELEVATION #1

SCALE: 1/4" = 1'-0"



BRICK RAKE

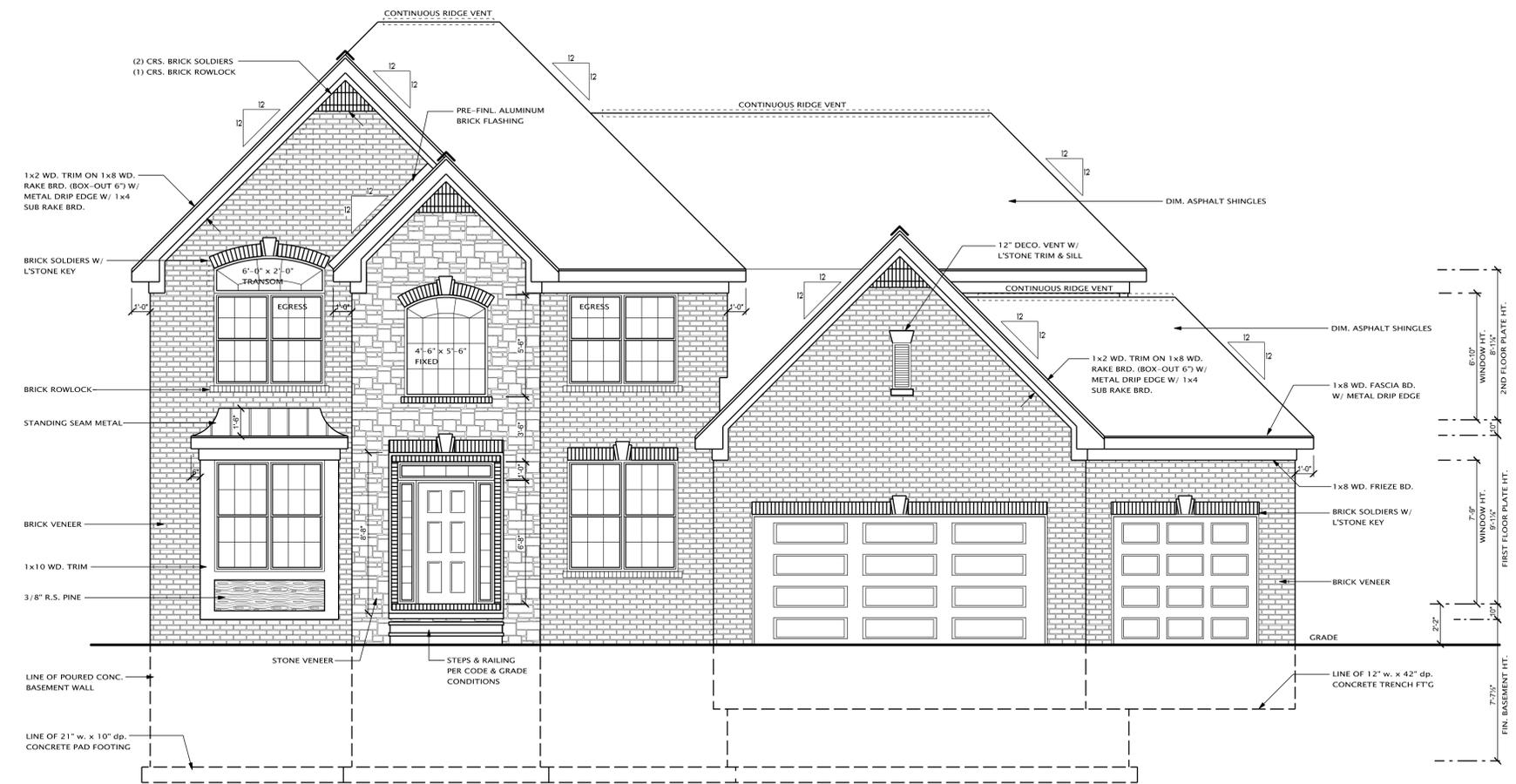
(BRICK VENEER OVER SLOPED ROOF)
 SCALE: N.T.S.



RAKE TRUSS-FLASHING DETAIL

SCALE: N.T.S.

*NOTE: FASTEN HOT DIP 3"x3"x1/4"x3" BENT PLATE STOPS 24" O.C. FOR SLOPES GREATER THAN 7:12 AND UP TO 12:12



FRONT ELEVATION #2

SCALE: 1/4" = 1'-0"

THE IDEAS AND DESIGN CONCEPTS EXPRESSED HEREIN AND THE GRAPHICALLY DISPLAYED ARRANGEMENT OF THEIR COMPONENTS REPRESENTED BY THIS DRAWING HAVE BEEN DEVELOPED FOR THE EXCLUSIVE USE OF THE SPECIFIED PROJECT AND ARE THE SOLE PROPERTY OF JMP DESIGN & BUILD, INC. INCORPORATED. A CONVEYANCE OR DISCLOSURE OF THE IDEAS DESIGN CONCEPTS OR USE OF ANY GRAPHICALLY DISPLAYED ARRANGEMENTS OF THE COMPONENTS SHALL BE AT THE DISCRETION AND ONLY THROUGH THE EXPRESSED WRITTEN CONSENT OF JMP DESIGN & BUILD, INC. ALL MATERIALS HEREIN ARE PROPRIETARY AND COPYRIGHTED.

PROJECT

MONDRIAN RESERVE

BUILDER



MONDRIAN PROPERTIES
50215 Schoenherr Road
Shelby Township, MI 48315

PRELIMINARY
CONSTRUCTION
PERMIT

SHEET TITLE:
FIRST FLOOR PLAN

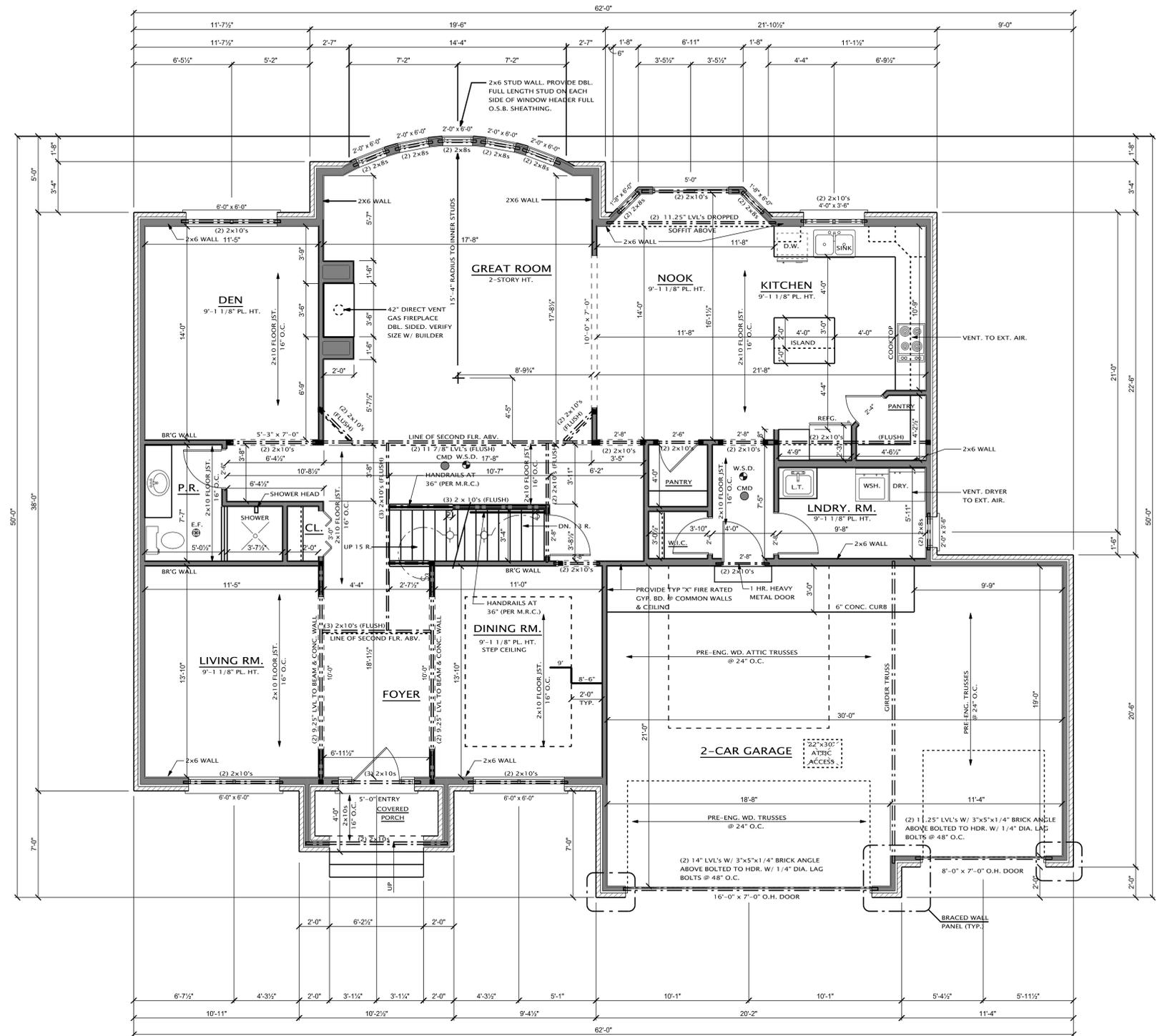
DATE: 01-08-2014 DRAWN BY: C.J.B.

CHECKED BY: J.M.P.

JOB NUMBER: 14 - 102

SHEET NUMBER:

A-2



SMOKE DETECTORS & CARBON MONOXIDE DETECTOR
SMOKE ALARMS SHALL BE INSTALLED IN THE FOLLOWING LOCATIONS:
- IN EACH SLEEPING ROOM
- OUTSIDE EACH SEPARATE SLEEPING AREA IN THE IMMEDIATE VICINITY OF THE BEDROOMS
- ON EACH ADDITIONAL STORY OF THE DWELLING, INCLUDING BASEMENTS BUT NOT INCLUDING CRAWL SPACES OR UNINHABITABLE ATTICS
ALL SMOKE DETECTORS SHALL BE INTERCONNECTED AND HAVE A BATTERY BACKUP.
CARBON MONOXIDE DETECTORS SHALL BE LOCATED IN THE NEAR VICINITY OF EACH BEDROOM AND IN BASEMENT NEAR STAIRS. HARD WIRED AND BATTERY BACK-UP. BACK UP

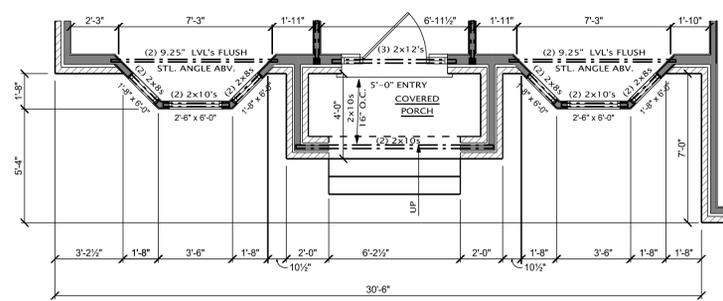
OPERABLE WINDOW NOTE:
ALL WINDOWS THAT ARE OPERABLE AND THE BOTTOM OF THE WINDOW IS 72" OR MORE ABOVE GRADE, SHALL BE 24" ABOVE FINISH FLOOR TO THE BOTTOM OF THE WINDOW.

ALL BEARING WALLS SHALL HAVE FLOOR JOIST UNDER EACH BEARING STUD. TYP. ALL BEARING WALLS

PROVIDE ALL ATTIC ACCESS W/ WEATHER STRIPPING

FIRST FLOOR PLAN

SCALE: 1/4" = 1'-0"
ELEVATION #1
1ST FLOOR = 1,800 SQ. FT.
SECOND FLOOR = 1,450 SQ. FT.
TOTAL = 3,250 SQ. FT.



OPTIONAL BAY WINDOWS

SCALE: 1/4" = 1'-0"
ELEVATION #1

THE IDEAS AND DESIGN CONCEPTS EXPRESSED HEREIN AND THE GRAPHICALLY DISPLAYED ARRANGEMENT OF THEIR COMPONENTS REPRESENTED BY THIS DRAWING HAVE BEEN DEVELOPED FOR THE EXCLUSIVE USE OF THE SPECIFIED PROJECT AND ARE THE SOLE PROPERTY OF JMP Design & Build, Inc. INCORPORATED. A CONVEYANCE OR DISCLOSURE OF THE IDEAS DESIGN CONCEPTS OR USE OF ANY GRAPHICALLY DISPLAYED ARRANGEMENTS OF THE COMPONENTS SHALL BE AT THE DISCRETION AND ONLY THROUGH THE EXPRESSED WRITTEN CONSENT OF JMP Design & Build, Inc. ALL MATERIALS HEREIN ARE PROPRIETARY AND COPYRIGHTED.

PROJECT

MONDRIAN RESERVE

BUILDER



MONDRIAN PROPERTIES
 50215 Schoenherr Road
 Shelby Township, MI 48315

PRELIMINARY ○
 CONSTRUCTION ●
 PERMIT ○

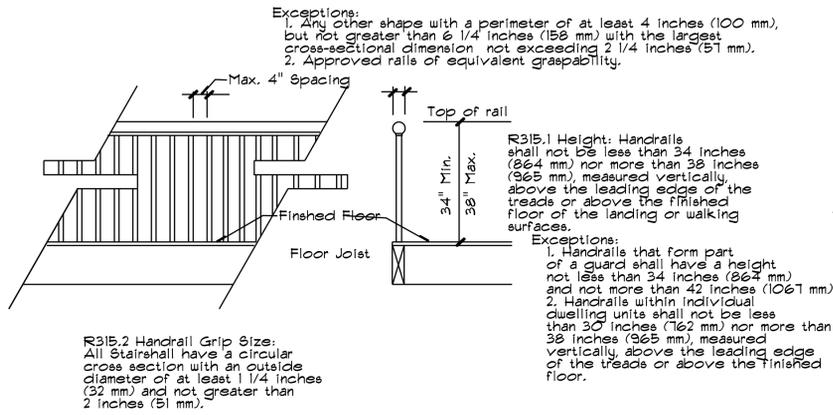
SHEET TITLE:
SECOND FLOOR PLAN

DATE: 01-08-2014 DRAWN BY: C.J.B.

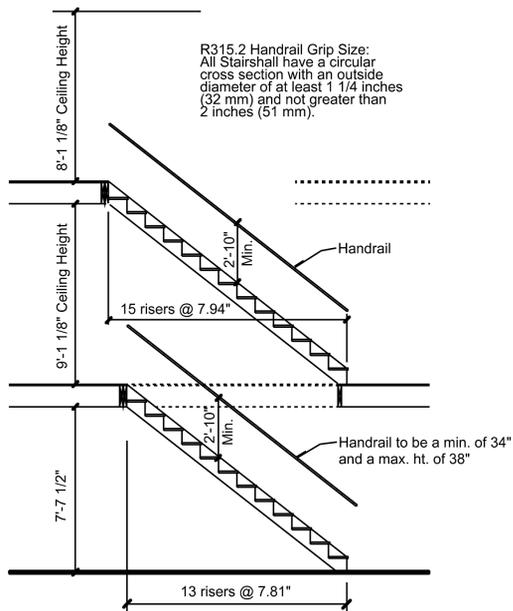
CHECKED BY: J.M.P.

JOB NUMBER:
 14 - 102

SHEET NUMBER:



Typical Railing Detail



TYPICAL STAIR SECTION

SMOKE DETECTORS & CARBON MONOXIDE DETECTORS

SMOKE ALARMS SHALL BE INSTALLED IN THE FOLLOWING LOCATIONS:
 - IN EACH SLEEPING ROOM
 - OUTSIDE EACH SEPARATE SLEEPING AREA IN THE IMMEDIATE VICINITY OF THE BEDROOMS
 - ON EACH ADDITIONAL STORY OF THE DWELLING, INCLUDING BASEMENTS BUT NOT INCLUDING CRAWL SPACES OR UNINHABITABLE ATTICS
 ALL SMOKE DETECTORS SHALL BE INTERCONNECTED AND HAVE A BATTERY BACKUP.

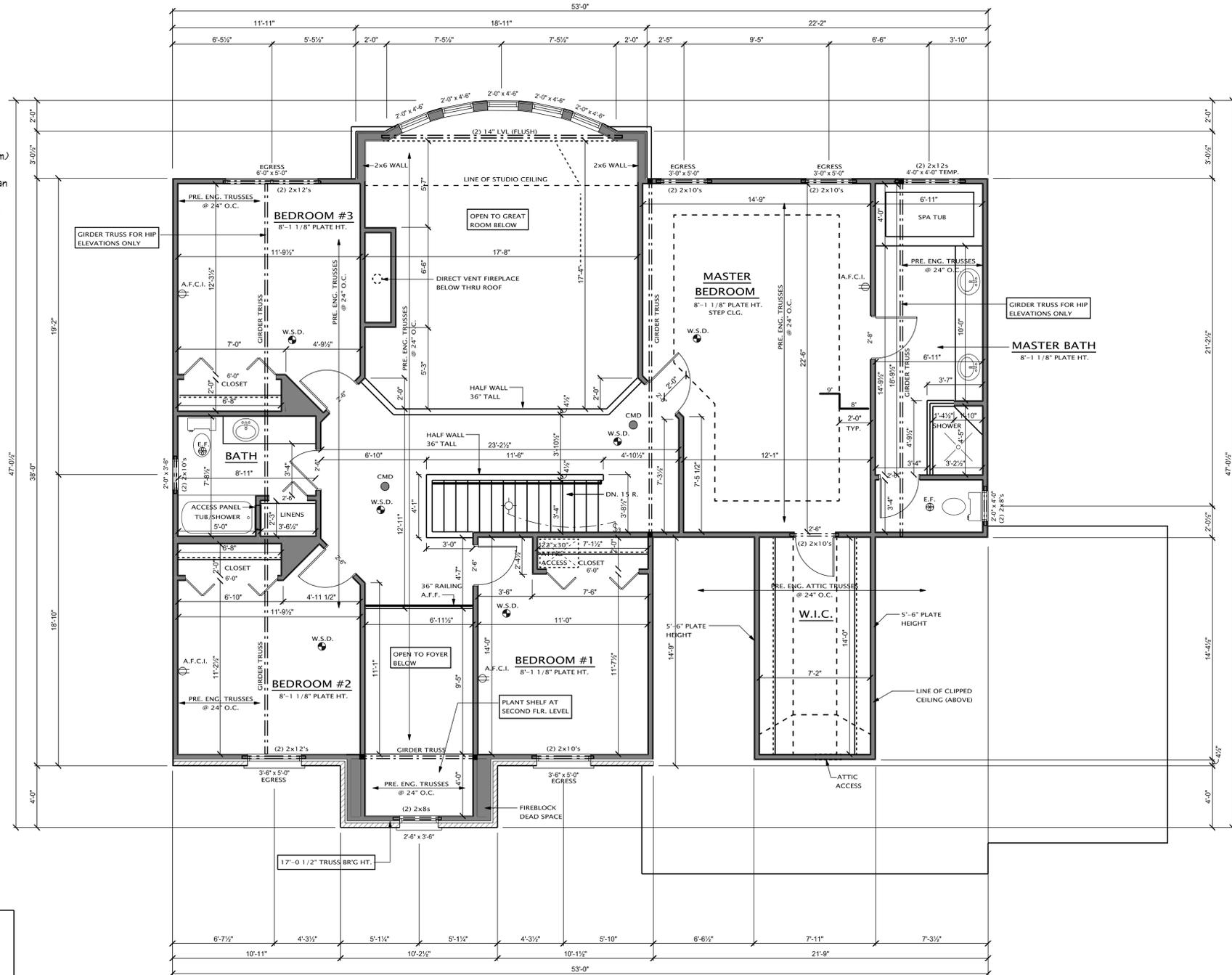
CARBON MONOXIDE DETECTORS SHALL BE LOCATED IN THE NEAR VICINITY OF EACH BEDROOM AND IN BASEMENT NEAR STAIRS, HARD WIRED AND BATTERY BACK-UP.
 BACK UP

OPERABLE WINDOW NOTE:

ALL WINDOWS THAT ARE OPERABLE AND THE BOTTOM OF THE WINDOW IS 72" OR MORE ABOVE GRADE, SHALL BE 24" ABOVE FINISH FLOOR TO THE BOTTOM OF THE WINDOW.

ALL BEARING WALLS SHALL HAVE FLOOR JOIST UNDER EACH BEARING STUD. TYP. ALL BEARING WALLS

PROVIDE ALL ATTIC ACCESS W/ WEATHER STRIPPING



SECOND FLOOR PLAN

SCALE: 1/4" = 1'-0" ELEVATION #1 SECOND FLOOR 1,450 SQ. FT.

SMOKE DETECTORS & CARBON MONOXIDE DETECTORS
 SMOKE ALARMS SHALL BE INSTALLED IN THE FOLLOWING LOCATIONS:
 - IN EACH SLEEPING ROOM
 - OUTSIDE EACH SEPARATE SLEEPING AREA IN THE IMMEDIATE VICINITY OF THE BEDROOMS
 - ON EACH ADDITIONAL STORY OF THE DWELLING, INCLUDING BASEMENTS BUT NOT INCLUDING CRAWL SPACES OR UNINHABITABLE ATTICS
 ALL SMOKE DETECTORS SHALL BE INTERCONNECTED AND HAVE A BATTERY BACKUP.
 CARBON MONOXIDE DETECTORS SHALL BE LOCATED IN THE NEAR VICINITY OF EACH BEDROOM AND IN BASEMENT NEAR STAIRS, HARD WIRED AND BATTERY BACK-UP.
 BACK UP

OPERABLE WINDOW NOTE:
 ALL WINDOWS THAT ARE OPERABLE AND THE BOTTOM OF THE WINDOW IS 72" OR MORE ABOVE GRADE, SHALL BE 24" ABOVE FINISH FLOOR TO THE BOTTOM OF THE WINDOW.

ALL BEARING WALLS SHALL HAVE FLOOR JOIST UNDER EACH BEARING STUD. TYP. ALL BEARING WALLS

PROVIDE ALL ATTIC ACCESS W/ WEATHER STRIPPING

SITE DATA:

LOCATION OF PROJECT:
#5318 JOHN R. ROAD (EAST SIDE OF JOHN R. ROAD, NORTH OF E. LONG LAKE)

SIZE OF PROPERTY: PHASE 1: 7.92 ACRES, PHASE 2: 7.92 ACRES, TOTAL 15.84 ACRES

PROPOSED USE OF PROPERTY: FORTY-SIX (46) DETACHED, SINGLE FAMILY HOMES

CURRENT ZONING: R-1C, ONE-FAMILY RESIDENTIAL DISTRICT

SURROUNDING PROPERTY DETAILS:

DIRECTION	ZONING	USE
NORTH	R-1C, ONE-FAMILY RESIDENTIAL DISTRICT	SINGLE-FAMILY HOMES
SOUTH	R-1C, ONE-FAMILY RESIDENTIAL DISTRICT	SINGLE-FAMILY HOMES
EAST	R-1C, ONE-FAMILY RESIDENTIAL DISTRICT	SINGLE-FAMILY HOMES
WEST	R-1C, ONE-FAMILY RESIDENTIAL DISTRICT	SINGLE-FAMILY HOMES

REQUIRED AND PROVIDED LOT DIMENSIONS:

	REQUIRED:	PROVIDED:
FRONT	30 FOOT SETBACK	30 FOOT SETBACK
REAR	40 FOOT SETBACK	40 FOOT SETBACK
SIDES	10 FOOT MIN. (ONE), 20 FOOT TOTAL	10 FOOT MIN. (ONE), 20 FOOT TOTAL
DENSITY (# OF UNITS)	3.1 UNIT/ACRE=49 UNITS	2.90 UNIT/ACRE=46 UNITS
LOT SIZE PER UNIT	10,500 SQ. FT. (FOR PROJECTS W/ SEWER)	9,477 SQ. FT. SMALLEST 11,032 SQ. FT. AVERAGE
MAXIMUM HEIGHT	30 FEET, 2.5 STORY	20'-1/4" FEET, 2 STORY
LOT WIDTH	85 FEET	76.50 FEET SMALLEST*
MAX. LOT AREA COVERED BY BUILDINGS	30 PERCENT	19 PERCENT
MIN. FLOOR AREA PER UNIT	1,200 SQ. FT.	1,800 SQ. FT.

* THE LOT SIZE AVERAGE OPTION HAS BEEN APPLIED PER SECTION 10.02.
MIN. UNIT WIDTH AT BLDG. SETBACK REQUIRED = 76.50 FEET

PROJECT AND SITE DESCRIPTION:
PROPOSED CONSTRUCTION OF A 46-LOT (21 LOTS IN PHASE 1 AND 25 LOTS IN PHASE 2) SINGLE FAMILY DETACHED SITE CONDOMINIUM PROJECT ON THE EAST SIDE OF JOHN R. ROAD. ACCESS TO ALL UNITS WILL BE VIA A NEW STREET OFF OF JOHN R. ROAD AND MAYFLOWER NORTH OF THE SITE. THE PROPOSED RESIDENTIAL USE IS PERMITTED BY-RIGHT IN THE R-1C DISTRICT. THE SITE IS CURRENTLY IMPROVED WITH ONE (1) SINGLE-FAMILY HOME, WHICH WILL BE DEMOLISHED.

SITE ARRANGEMENT:
THE PROPOSED SITE CONDOMINIUM CONSISTS OF 46 UNITS WITH LOT SIZES RANGING BETWEEN 9,477 SQUARE FEET TO 18,576 SQUARE FEET, WITH AN AVERAGE LOT SIZE OF 11,032 SQUARE FEET. ALL UNITS FRONT ON THE NEW PUBLIC STREET CONNECTION. THE PROPOSED LOTS ARE REGULAR IN SHAPE, ALLOW FOR ADEQUATE SETBACKS, AND PERMIT SUFFICIENT SPACE FOR THE HOMES AND INGRESS AND EGRESS FOR EACH UNIT ACCORDING TO THE LOT SIZING AVERAGING OPTION.

NATURAL RESOURCES:
THE SITE IS CURRENTLY IMPROVED WITH ONE (1) SINGLE-FAMILY HOME, WHICH WILL BE DEMOLISHED. THE SITE IS ENCUMBERED WITH ONE (1) NON-REGULATED WETLAND AND SIGNIFICANT TREE COVER.

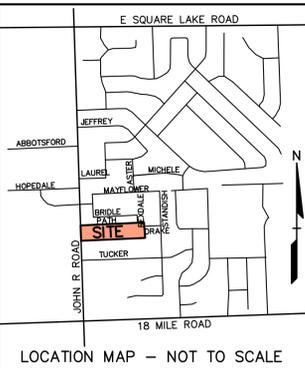
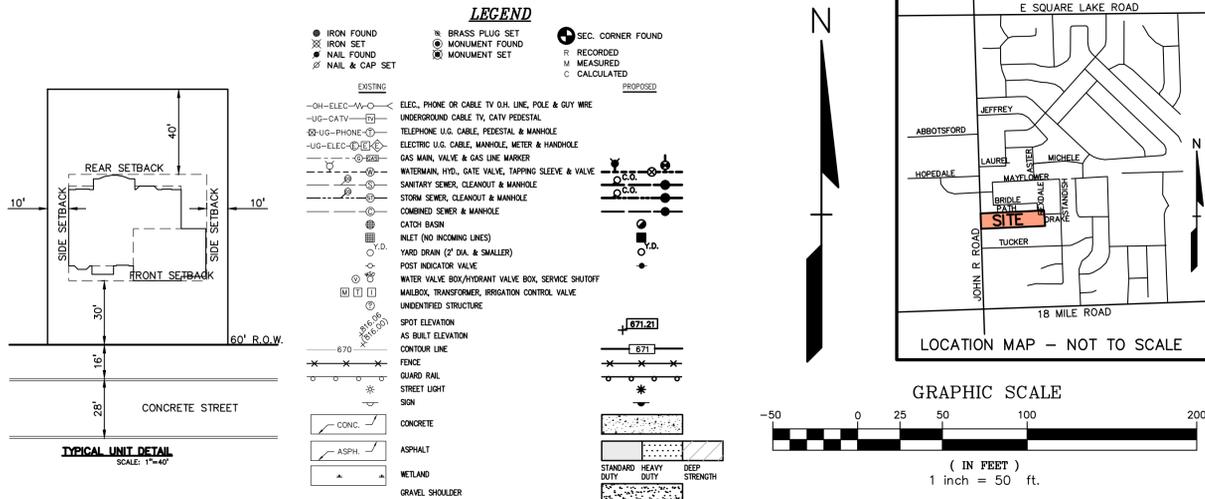
FLOODPLAIN:
ACCORDING TO FEMA FLOODPLAIN MAP 100 YEAR FLOODPLAIN (AE) IMPACTS THE WESTERLY PORTION OF THE DEVELOPMENT. BASED ON CURRENT TOPO DATA THIS IMPACT IS SIGNIFICANTLY REDUCED AND LIMITED TO THE LINEAR DITCH ASSOCIATED WITH BACKWATER FROM AN OFF-SITE 36" STORM SEWER. APPLICATION IS BEING MADE TO FEMA FOR A LOMA.

ACCESS AND CIRCULATION:
VEHICULAR ACCESS AND CIRCULATION:
VEHICULAR ACCESS TO ALL UNITS WILL BE VIA A NEW PUBLIC STREET OFF JOHN R ROAD AND OFF OF THE STUB TO THE NORTH OF THE SITE. THE NEW ROAD WILL HAVE A SIXTY (60) FOOT WIDE RIGHT-OF-WAY

PEDESTRIAN ACCESS AND CIRCULATION:
A 5-FOOT WIDE SIDEWALK IS PROVIDED AT BOTH SIDES OF THE NEW ROAD.

UTILITIES:
UTILITIES ARE PLACED WITHIN STREET RIGHT-OF-WAY, OR WITHIN EASEMENTS APPROVED AS TO SIZE AND LOCATION BY THE CITY ENGINEER.

ALL SITES ARE SERVED BY PUBLIC WATER, SANITARY SEWER, STORM WATER AND DETENTION/RETENTION SYSTEMS CONSTRUCTED TO CITY STANDARDS. AT THE EXPENSE OF THE DEVELOPER, EASEMENTS OVER THESE SYSTEMS SHALL BE CONVEYED AND RECORDED BEFORE OCCUPANCY PERMITS ARE ISSUED FOR DWELLING UNITS.



REVISIONS

NO.	BY	CHK	DESCRIPTION	DATE

WEST 1/4 CORNER SECTION 12 T.2N., R.11E. CITY OF TROY, OAKLAND COUNTY, MI



Lot Area Table

Lot #	Area (sf)
1	12,596
2	10,197
3	10,197
4	10,197
5	10,197
6	10,197
7	10,197
8	10,197
9	10,197
10	17,403
11	14,603
12	10,793
13	12,603
14	10,197
15	10,197
16	10,197
17	10,197
18	10,197
19	10,197
20	10,197
21	10,197
22	14,192
23	9,477
24	9,477
25	9,477
26	10,197
27	10,197
28	10,197
29	10,197
30	10,197
31	10,196
32	12,649
33	10,460
34	12,556
35	18,576
36	10,197
37	10,197
38	10,197
39	10,197
40	10,197
41	10,197
42	10,197
43	10,197
44	10,197
45	10,796
46	15,729
Total:	507,493
Avg:	11,032

CAUTION!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION.

THIS DRAWING AND DESIGN ARE THE PROPERTY OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. THEY ARE NOT TO BE REPRODUCED, COPIED, OR USED IN ANY MANNER WITHOUT THE WRITTEN CONSENT OF PROFESSIONAL ENGINEERING ASSOCIATES, INC. ALL COMMON LAW RIGHTS OF COPYRIGHT AND OTHER RIGHTS ARE HEREBY RESERVED.

CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR SHALL BE REQUIRED TO ASSUME SOLE RESPONSIBILITY FOR THE SAFETY OF ALL PERSONS AND PROPERTY THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONSTRUCTION AND NOT LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL ENGINEER FROM ANY AND ALL LIABILITY, FINANCIAL OR OTHERWISE, ARISING FROM OR RESULTING FROM WORK ON THIS PROJECT EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE DESIGN PROFESSIONAL ENGINEER.

3 FULL WORKING DAYS BEFORE YOU DIG CALL

811

Know what's below
Call before you dig

MISS DIG System, Inc.
1-800-482-7171 www.missdig.net

PROFESSIONAL ENGINEERING ASSOCIATES
2430 Rochester Ct. Suite 100
Troy, MI 48063-1872
Phone: (248) 689-9090
Fax: (248) 689-1044
website: www.peainc.com

MONDRIAN PROPERTIES HUNTERS PARK LLC
50216 SCHOENHERR
SHELBY TWP., MI 48315

ALTERNATE PRELIMINARY SITE PLAN
HUNTERS PARK 2
PART OF THE SW 1/4 OF SECTION 12, T.2N. R. 11E. CITY OF TROY, OAKLAND COUNTY, MICHIGAN

DES: TMK DN TMK SUR DEC P.M.
JBT

ORIGINAL ISSUE DATE: MARCH 4, 2015
PEA JOB NO. 2014-249
SCALE: 1" = 50'
DRAWING NUMBER:
ALT

SOUTHWEST CORNER SECTION 12 T.2N., R.11E. CITY OF TROY, OAKLAND COUNTY, MI



RICK SNYDER
GOVERNOR

STATE OF MICHIGAN
DEPARTMENT OF ENVIRONMENTAL QUALITY
SOUTHEAST MICHIGAN DISTRICT OFFICE



DAN WYANT
DIRECTOR

February 23, 2015

Mr. Joseph Maniaci
Mondrian Properties
50215 Schoenherr Road
Shelby Township, MI 48315

RECEIVED
MAR 5 - 2015
PLANNING

Dear Mr. Maniaci:

SUBJECT: Preapplication Meeting – Holman Parcel – John R, Troy
Michigan Department of Environmental Quality (MDEQ)
File Number 15-63-0001-P

This letter is a follow-up to our February 13, 2015, preapplication meeting regarding the proposed subdivision in the City of Troy, Oakland County at 5318 John R Road. The purpose of the preapplication meeting was to review the presence of a small flagged wetland, and to provide you with information that will clarify the permit process, answer preliminary questions about your specific project in order to avoid delays at a later date, and to determine, if possible, the need for wetland or inland lakes and streams permits.

During this on-site meeting we reviewed the need to obtain a permit under Part 303, Wetlands Protection, of the Natural Resources and Environmental Protection Act, 1994 PA 451, as amended (NREPA). The review was based on discussion of the proposed project and the proposed site conditions.

Based on the information provided and reviewed in our office, including your delineation of wetlands on the proposed project site, the MDEQ's Water Resources Division (WRD) has determined that a permit is not required under Part 303 of the NREPA. Please reference the file number at the top of this letter when referencing this project.

This determination is based on the attached project site plan, date-stamped January 2, 2015, along with other information provided at the time of this meeting only. Provided that the proposed project and location are not altered, this determination is binding on the MDEQ for a period of two years from the date of this meeting.

During the review of the project site, the MDEQ's Water Resources Division (WRD) staff made the following findings regarding the need for a permit under Part 301 and Part 303 of the NREPA:

- A permit is required for the project as proposed.
- A permit is not required for the project as proposed.
- It cannot be determined whether a permit is required given the information presented at this time.

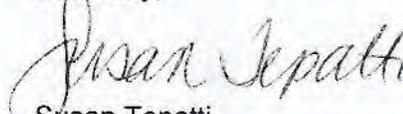
15-63-0001-P
Page 2
February 23, 2015

One wetland area had been identified by King & MacGregor Environmental. Based on the field inspection it was determined that the wetland was isolated and less than five acres in size, therefore not regulated by Part 303. The linear drainage feature located on the parcel to the north, was determined not to meet the definition of a stream under File #13-63-0281-P, and therefore, the adjacent wetland on this parcel, is not regulated by Part 303.

Please note that this is not a permit. The MDEQ file number assigned to this project is 15-63-0001-P. Please keep a record of this file number, and use it when referencing this project or otherwise corresponding with our office on this project.

We appreciate the opportunity to meet with your representative to address these concerns. We have established a file for this project, and the information submitted to date will be kept on file. If you should have follow up questions before then, please contact me at 586-256-7272; or at tepattis@michigan.gov; or MDEQ, WRD, Southeast Michigan District Office, 27700 Donald Court, Warren, MI 48092-2793.

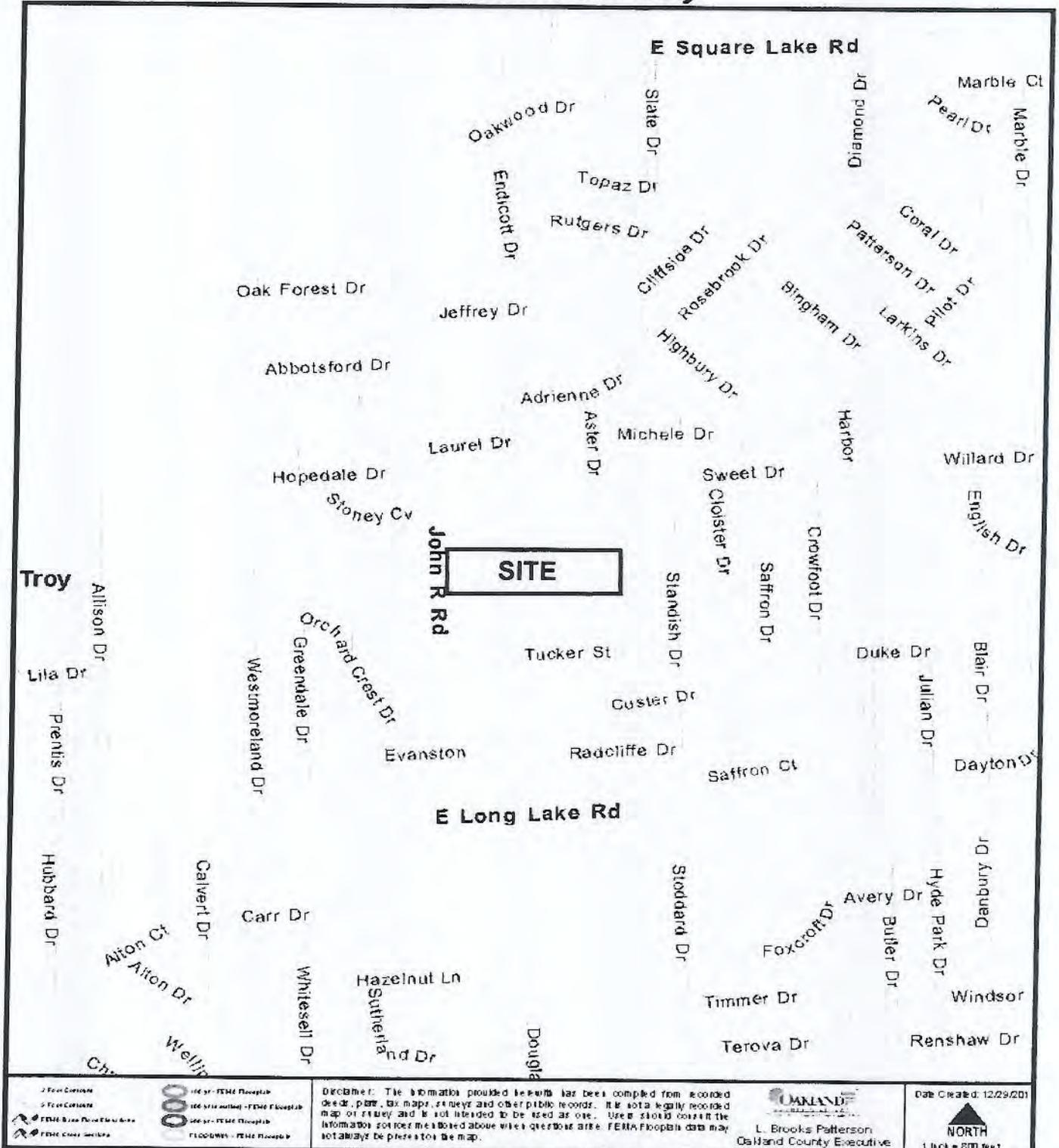
Sincerely,


Susan Tepatti
Water Resources Division

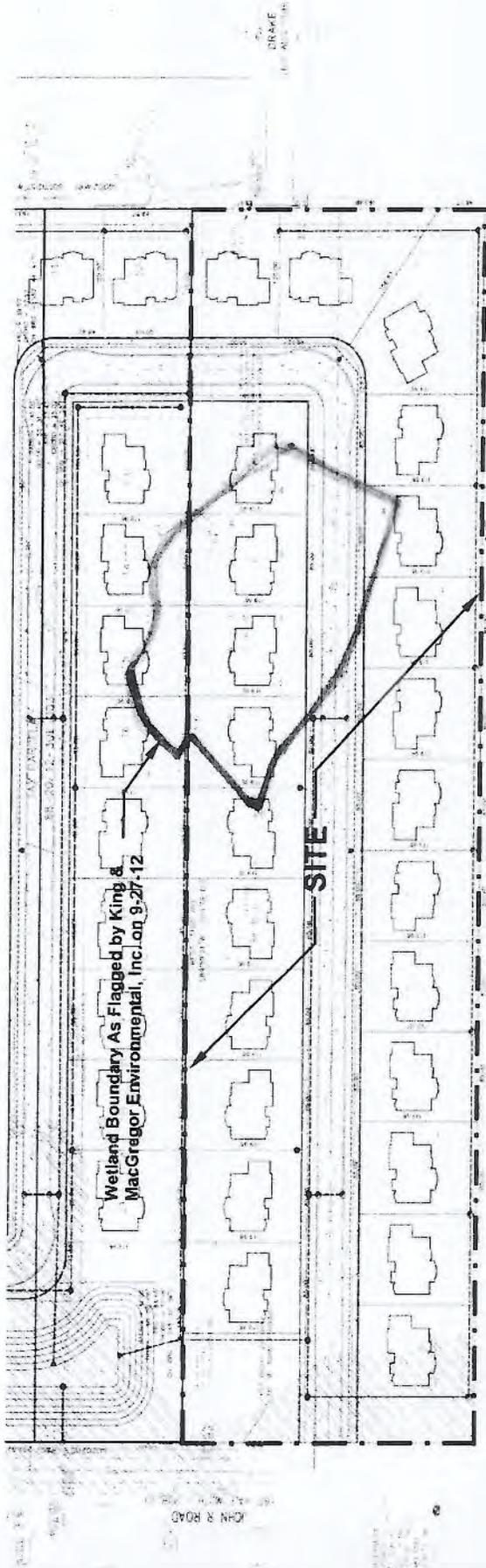
Attachment

cc: Woody Held, King & MacGregor Environmental (Canton)
City of Troy, Engineering

5318 John R Troy



15-63-0001
DEQ



**PROPOSED SITE PLAN
5318 JOHN R ROAD, CITY OF TROY**

DEB #15-632001P

Pertaining to City Council resolution #87-1086 on 9/14/87 regarding the final approval of the preliminary plat for Long Lake Meadows Subdivision:

"BE IT FURTHER RESOLVED, That before removal of the barrier to and from Tucker Street, said street shall be paved, A Public Hearing is to be scheduled and notice of the Public Hearing is to be sent to all property owners on Tucker Street."

This is a request to City Council to overturn that decision and have the barricade on Tucker removed, whether or not Tucker is paved, per the following considerations:

1. Public Safety.

Both **Fire Chief Nelson** and **Fire Marshall Roberts** are very supportive of this request. Fire Marshall Roberts said I could quote him in saying **"Tucker is the most direct route"** to access residents in our neighborhood in case of an emergency.

Fire Chief Nelson also stated that the fire department came in on the wrong side of the barricade just last year but fortunately no personal injuries were incurred.

"I personally think that the Planning Commission's number one goal is to consider the protection of Health and Welfare."

Donald Edmunds, Planning Commission Chair, Planning commission meeting of March 24th, 2015 regarding the Drake connection to Hunters Park II.

"Interconnected streets are the preferred desire of the Fire Department and Public Safety."

"In addition, neighborhood connections is strongly preferred by other services such as Mail, Garbage, School Buses, etc., so there are many people who actually use these interconnected neighborhood streets."

Ben Carlisle, Planning Commission meeting of March 24th, 2015 regarding the Drake connection to Hunters Park II.

2. Distribution of traffic on Saffron, Mayflower and the proposed Hunters Park II connection of Drake Street.

With the newer homes on Radcliff Street and the development of Bridgewater Estates, Hunters Park I and II along with the existing residents of Long Lake Meadows, none of which existed at the time of the barricade, the traffic would be more evenly distributed between Saffron, Mayflower, Drake and Tucker providing safer neighborhood streets.

"Interconnected streets and neighborhoods are a policy and direction of the City. This in both of the Master Plan and Zoning Ordinance and better distributes traffic within neighborhoods. You're not pushing it to one Street or another."

Ben Carlisle, Planning Commission meeting of March 24th, 2015 regarding the Drake connection to Hunters Park II.

3. Tucker is a Public Road.

As residents and tax payers of Troy, we in essence, over the last 27 years, have paid for the up keep of a public road that we are not allowed to utilize within our own neighborhood. Other dirt roads in Troy are not barricaded off from public use, for example; Fernleigh and Willow Grove both connect to improved subdivision roads.

Planning Chair Donald Edmund's response to a question raised at the March 24th 2015 Planning Commission Meeting regarding whether or not the City had any current or future plans to pave Tucker.

"However, I like to say that you raise a really good point. I don't know if most residents know that it costs considerably more to maintain a gravel road and I think we're down to less than 7 miles or 5 miles left in Troy and I have one right near my house in fact. And I was out there today (on Tucker) and they look like they all really, for the most part, the majority of those homes are large homes. So I don't think they would necessarily qualify for a community log grant development, which is a low income thing. But, it always bothered me that we, the rest of us residents are actually subsidizing those people who won't give up their gravel roads and there are quite attractive 30 year long term financing plans that the Assessing Department offers at a low interest. So, I hope that at some point City Council will look into that again and see if we can't do that."

4. If the barricade were removed it would provide a Vehicular Route to Wass Elementary school for residents on the gravel side of the barrier without having to access a major road.

William J Huotari

From: William J Huotari
Sent: Thursday, July 16, 2015 11:29 AM
To: 'dansemi'
Cc: Kurt Bovensiep; Brent Savidant
Subject: RE: Follow up question
Attachments: RE: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Dan, in regard to your questions:

1. The wood posts are supplied by Burt Forest Products- 227 Felch St., Ann Arbor, MI 48107
2. Email is attached

Mr. Savidant has been copied on the emails I have received as of yesterday. I did receive a couple of more emails late last night and this morning, so a complete package of information received will be provided to Mr. Savidant prior to the August 11 Planning Commission meeting.

Thanks, Bill

From: dansemi [mailto:dansemi@yahoo.com]
Sent: Thursday, July 16, 2015 9:56 AM
To: William J Huotari
Cc: Dansemi
Subject: Follow up question

Hi Bill,

Thank you very much for facilitating the Traffic Committee's meeting last night and for providing the laptop for the presentation.

If you don't mind I have 2 more follow up questions and 1 request:

1 - Can you please let me know the supplier the City of Troy is using for the EVA wooden posts that are installed in the 12 EVAs currently available in the City.

2 - You mentioned something about an e-mail answer you received from RCOC regarding John R/ Tucker intersection. Can you please share that e-mail with me.

Request: Can you please forward all the e-mails that you have received regarding the "Tucker barrier removal" issue to Mr. Savidant in the Planning Department. That way we (the residents that wrote to you) don't have to start the e-mail process from scratch for the next phase which will be the Planning Commission.

Thank you again for all your help and support,

Dan

William J Huotari

From: William J Huotari
Sent: Thursday, July 16, 2015 11:24 AM
To: 'Cercone Marco (FCA)'
Subject: RE: Traffic Committee Meeting from 15JUL15

Mr. Cercone, I will forward this email as well as the other emails received on this subject to the Planning Director for inclusion when the item is presented to the Planning Commission at their meeting of August 11.

All of the information from the Traffic Committee meeting as well as the Planning Commission meeting will be forwarded to City Council when the item is presented for their consideration at their meeting of August 24.

Thanks, Bill

From: Cercone Marco (FCA) [mailto:marco.cercone@fcagroup.com]
Sent: Thursday, July 16, 2015 11:00 AM
To: William J Huotari
Subject: Traffic Committee Meeting from 15JUL15

Bill,

After experiencing my first traffic committee meeting Jul 15th, 2015 I left in much disbelief....

First let me start off by saying that I have lived in my home at 2349 Tucker for over 24 years. In that time period and before the new development of the three new subdivision just north and west of me (Bridgewater, and Hunters Park phase 1 & 2) there has been only 1 entrance and exit for the homeowners who live on Drake, Standish, Custer, Radcliffe, and Tucker (east of the barricade). For over 23 years no complaints from any homeowner on the mentioned roads were issued. The entrance/exit was to take Saffron out to Long lake.

This is the very corner I live at and have watched this traffic go by home for over 23 years! 23 years, with no complaints!

Now that the development of three new subdivision have started there will now be 2 new entrances/exit available to the residents who live on the mentioned roads above. That is a 66% improvement to Troy's policy of roads being interconnected, quicker routes for police and fire response, per the Police Chief, Fire Chief, and Station 5 Fire Captain.

I find it very disturbing that the resident who lives at 2237 Drake (James and Dorothy Konarske) find the Tucker barricade to be a problem for them. It seems to me that because they lost their "dead end" status in front of their home on Drake, they now have a personal vendetta to remove a barricade that has nothing to do with them, and that some members of the traffic committee who also don't live in the area, can over-rule what all the homeowners on Tucker (both east and west of the barricade) have requested, to leave the barricade alone!

Please attached/forward this e-mail to the city council when this topic comes up for vote to share my concern has a homeowner who lives in the area of this barricade.

Regards,

Marco Cercone

2349 Tucker Dr.
Troy, MI.

William J Huotari

From: Cercone Marco (FCA) <marco.cercone@fcagroup.com>
Sent: Thursday, July 16, 2015 11:00 AM
To: William J Huotari
Subject: Traffic Committee Meeting from 15JUL15

Bill,

After experiencing my first traffic committee meeting Jul 15th, 2015 I left in much disbelief....

First let me start off by saying that I have lived in my home at 2349 Tucker for over 24 years. In that time period and before the new development of the three new subdivision just north and west of me (Bridgewater, and Hunters Park phase 1 & 2) there has been only 1 entrance and exit for the homeowners who live on Drake, Standish, Custer, Radcliffe, and Tucker (east of the barricade). For over 23 years no complaints from any homeowner on the mentioned roads were issued. The entrance/exit was to take Saffron out to Long lake.

This is the very corner I live at and have watched this traffic go by home for over 23 years! 23 years, with no complaints!

Now that the development of three new subdivision have started there will now be 2 new entrances/exit available to the residents who live on the mentioned roads above. That is a 66% improvement to Troy's policy of roads being interconnected, quicker routes for police and fire response, per the Police Chief, Fire Chief, and Station 5 Fire Captain.

I find it very disturbing that the resident who lives at 2237 Drake (James and Dorothy Konarske) find the Tucker barricade to be a problem for them. It seems to me that because they lost their "dead end" status in front of their home on Drake, they now have a personal vendetta to remove a barricade that has nothing to do with them, and that some members of the traffic committee who also don't live in the area, can over-rule what all the homeowners on Tucker (both east and west of the barricade) have requested, to leave the barricade alone!

Please attached/forward this e-mail to the city council when this topic comes up for vote to share my concern has a homeowner who lives in the area of this barricade.

Regards,

Marco Cercone

2349 Tucker Dr.
Troy, MI.

William J Huotari

From: William J Huotari
Sent: Thursday, July 16, 2015 8:47 AM
To: 'Fiori Buz'
Subject: RE: Tucker removal barrer
Attachments: Item #7 from Agenda_Tucker Street.pdf

Fiyori, the request to remove the barricade on Tucker Street, east of John R, was made by James and Dorothy Konarske of 2237 Drake. The Konarske's point out an inconsistency related to the City's street connectivity policy. Specifically, Tucker Street was barricaded in 1987, which eliminated a vehicular connection to John R Road and reduced interconnectivity.

The newly approved Hunters Park 2 Site Condominium development, north of Tucker, includes a vehicular connection to Drake. This connection eliminated the "dead end" status of Drake and provided a more direct vehicular connection with John R.

Attached is the section of the agenda that pertains to the Tucker barricade and information on the item that was considered at the meeting last night.

The Traffic Committee made no recommendation as there was a 3-3 tie vote when the members voted.

The item will be on the Planning Commission agenda at their meeting of August 11 for their consideration.

Recommendations from the Traffic Committee and Planning Commission will be forwarded to Troy City Council at their meeting of August 24.

Sincerely,

Bill

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

From: Fiori Buz [mailto:flowerb800@gmail.com]
Sent: Thursday, July 16, 2015 7:49 AM
To: William J Huotari
Subject: Tucker removal barrer

Good morning Mr. Huotari,

My family and I live in the sub where Tucker Dr is (2221 Custer Dr).

We are against opening up. I also don't understand why Duke also requested to open.
I want to attend last night meeting was unable.

We would be interested in information why the two street are required to open.

Thank you,
Fiyori Buzuayene

William J Huotari

From: Fiori Buz <flowerb800@gmail.com>
Sent: Thursday, July 16, 2015 7:49 AM
To: William J Huotari
Subject: Tucker removal barrer

Good morning Mr. Huotari,

My family and I live in the sub where Tucker Dr is (2221 Custer Dr).

We are against opening up. I also don't understand why Duke also requested to open.
I want to attend last night meeting was unable.

We would be interested in information why the two street are required to open.

Thank you,
Fiyori Buzuayene

William J Huotari

From: Suzanne Monck <smonck@bloomfield.org>
Sent: Wednesday, July 15, 2015 5:53 PM
To: William J Huotari
Subject: Committee Meeting: Tucker Barricade

As a resident of the Long Lake/Spring Meadows subdivision, I am writing to express my opposition to the removal of the barricade on Tucker. With the recent development of the subdivision to the north of Long Lake Road, there is sufficient access to major roads for safety vehicles and residents to have access to the subdivisions. Additionally, this proposed change will unnecessarily increase the non-residential traffic within the subdivision detracting from the safety and privacy of the subdivision.

Sincerely,

Suzanne Monck
5228 Standish Drive

--

Suzanne Monck

International Academy
www.iatoday.org



William J Huotari

From: Ligia Murza <cafelutsa@yahoo.com>
Sent: Wednesday, July 15, 2015 4:06 PM
To: William J Huotari
Subject: Tucker Drive

Hello Mr. Huotari,

My name is Ligia Murza and I live on 'Old Tucker' as it has been called. We really do like our street and that it's a historic part of Troy with Tucker family history and the Schoenherr family that lived on this street. And so yes, I highly oppose the barrier being taken down.

Indeed there are the personal reasons in that my children and the neighboring children are very often outside biking, or walking across, or playing on the street or near the street..and it being closed off makes it safe for them to do this.

Jesus Christ Rules!

William J Huotari

From: Adriana Apahidean <adrianadean@comcast.net>
Sent: Wednesday, July 15, 2015 4:05 PM
To: William J Huotari
Subject: Re: Tucker Barrier- opposed to removal

Btw, very much like your take away.

I am an Engineering Specialist of Tech Collaboration at General Dynamics, Maneuver Collaboration Center in Sterling Heights.

We have a very similar motto for bringing in the best of industry and individuals to the table, to care for the Warfighter's needs.

Anyhow, for what's it's worth, when you invite excellence, excellence comes.

Adriana

Sent from my iPhone

On Jul 15, 2015, at 11:34 AM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Thank you for your email.

I will provide a copy of the same to the Traffic Committee members when the item is discussed.

Sincerely,

Bill

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov 

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

-----Original Message-----

From: Adriana Apahidean [<mailto:adrianadean@comcast.net>]

Sent: Wednesday, July 15, 2015 11:16 AM

To: William J Huotari

Subject: Tucker Barrier- opposed to removal

Hello Mr. Huotari:

Just a short note that I too am opposed to the removal of the barrier on Tucker Dr.

This issue requires a thorough traffic study and cost assessment. I urge the Traffic Committee to make an educated and well researched decision.

Thank you kindly,

Adriana Apahidean

2223 Tucker Dr.

Troy, Mi

William J Huotari

From: William J Huotari
Sent: Wednesday, July 15, 2015 3:23 PM
To: 'dansemi'
Subject: RE: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).
Attachments: Email Results.pdf

Dan, attached is a depiction of the locations of residents that want to KEEP the barricade (red box with an X) and those that want to REMOVE the barricade and OPEN Tucker (green box with an O).

Where a number is shown in a circle is an address where more than one email was received from the same address.

I will provide copies of the same to the Traffic Committee members.

Thanks, Bill

From: dansemi [mailto:dansemi@yahoo.com]
Sent: Wednesday, July 15, 2015 2:45 PM
To: William J Huotari
Subject: Re: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).

Thank you Bill.....Dan

On Wednesday, July 15, 2015 2:38 PM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Dan, I'll see if I can put something together.

My GIS person is out so it may not be fancy.

Bill

From: dansemi [<mailto:dansemi@yahoo.com>]
Sent: Wednesday, July 15, 2015 2:29 PM
To: William J Huotari
Subject: Re: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).

Bill,

If I'm not asking for too much, can you please, tonight when you present the e-mails that were written **for and against** the barrier removal to also present a list to where these residents that expressed their feelings reside.

Interested in how many people are asking for the barrier to be removed and how far away they reside in relation to the barrier.

Much appreciated ...Dan

On Monday, July 13, 2015 9:59 PM, William J Huotari <HuotariWJ@troymt.gov> wrote:

Dan, the 300' radius is a minimum distance used for generating a mailing list and is used for sidewalk waivers which require a public hearing. We typically follow the same criteria for other items such as stop signs, etc. but can generate a list that covers a larger area depending on the issue and/or how roads, areas, etc. interact with the request.

For example, if a request were to come in for an item on a dead end street that exceeded 300', then the mailing list would typically be created for the entire street and not just within 300' of the subject area.

The Tucker item is a Regular Business item at the Traffic Committee meeting. The Traffic Committee will make a recommendation that will be forwarded to City Council.

The same item will also be sent to the Planning Commission at their meeting of August 11 for their consideration and recommendation.

The recommendations by the Traffic Committee and the Planning Commission will be forwarded to City Council at their meeting of August 24.

The mailing list was prepared to provide notice to residents in the area that the item was being discussed so that they would have the opportunity to provide their input. The mailing list included 322 properties if I remember correctly.

I hope that helps.

Thanks, Bill

Sent from my iPhone 6+

On Jul 13, 2015, at 8:15 PM, dansemi <dansemi@yahoo.com> wrote:

Bill,

I'm hoping you can help me shed some light into the following subject:

-- the mailers regarding the Wednesday hearing went out to a large number of homes in the North-East corner of the John R - Long Lake section area.

Searching the City of Troy web site regarding the City Ordinances in the City Code and Charter, the only thing that I can find out regarding the handling of the mailers for a hearing is the following:

35.05 Notice of Hearing. The Traffic Engineer shall submit the completed application, in addition to all documents relating to the sidewalk variance request, to the Traffic Committee. The Traffic Engineer shall also set the requested sidewalk variance request for a public hearing before the Traffic Committee. This public hearing shall be scheduled as soon as possible. The Traffic Engineer shall also give notice of the public hearing to discuss the requested sidewalk variance request to persons who are assessed for real **property within 300 feet of the subject premises, and to the occupants of single and two family dwellings within 300 feet of the subject premises**. The notice shall be delivered personally or by mail addressed to the respective owners and tenants at the address given on the last assessment roll. If the tenant's name is not known, the term occupant may be used.

The above paragraph talks about a 300 foot radius of the subject premises. I couldn't find any other ordinance that deals with this issue.

Was the above ordinance used in generating the mailing list ?

Is there another ordinance that I have missed that deals with other kind of hearings (like the one on Wed related to the barrier removal) ?

Can you please point me to the correct City of Troy ordinance that was used for generating the mailing list.

Your help is greatly appreciated.

Thank you....Dan

On Monday, July 13, 2015 11:07 AM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Dan, I'll put the request in for a laptop. There is a projector in the room.

The meeting is scheduled to be in the Lower Level Conference Room, but depending on the size of the crowd it may have to be moved to the Council Chambers which is a much larger room but not as conducive for a discussion.

Bring your laptop and if possible just place the presentation on a USB drive and it can be copied to the laptop that will already be set up.

Thanks, Bill

From: dansemi [<mailto:dansemi@yahoo.com>]

Sent: Monday, July 13, 2015 10:55 AM

To: William J Huotari

Subject: Re: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).

Mr Huotari,

Thank you for quick reply.

Have one more question/ request for you:

-- I would like to have a Powerpoint presentation for the Traffic Committee Members explaining the reasons why I don't see the removal of the barrier as a feasible solution. Questions for you:

1 - Is there a projector in the room where the meeting will take place ?

2 - Would I be able to hook up my laptop to your projector to display my presentation ?

Thank you...Dan

On Monday, July 13, 2015 6:00 AM, William J Huotari <HuotariWJ@troy.mi.gov> wrote:

Mr. Fratila, thank you for your email.

I will provide a copy of the same to the Traffic Committee members when they discuss the item.

Sincerely,

Bill Huotari, PE
Deputy City Engineer/Traffic Engineer
City of Troy

Sent from my iPhone 6+

On Jul 12, 2015, at 10:52 PM, dansemi <dansemi@yahoo.com> wrote:

Dear Mr Huotari,

I'm writing to you in regards to the proposed request to remove the barrier that is currently located on Tucker Drive.

I am strongly against the removal of the barrier due to safety concerns regarding the reduced visibility, limited line of sight at the intersection of Tucker Dr and John R Road.

South of Tucker Road there is a bridge over Gibson Drain that has a cement head and a large guardrail installed for safety reasons.

Turning left (South) on John R Road from the Tucker Dr. is always a challenge due the visibility issue.

There are at most 260 ft of clear line of sight visibility before deciding on proceeding with the left turn on John R Rd

At the currently posted speed limit of **45 mph** it takes a vehicle traveling northbound on John R Rd. only **1.1 seconds** to reach the Tucker Dr. intersection while the left turning vehicle from Tucker Dr. will need to clear the John R northbound and merge onto the southbound traffic lane. Accident reconstruction specialists use **1.5 seconds** as average driver reaction brake time.

This visibility issue combined with the exponential increase in traffic on Tucker Dr. (if the barrier is removed) will be a **clear endangerment of the health, safety and welfare of the immediate community surrounding Tucker**

Also, two additional openings have been opened in the last year (one currently available and the other one to be completed within the next year, when Hunter II development will be completed). These 2 new connections are Mayflower and Drake, that are each within 150 yards of each other and of Tucker Road.

- These 2 new connections should provide ample access (including backup access, if necessary) to all emergency vehicles to quickly and safely deploy the necessary resources to any residence in the neighborhood, therefore alleviating the concerns raised by the Police and Fire Departments

I would appreciate if you can inform the Traffic Committees Members of the above issue that I have brought to your attention in this e-mail and also to inform them that I'm strongly opposing the opening/ removal of the barrier. I would like to respectfully ask the Committee Members to vote:

b. RESOLVED, that NO CHANGE be made on Tucker Street, east of John R

Thank you very much for your attention,

Dan Fratila

2192 Tucker Dr.

William J Huotari

From: Keller, Chuck <ckeller@rcoc.org>
Sent: Wednesday, July 15, 2015 12:03 PM
To: William J Huotari; Sintkowski, Scott
Subject: RE: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Bill:

I had a call from Steve Dearing, OHM, yesterday inquiring about the same thing. I searched the files we have in the Traffic-Safety Department and couldn't find the mentioned report. If my memory serves me correctly, when the church proposed a driveway connection to Tucker, the residents were against it and a driveway was proposed out to John R Road. I believe it was the driveway out to John R Road that was the concern being located so close to the bridge. This is why the church driveway connects with Long Lake Road.

Looking at this location today, it appears that the approach of Tucker to John R Road is within current RCOC Guide for Corner Sight Distance requirements.

Chuck

From: William J Huotari [mailto:HuotariWJ@troymi.gov]
Sent: Wednesday, July 15, 2015 11:13 AM
To: Sintkowski, Scott; Keller, Chuck
Subject: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Scott & Chuck, we have an item on our Traffic Committee agenda tonight to discuss the removal of a barricade on Tucker, on the east side of John R, north of Long Lake.

I have heard reference to a "report" from the RCOC regarding limited site distance at the end of Tucker and/or related to a proposed driveway from the church to John R back in 1996/1997.

Would either of you have a copy of the report, memo, whatever that might shed some light on this claim? I have asked for a copy of the report from the resident but they have not provided anything to date.

Thanks, Bill

This was from one of the emails that I received prior to the meeting tonight:

1. There is limited site distance at the end of our street as determined by the Oakland County Road Commission report dated May 2, 1996. This is due to the concrete barrier over the Gibson Drain located south of Tucker Drive on John R.

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

William J Huotari

From: Rachele Lyngklip <lyngklipr@aol.com>
Sent: Wednesday, July 15, 2015 11:24 AM
To: William J Huotari
Subject: Tucker Dr.

I am writing to voice my opposition to the opening of the barrier on Tucker Dr in Troy Mi. There is no change that necessitates the opening of this Street. This street has been closed the entire time I have lived on Tucker without incident. I challenge the legal standing that the couple that lives on Drake to initiate this request with the city. I don't understand what interest that they have that would justify their actions in initiating this matter. There are currently two additional entryways to John R road that are open (Mayfield) and Duke that allow access from the new developments. We do not have any new building or developments that necessitates the additional access to John R. In addition, East Tucker is not paved and there is no sidewalks. Tons of kids use this street to walk or ride their bikes to 7 Eleven. It would be very dangerous to open the barrier to cars without the city also putting in sidewalks. It would be a lawsuit waiting to happen with the severe consequence being a pedestrian injury or death. In addition, the intersection of John R and Long Lake is very busy with frequent accidents. Opening Tucker (a street so close in proximity with the Long Lake and John R intersection) would simply make traffic issues and traffic safety more of a concern. For these reasons, I am strongly against the opening of the barrier on Tucker Dr. Please feel free to contact me with any additional questions.

Sincerely,

Rachele and Pete Lyngklip
2262 Tucker Dr. Troy MI

Rachele Lyngklip

CAbi Fashion Consultant

phone: (248)506-0878

email: lyngklipr@aol.com

view the current season at www.rachelelyngklip.cabionline.com

for the latest fashion fun, visit the CAbi blog at www.cabionline.com/blog



William J Huotari

From: Adriana Apahidean <adrianadean@comcast.net>
Sent: Wednesday, July 15, 2015 11:16 AM
To: William J Huotari
Subject: Tucker Barrier- opposed to removal

Hello Mr. Huotari:

Just a short note that I too am opposed to the removal of the barrier on Tucker Dr.

This issue requires a thorough traffic study and cost assessment. I urge the Traffic Committee to make an educated and well researched decision.

Thank you kindly,

Adriana Apahidean
2223 Tucker Dr.
Troy, Mi

Sent from my iPhone

William J Huotari

From: Nalette, Lisa <LNALETT1@hfhs.org>
Sent: Wednesday, July 15, 2015 9:48 AM
To: William J Huotari
Subject: Tucker street barrier

Hi,

Sorry that this is last minute. I would like to raise my concerns about NOT opening Tucker to Long Lake Meadows subdivision. I hope you can still consider my thoughts in the planning.

I live at 5344 Standish and have been at that residence since 1992 when it was opened. For years, the residents of Standish lived without an open street. There was no quick access for ambulances or fire trucks. Requests to open Tucker were denied. We used to joke that someone on the old Tucker had a friend in the Troy government. Now that Standish is open to Mayflower it is open season for drive thru's. The traffic on my street has increased ten fold as you might imagine. I would like to see Tucker opened now because:

a: why should it not be open? I lived on Finch Road in Troy which was dirt then and the city had no problem opening us up to the subdivision to our South back in the 70's. We remained a dirt road for at least another twenty years even though our traffic increased from the Huntsford subdivision. What makes Tucker so special that it cannot be opened?

B: I would like to see the entry into Long Lake Meadows subdivision from John R be shared not only by the Mayflower access but also Tucker. This will ease some of the burden on Standish.

Thank you. Lisa Taylor-Nalette 5344 Standish Troy 248 701 1348

CONFIDENTIALITY NOTICE: This email contains information from the sender that may be CONFIDENTIAL, LEGALLY PRIVILEGED, PROPRIETARY or otherwise protected from disclosure. This email is intended for use only by the person or entity to whom it is addressed. If you are not the intended recipient, any use, disclosure, copying, distribution, printing, or any action taken in reliance on the contents of this email, is strictly prohibited. If you received this email in error, please contact the sending party by reply email, delete the email from your computer system and shred any paper copies.

Note to Patients: There are a number of risks you should consider before using e-mail to communicate with us. See our Privacy & Security page on www.henryford.com for more detailed information as well as information concerning MyChart, our new patient portal. If you do not believe that our policy gives you the privacy and security protection you need, do not send e-mail or Internet communications to us.

William J Huotari

From: Mykola Murskyj <mmurskyj@gmail.com>
Sent: Tuesday, July 14, 2015 8:21 PM
To: William J Huotari
Subject: Opening Tucker

Attention: Mr. Huotari, Traffic Engineer

I have been a resident in the Long Lake Meadows subdivision for over twenty years and our street intersects with Tucker.

In response to your notice of June 30th, I have waited a very long time and would sincerely appreciate the simple barrier removal on Tucker. This would effectively enhance response time for all emergency vehicles, greatly remove resident concerns and is the most economical method for resolving this issue.

Please open up Tucker and keep me updated with any future developments.

Thanks,

Mykola

William J Huotari

From: barb northam <waba59@wowway.com>
Sent: Tuesday, July 14, 2015 5:01 PM
To: William J Huotari
Subject: Barricade on Tucker

My name is Barbara Northam and I live at 5241 Standish. Recently I received a notice regarding the removal of the barricade on Tucker. As long as Tucker remains a dirt road on the west side of the barricade, I feel that traffic in that area should NOT be increased by removal of the barrier. We currently have another exit from our sub on the north and I understand that a paved Drake will eventually be opened to John R. With that street then being accessible via two other streets, I see no need to open a dirt road to more traffic. Thank you for your consideration in this matter.

Barbara J. Northam

--

--

WOW! Homepage (<http://www.wowway.com>)

William J Huotari

From: Nick Vendittelli <nvendittelli@wowway.com>
Sent: Tuesday, July 14, 2015 10:12 AM
To: William J Huotari
Subject: July 15, 2015 Meeting Agenda item Remove Barricade on Tucker

Thank you for the notice asking for our input in regards to removing the barricade on Tucker. I will not be able to attend the meeting, but would like to provide my comments. I am not in favor of removing the barricade. We have live on Standish for over 24 years, one of the original owners. For all these years we have not had Tucker open, and we are used to it. We now have Mayflower open to John R and my understanding is that Drake will also open on to John R. So we had no access to John R for 24 years and now will have two roads connecting to John R, which is more than enough for the amount of traffic and makes it unnecessary to have Tucker opened as well. From a financial point the added expense to the residences on Tucker if the road were to be paved or to the city for the multiple regrading and gravel of Tucker during a year which will be required if it were to remain a gravel road is a waste of money, with Mayflower and then Drake, both paved, being opened. To me the expense either way is not justified over a perceived traffic flow issue.

Sincerely

Nick Vendittelli

5132 Standish, Troy

William J Huotari

From: Gerry Seip <justgerrya@yahoo.com>
Sent: Tuesday, July 14, 2015 10:10 AM
To: William J Huotari
Subject: Tucker Barricade

We are residents at 5297 Standish Dr. at the corner of Drake. We have lived in our home since it was built in 1991. We are very much in favor of the Tucker barricade being removed for the following reasons: 1) it would be consistent with the opening of all stub streets to the main roads 2) it allows a very clear access to the Long Lake Meadows subdivision for the reasons stated by the Fire Chief, Police Chief, etc. 3) it will alleviate traffic flow through Mayflower and Drake (when it is opened) and give residents a third access from John R. We are unable to attend the meeting, however, we would like you to consider our thoughts when voting on this matter. Also, it is our opinion that it would be economical for the city to pave Tucker at the same time that John R is being widened. Thank you for your consideration, Chuck & Gerry Seip

William J Huotari

From: Dennis Angelo <blitz96.da@gmail.com>
Sent: Tuesday, July 14, 2015 7:32 AM
To: William J Huotari
Subject: Please keep Tucker Dr. closed

Hello Mr. Huotari -

My name is Dennis Angelo, and I live on Crowfoot Drive, which connects to Saffron in the Long Lake Meadows subdivision. My family and I have lived here for eleven years, and I am familiar with the traffic flow in this area.

Based on what I have seen and know, I strongly recommend that you do your part to keep Tucker Drive from opening up for through traffic. There is already heavy traffic on Saffron, and drivers travel too fast much of the time. A good friend of mine, Jim Davis was living on Saffron, and witnessed a vehicle traveling too fast, losing control, and driving right through several front yards on this busy street. We have several children and dogs in this area.

Allowing more traffic to pass through our subdivision by opening up Tucker is a bad idea.

Thank you,
Dennis Angelo
Master of Science, Mechanical Engineering
(248) 979-8502

William J Huotari

From: petrulupas <petrulupas@yahoo.com>
Sent: Tuesday, July 14, 2015 7:16 AM
To: William J Huotari
Subject: Fwd: Tucker dr Troy

Hello,

My name is Petru Lupas and I have lived on Tucker street for 20 years. I am against opening the barrier at the end of Tucker for the following reasons.

1. Tucker Rd. is unpaved, so it often needs to be leveled. If the barrier were to open this leveling service will have to be done more frequently.

2. Those who come from the subdivision will complain to the city as cars will get dirty on the unpaved street .Also the sand and gravel will be carried on to the paved street.

3. There is very reduced visibility from Tucker to John R. The guard rail of the bridge cuts visibility to traffic .If barrier were to open a traffic light will have to be placed at the exit of Tucker onto John R.

Now in hours of traffic we must wait 3-5 minutes to enter on to the John R.

What happen if you open the barrier ? 10 or more cars will be waiting to enter John R rd.

4. If you open the barrier and do not pave the street , you put us in danger, all of us who live at the entrance of unpaved zone .At high speed the cars are sliding the same on sand as snow.

Conclusion: If you open the barrier you must pave the street and put a stop light at your cost.

My best regards

Petru Lupas

2197 Tucker dr
Troy MI 48085
Tell 248-835-3844

William J Huotari

From: dansemi <dansemi@yahoo.com>
Sent: Monday, July 13, 2015 11:15 PM
To: William J Huotari
Cc: Mark F Miller; Brent Savidant
Subject: Re: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).

Bill,

Thank you very much for your quick reply.

I am very surprised to find out (if I understand it correctly) that there is **no City Ordinance that deals** with the mailing area for a particular hearing.

To expand a little bit on your point about Tucker Dr., the length of the gravel road is around 1000 ft.

The mailer contained 322 addresses and was reaching as far as Sweet St. which is over 1800 ft radius from the actual barrier (it is far enough from Tucker that most people don't even know where Sweet St is located in reference to Tucker !). There are also other streets that are closer to Tucker than Sweet St and have not been included in the mailer.

I am also very disappointed that "a resident" can make what it is labeled a "formal complaint" that can generate so much work, tying up resources from multiple departments and not to mention costly - money spent on mailers (which according to your note would happen at least 3 times for this particular issue - Traffic, Planning and then Council) which will amount to over \$450.

Thank you....Dan

On Monday, July 13, 2015 9:58 PM, William J Huotari wrote:

Dan, the 300' radius is a minimum distance used for generating a mailing list and is used for sidewalk waivers which require a public hearing. We typically follow the same criteria for other items such as stop signs, etc. but can generate a list that covers a larger area depending on the issue and/or how roads, areas, etc. interact with the request.

For example, if a request were to come in for an item on a dead end street that exceeded 300', then the mailing list would typically be created for the entire street and not just within 300' of the subject area.

The Tucker item is a Regular Business item at the Traffic Committee meeting. The Traffic Committee will make a recommendation that will be forwarded to City Council.

The same item will also be sent to the Planning Commission at their meeting of August 11 for their consideration and recommendation.

The recommendations by the Traffic Committee and the Planning Commission will be forwarded to City Council at their meeting of August 24.

The mailing list was prepared to provide notice to residents in the area that the item was being discussed so that they would have the opportunity to provide their input. The mailing list included 322 properties if I remember correctly.

I hope that helps.

Thanks, Bill

Sent from my iPhone 6+

On Jul 13, 2015, at 8:15 PM, dansemi <dansemi@yahoo.com> wrote:

Bill,

I'm hoping you can help me shed some light into the following subject:

-- the mailers regarding the Wednesday hearing went out to a large number of homes in the North-East corner of the John R - Long Lake section area.

Searching the City of Troy web site regarding the City Ordinances in the City Code and Charter, the

only thing that I can find out regarding the handling of the mailers for a hearing is the following:

35.05 Notice of Hearing. The Traffic Engineer shall submit the completed application, in addition to all documents relating to the sidewalk variance request, to the Traffic Committee. The Traffic Engineer shall also set the requested sidewalk variance request for a public hearing before the Traffic Committee. This public hearing shall be scheduled as soon as possible. The Traffic Engineer shall also give notice of the public hearing to discuss the requested sidewalk variance request to persons who are assessed for real **property within 300 feet of the subject premises, and to the occupants of single and two family dwellings within 300 feet of the subject premises**. The notice shall be delivered personally or by mail addressed to the respective owners and tenants at the address given on the last assessment roll. If the tenant's name is not known, the term occupant may be used.

The above paragraph talks about a 300 foot radius of the subject premises. I couldn't find any other ordinance that deals with this issue.

Was the above ordinance used in generating the mailing list ?

Is there another ordinance that I have missed that deals with other kind of hearings (like the one on Wed related to the barrier removal) ?

Can you please point me to the correct City of Troy ordinance that was used for generating the mailing list.

Your help is greatly appreciated.

Thank you....Dan

On Monday, July 13, 2015 11:07 AM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Dan, I'll put the request in for a laptop. There is a projector in the room. The meeting is scheduled to be in the Lower Level Conference Room, but depending on the size of the crowd it may have to be moved to the Council Chambers which is a much larger room but not as conducive for a discussion. Bring your laptop and if possible just place the presentation on a USB drive and it can be copied to the laptop that will already be set up.
Thanks, Bill

From: dansemi [<mailto:dansemi@yahoo.com>]

Sent: Monday, July 13, 2015 10:55 AM

To: William J Huotari

Subject: Re: July 15, 2015 – City of Troy, Traffic Committee meeting to Request to Discuss Interconnection – Tucker, John R to Standish (Agenda item #7).

Mr Huotari,

Thank you for quick reply.

Have one more question/ request for you:

-- I would like to have a Powerpoint presentation for the Traffic Committee Members explaining the reasons why I don't see the removal of the barrier as a feasible solution.

Questions for you:

1 - Is there a projector in the room where the meeting will take place ?

2 - Would I be able to hook up my laptop to your projector to display my presentation ?

Thank you...Dan

On Monday, July 13, 2015 6:00 AM, William J Huotari <HuotariWJ@troymt.gov> wrote:

Mr. Fratila, thank you for your email.

I will provide a copy of the same to the Traffic Committee members when they discuss the item.

Sincerely,

Bill Huotari, PE

Deputy City Engineer/Traffic Engineer

City of Troy

Sent from my iPhone 6+

On Jul 12, 2015, at 10:52 PM, dansemi <dansemi@yahoo.com> wrote:

Dear Mr Huotari,

I'm writing to you in regards to the proposed request to remove the barrier that is currently located on Tucker Drive.

I am strongly against the removal of the barrier due to safety concerns regarding the reduced visibility, limited line of sight at the intersection of Tucker Dr and John R Road.

South of Tucker Road there is a bridge over Gibson Drain that has a cement head and a large guardrail installed for safety reasons.

Turning left (South) on John R Road from the Tucker Dr. is always a challenge due the visibility issue.

There are at most 260 ft of clear line of sight visibility before deciding on proceeding with the left turn on John R Rd

At the currently posted speed limit of **45 mph** it takes a vehicle traveling northbound

on John R Rd. only **1.1 seconds** to reach the Tucker Dr. intersection while the left turning vehicle from Tucker Dr. will need to clear the John R northbound and merge onto the southbound traffic lane. Accident reconstruction specialists use **1.5 seconds** as average driver reaction brake time.

This visibility issue combined with the exponential increase in traffic on Tucker Dr. (if the barrier is removed) will be a **clear endangerment of the health, safety and welfare of the immediate community surrounding Tucker**

Also, two additional openings have been opened in the last year (one currently available and the other one to be completed within the next year, when Hunter II development will be completed). These 2 new connections are Mayflower and Drake, that are each within 150 yards of each other and of Tucker Road.

- These 2 new connections should provide ample access (including backup access, if necessary) to all emergency vehicles to quickly and safely deploy the necessary resources to any residence in the neighborhood, therefore alleviating the concerns raised by the Police and Fire Departments

I would appreciate if you can inform the Traffic Committees Members of the above issue that I have brought to your attention in this e-mail and

also to inform them that I'm strongly opposing the opening/ removal of the barrier. I would like to respectfully ask the Committee Members to vote:

b. RESOLVED, that NO

CHANGE be made on Tucker

Street, east of John R

Thank you very much for your attention,

Dan Fratila

2192 Tucker Dr.

William J Huotari

From: Sandy Isaacs <sisaac@trinitydavison.org>
Sent: Monday, July 13, 2015 9:18 PM
To: William J Huotari
Subject: Tucker Road Barrier Issue

Elaine Wolf

2150 Tucker Rd

Troy, MI 48085

July 13, 2015

Mr. William J. Huotari, P.E.

Deputy City Engineer/Traffic Engineer

City of Troy

500 W. Big Beaver Rd.

Troy, MI 48084

RE: Tucker Road Barrier Issue

Mr. Huotari,

I am writing to advise you that I am strongly opposed to the removal of the Tucker Road barrier that is on the agenda for discussion at the July 15, 2015 Traffic Committee regular meeting.

When I purchased my home on Tucker Road in the 1970's, I chose the property because of the rural appeal, beautiful surrounding nature and wildlife, the quietness, seclusion and privacy. It was "country living" in the city; the perfect place to raise a family. If I wanted to live in a subdivision, with houses built so close together and high traffic, I would've moved to a subdivision. But I didn't. I chose my little piece of country on 2.5 acres on Tucker Road 30+ years ago. Many generations of my family and I have enjoyed this peaceful property for many, many years and it is my hope that we will be able to continue doing so.

They have developed, and are continuing to develop, every inch of land surrounding Tucker Road. Removing the barrier would only create an abundance of traffic on our nice, quiet road. Please don't take the last bit of peace and quiet the residents of Tucker Road have left by removing the barrier.

Thank you for your time and consideration regarding my concerns.

Sincerely,

Elaine Wolf
(248) 835-9520

William J Huotari

From: Jim White <jwhite13453@gmail.com>
Sent: Monday, July 13, 2015 7:47 PM
To: William J Huotari
Subject: Tucker Road barrier

Bill Huotari

My wife and I are opposed to removal of the Tucker Road barrier. We have been residents at 2333 Tucker since the 1st of October 1990 . We already have enough traffic on Tucker without adding a direct entrance off John R Road. We have enough entrances off of Long Lake and John R roads without adding another one.

Thank You

Jim & Kathy White
2333 Tucker (east end of tucker)
Troy, Mi 48085
(248) 879-9116

William J Huotari

From: ivanna Murskyj <imurskyj@gmail.com>
Sent: Monday, July 13, 2015 5:05 PM
To: William J Huotari
Subject: Tucker barrier

To whom it may concern,

I have been a resident of the Long Lake Meadows neighborhood for my entire life and I would be thrilled to see the removal of the barrier on Tucker. This would allow more access to the neighborhood by emergency vehicles and would improve resident access to their homes. This would greatly improve the safety of everyone in our neighborhood and reduce traffic in other areas. I would really like to see the barrier at the end of Tucker removed.

Thank you for your time,

Ivanna Murskyj

William J Huotari

From: Tim Monck <tmonck@gmail.com>
Sent: Monday, July 13, 2015 4:22 PM
To: William J Huotari
Subject: Request to Remove Barricade on Tucker

Mr. Huotari,

Unfortunately I will be out of town during the traffic committee meeting scheduled to discuss this proposed change. As a city resident living in proximity to the proposed barricade removal, I am **STRONGLY AGAINST** this change. As you know a new access point to the subdivision (Mayflower) was created this past year from John R. This allows emergency vehicles access to not only the new subdivisions being built but also Long Lake and Spring Meadows. There will also be another access created off of John R (Kingston Drive, with Drake opening to Standish). With these new roads there is ample and improved access throughout the subdivisions in this area, with only a slightly reduced response time in the southwest compared to opening Tucker. While every second counts in an emergency situation, the benefit would be minimal, especially coming from the north (we are in Station 5's area of responsibility).

The primary benefit to opening Tucker appears to be to create a convenient route to John R for the Long Lake subdivision. This will increase the amount of traffic on Tucker, as well as the speeds at which vehicles travel. We are currently seeing the impact of Standish being opened to Mayflower with higher speeds and heavier traffic flows now. I do not believe the increased risk to small children playing/pedestrians walking in the area as well as a decline in the quality of life for residents is worth the minor convenience removing the barricade would provide. While some area residents not living close to Tucker or Standish will be in favor of this change as it would be convenient and would not negatively impact them, please give careful consideration to those residents who would be most impacted. Taking an informal poll of my neighbors I have found no one in favor of removing the barricade.

Thank you in advance for your consideration.

Sincerely,

Tim Monck
5228 Standish Drive
Troy, MI 48085

William J Huotari

From: Deb Tosch <mstgarden@gmail.com>
Sent: Monday, July 13, 2015 2:16 PM
To: William J Huotari
Cc: John Tosch
Subject: Resident Request to Open Tucker Drive
Attachments: Endicott-Oakwood.JPG; Ruby-Colleen.JPG; Mayflower-Sweet.JPG

Dear Mr. Houtari

We are Deb and John Tosch from 2088 Tucker Drive and we are adamantly against the removal of the barricade separating us from the subdivision.

Our reasons are as follows:

1. There is limited site distance at the end of our street as determined by the Oakland County Road Commission report dated May 2, 1996. This is due to the concrete barrier over the Gibson Drain located south of Tucker Drive on John R.
2. The City Council already reviewed this issue in 1987 and determined Tucker Drive should remain closed until such time as further development on Tucker occurs. There has been no further development to warrant the removal of the barrier.
3. The City's Policy of interconnectivity is a very inconsistent. We have three such inconsistencies in the square mile of the subject street.

The first is Endicott to Oakwood in the northwest corner. This was probably thought best because the traffic to bypass the light at Square Lake would have been severe. However, the streets are curved which would have slowed traffic considerably.

The second is Ruby to Colleen in the northeast corner. These two streets were not connected which is a mystery to us since neither is close to a main road. These streets also have curves which would have slowed traffic as well. Instead of connecting, this is an EVA only.

Third is Sweet to Mayflower which is part of the Bridgewater Estates development. Instead of opening Sweet to Mayflower as well as to Standish, this connection was not made. We can only assume (which is not always a good idea) the reasoning behind opening Standish and not Sweet as well is the distance of paved straight road which probably would have cause excessive speeds for the subdivision.

How do we know this? We were residents on Highbury for over 18 years. We petitioned the city for a stop sign at Endicott several times to slow the drivers down. Highbury is the first main entrance to the subdivision south of Square Lake and has a relatively long straight stretch before it curves. Our dog was hit by a racing driver. We are thankful that none of our children were. We decided to move to Tucker 22 years ago to get away from the street traffic with the added benefit that our children would remain in the same schools.

Tucker Drive is a quarter mile from a major intersection. Traffic going north and south during rush hours is tremendous. Opening up Tucker Drive will provide over a quarter mile of straight road, much longer than the one on Highbury. This street was not designed to be an entrance to a large developed subdivision.

One last point we would like to make refers to the comment that gravel roads are costly to maintain. According to a study published by NPR(National Public Radio) News on October 26, 2010, the cost to maintain a paved road over the entire life cycle is about 3 times more than maintaining a gravel road. Looking at the cost on a year to year basis is short sighted. "Over a 40 year lifespan, a low-volume paved road will need to be chip sealed twice, undergo overlay once and then reclaim/overlay. The yearly maintenance costs of gravel roads make them appear inefficient until you consider the capital improvement costs associated with bituminous roads at the middle and end of their lives, which isn't always reflected in yearly maintenance figures." The study was for streets in Minnesota north of the Twin Cities. Their climate is similar with cold winters; however, they do not have the freeze-thaw cycles that we have that shortens the lifespan of the roads in Southeast Michigan. A report by the Wall Street Journal also reported that "In Michigan, at least 38 of the 83 counties have converted some asphalt roads to gravel in recent years" due in part to the escalating expense of maintenance.

In conclusion, the request is being made by a resident of Drake who have lost their dead end. Drake is a short street with only 2 residents. The connection that is being made will have curves slowing traffic on both ends of traffic flow. Opening Tucker Drive will be a safety hazard as inconsiderate drivers will have a straight quarter mile shot into and out of the neighborhood as well as the hazard of the limited site distance while turning left on to John R.

If fire and public safety is at issue, then another solution would be to make the barricade into a EVA.

Sincerely,

John and Deb Tosch

William J Huotari

From: dansemi <dansemi@yahoo.com>
Sent: Monday, July 13, 2015 1:10 PM
To: William J Huotari
Subject: Re: Request for people with disability access to the Traffic Committee Meeting on coming Wed 7/15/2015

Sounds good, thank you Bill.....Dan

On Monday, July 13, 2015 1:03 PM, William J Huotari wrote:

Dan, the meeting is scheduled for the Lower Level Conference Room in Troy City Hall, which is on the main floor and accessible from the main parking lot on the east side of the building. If the attendance of the public at the meeting exceeds a comfortable level, then we may try to relocate to City Council Chambers, which is on the 2nd floor, but accessible by elevator.

Thanks, Bill

From: dansemi [mailto:dansemi@yahoo.com]
Sent: Monday, July 13, 2015 11:53 AM
To: William J Huotari
Subject: Request for people with disability access to the Traffic Committee Meeting on coming Wed 7/15/2015

Mr. Huotari,

I'm writing to you on behalf of my neighbor Cynthia Veggian, resident at 2155 Tucker Dr.

Mrs Veggian is currently 100% dependent on an oxygen tube and use a walker to move around.

She asked me to request that the City schedules this Traffic Committee meeting in a Room that will be accessible for her.

She wants to personally participate to this meeting and express her opposition to the removal of the Tucker barrier.

Thank you very much for your assistance with this issue....Dan
Fratila

William J Huotari

From: Buchanan <mbuchanan@wideopenwest.com>
Sent: Monday, July 13, 2015 1:05 PM
To: William J Huotari
Subject: Tucker Dr. request

Dear Mr. Huotari,

I am against removing the barrier on Tucker Dr.

With the addition of 2 new connectors to John R, Mayflower which is currently open and Drake due to open in the future I see no need for the removal.

Thank you for your time,

Michael Buchanan

2314 Tucker Dr.

Troy, Mi 448085

William J Huotari

From: semidaf@yahoo.com
Sent: Monday, July 13, 2015 12:41 PM
To: William J Huotari
Subject: Re: Traffic Committee Meeting - removal of Tucker barrier opposition

Bill,
That would be perfect. Thank you very much for all your help and assistance....Semida

On Jul 13, 2015, at 12:04 PM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Semida, I will provide a copy of your email to the Traffic Committee members along with the Q&A's from your other emails as information as well as opposition to the removal.
Thank you for your input.
Sincerely,

Bill

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov 

“We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best.”

From: semidaf@yahoo.com [<mailto:semidaf@yahoo.com>]

Sent: Monday, July 13, 2015 12:03 PM

To: William J Huotari

Subject: Traffic Committee Meeting - removal of Tucker barrier opposition

Dear Mr. Huotari,

I'm writing in regards to the Tucker Road barrier and the desire of the City of Troy to removing this barrier.

I would like to express my strong opposition about removing the barrier on Tucker Road.

I have a very serious visibility concern with the intersection of Tucker and John R.

Trying to turn left (South) on John R can be a challenge at times (to say the least) due to the bridge safety rails that exist south of Tucker Road.

Not to make it too long, but I was personally pulled over by a Troy Police officer and told that I didn't stop at the STOP sign posted at the end of Tucker and that in fact "I rolled through the stop sign". I explained to the officer that I did perform a FULL stop at the STOP sign and that he just couldn't see me due to the visibility issue that I pointed out to him. When he looked around and understood what I was talking about he allowed me to go without issuing a Citation or even a Warning.

Opening Tucker will just increase the amount of traffic that will try to use it as a shortcut and will only increase the risk associated with the low visibility while turning left.

I believe that the safety, security and welfare of our local Tucker community will be but in jeopardy by allowing the traffic to increase on Tucker by removing the barrier.

I would also like to mention that for the last 27 years there was no access (zero entrances) from John R to the Subdivisions East of John R. At this point we have Mayflower and soon to be constructed Kingston Dr. that will connect to Drake and will become the second access street to the above mentioned Subdivisions.

Please submit to the Traffic Committee Members my strong opposition to the removal of the Tucker barrier.

Sincerely,
Semida Fratila
[2192 Tucker Dr.](#)

William J Huotari

From: semidaf@yahoo.com
Sent: Monday, July 13, 2015 12:40 PM
To: Kurt Bovensiep
Cc: Brian M Kischnick; Mark F Miller; Timothy L Richnak; Steven J Vandette; William J Huotari; Brent Savidant
Subject: Re: Follow up to the phone conversation regarding gravel roads in Troy

Kurt,
Thank you very much for the information you've provided. It is exactly what I was looking for. Thanks again...Semida

On Jul 13, 2015, at 10:19 AM, Kurt Bovensiep <K.Bovensiep@troymi.gov> wrote:

Ms. Fratila,

Please see the highlighted answers to your questions below.

Kurt Bovensiep | Public Works Manager

City of Troy | 4693 Rochester Rd., Troy, MI 48085 | Office: 248-524-3489 | Cell: 248-885-1953
troymi.gov  

“We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best.”

From: semidaf@yahoo.com [<mailto:semidaf@yahoo.com>]

Sent: Monday, July 13, 2015 9:00 AM

To: Kurt Bovensiep

Subject: Follow up to the phone conversation regarding gravel roads in Troy

Dear Mr. Bovensiep,

Thank you very much for you calling back [on Friday 7/20/2015](#) and providing the gravel road maintenance numbers for Tucker Dr.

Just to recap:

- the total amount for gravel roads maintenance for the 2015 Fiscal Year came in at \$70,000.
- TPW maintains 4.4 miles of gravel roads in Troy.
- Tucker Dr. is roughly 1127 ft (0.22 miles) which will amount to \$3500/ yr for Tucker Rd maintenance.

I would have 4 more questions on the same road maintenance subject:

1 - How many miles of concrete roads is DPW responsible for maintaining ? Mainly interested in the secondary/ subdivision type roads.

264.42 miles of local roads.

2 - What was the final budget amount for the 2015 Fiscal Year for the above roads maintenance?

FY 2015 estimate for maintenance only- \$463,000 (does not include drains maintenance or snow, very similar to what I gave you for gravel roads)

3 - Also, does the above amount cover just the concrete roads maintenance (hole patching, etc) or does it also include the concrete reconstruction/replacement projects (like it was the case on Highbury and other secondary streets this Summer) ? Would you happen to know the figure for the road reconstruction projects for secondary roads (i.e. Subdivision type roads) - if it's easy to isolate/ separate from the actual budget ?

The \$463,000 is just maintenance. The replacement or reconstruction of concrete local roads is roughly \$2.5 million a year.

4 - And finally, what was the actual versus projected TPW budget ? I can see what the 2015 estimated amount is (\$5.76 mil) vs 2015 amended (\$6.273 mil).

Not sure what document you are looking at but actual 2015 numbers is what we expensed. Since or fiscal year ended June 30 it takes several months to process all the invoices and expenses so an official number is not available until fall. Estimated amounts are where I project we will be at on June 30. Sometimes this takes requires an amendment to the budget if I am over budget. Projections are made 4-5 months before June 30 so it can be a little off.

Thank you very much for your help,
Semida Fratila
2192 Tucker Dr.

William J Huotari

From: William J Huotari
Sent: Monday, July 13, 2015 12:03 PM
To: 'semidaf@yahoo.com'
Subject: RE: EVAs and collapsible barricades in the City or Troy
Attachments: Local Road Paving Detail Rev11-07.pdf

Semida, our Standard Detail Sheet for Local Road paving has a "REMARKS" date of 10/15/2007 for "Add EVA detail and general updates", so that would be when we standardized the detail (copy attached).

What the date of the first installation of any EVA, I am not sure. I spoke with one of our engineer's that does a lot of the residential subdivision/site condominium reviews and he believes the first EVA was installed in around 2002.

This was part of the West Oaks 1 & 2 development and originally was to be a proposed chain link fence with a gate for emergency vehicle access. I don't recall if this was ever actually installed as there was a time when temporary (water filled) barricades were placed prior to the EVA's that sit there now.

I have not heard back from our Fire Dept. on how many times they have been replaced as they have been around for less than 15 years. I did check with our DPW and they did not have a specific number of times, but stated that they are "only aware of replacing them because of accidents or snow removal in the last three years".

Thanks, Bill

-----Original Message-----

From: semidaf@yahoo.com [mailto:semidaf@yahoo.com]
Sent: Friday, July 10, 2015 3:13 PM
To: William J Huotari
Subject: Re: EVAs and collapsible barricades in the City or Troy

Hi Bill,

Thank you for your quick reply.

Hopefully my last 2 question before the Wednesday's hearing:

- when(what year) did Troy start installing EVAs;
- how many times where the EVAs actually used - let's say in the last 10-20 years. Usage defined in terms of having to replace the wooden posts.

Much appreciated,

Semida

> On Jul 9, 2015, at 3:17 PM, William J Huotari <HuotariWJ@troymi.gov> wrote:

>
> Semida, I am not aware of any of those types of barriers in Troy.
>
> The EVA's that we install are of the wood post variety with holes drilled for breakaway.
>
> Thanks, Bill
>
> -----Original Message-----
> From: semidaf@yahoo.com [mailto:semidaf@yahoo.com]
> Sent: Thursday, July 09, 2015 3:16 PM
> To: William J Huotari
> Cc: semidaf@yahoo.com
> Subject: Re: EVAs and collapsible barricades in the City or Troy
>
> Bill,
> Thank you very much for the information you have provided. It is perfect.
>
> In regards to the collapsible barrier subject:
> - is there any barrier within the city of Troy that looks like a permanent barrier that is actually collapsible when you push on the barrier and try to drive over it. It can either be pylons with collapsible shear plates at the bottom or a continuous solid barrier - guard rail type - that has the legs collapsible and you can push the whole barrier down and drive over it.
> These are in fact EVA type barriers that look like permanent/ solid ones.
> City of Troy might not have any barriers of this type,
>
>
> Thanks again,
>
> Semida

William J Huotari

From: semidaf@yahoo.com
Sent: Monday, July 13, 2015 12:03 PM
To: William J Huotari
Subject: Traffic Committee Meeting - removal of Tucker barrier opposition

Dear Mr. Huotari,

I'm writing in regards to the Tucker Road barrier and the desire of the City of Troy to removing this barrier. I would like to express my strong opposition about removing the barrier on Tucker Road.

I have a very serious visibility concern with the intersection of Tucker and John R.

Trying to turn left (South) on John R can be a challenge at times (to say the least) due to the bridge safety rails that exist south of Tucker Road.

Not to make it too long, but I was personally pulled over by a Troy Police officer and told that I didn't stop at the STOP sign posted at the end of Tucker and that in fact "I rolled through the stop sign". I explained to the officer that I did perform a FULL stop at the STOP sign and that he just couldn't see me due to the visibility issue that I pointed out to him. When he looked around and understood what I was talking about he allowed me to go without issuing a Citation or even a Warning.

Opening Tucker will just increase the amount of traffic that will try to use it as a shortcut and will only increase the risk associated with the low visibility while turning left.

I believe that the safety, security and welfare of our local Tucker community will be but in jeopardy by allowing the traffic to increase on Tucker by removing the barrier.

I would also like to mention that for the last 27 years there was no access (zero entrances) from John R to the Subdivisions East of John R. At this point we have Mayflower and soon to be constructed Kingston Dr. that will connect to Drake and will become the second access street to the above mentioned Subdivisions.

Please submit to the Traffic Committee Members my strong opposition to the removal of the Tucker barrier.

Sincerely,
Semida Fratila
[2192 Tucker Dr.](#)

William J Huotari

From: Kurt Bovensiep
Sent: Monday, July 13, 2015 10:19 AM
To: 'semidaf@yahoo.com'
Cc: Brian M Kischnick; Mark F Miller; Timothy L Richnak; Steven J Vandette; William J Huotari; Brent Savidant
Subject: RE: Follow up to the phone conversation regarding gravel roads in Troy

Ms. Fratila,

Please see the highlighted answers to your questions below.

Kurt Bovensiep | Public Works Manager

City of Troy | 4693 Rochester Rd., Troy, MI 48085 | Office: 248-524-3489 | Cell: 248-885-1953 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

From: semidaf@yahoo.com [mailto:semidaf@yahoo.com]
Sent: Monday, July 13, 2015 9:00 AM
To: Kurt Bovensiep
Subject: Follow up to the phone conversation regarding gravel roads in Troy

Dear Mr. Bovensiep,

Thank you very much for you calling back [on Friday 7/20/2015](#) and providing the gravel road maintenance numbers for Tucker Dr.

Just to recap:

- the total amount for gravel roads maintenance for the 2015 Fiscal Year came in at \$70,000.
- TPW maintains 4.4 miles of gravel roads in Troy.
- Tucker Dr. is roughly 1127 ft (0.22 miles) which will amount to \$3500/ yr for Tucker Rd maintenance.

I would have 4 more questions on the same road maintenance subject:

1 - How many miles of concrete roads is DPW responsible for maintaining ? Mainly interested in the secondary/ subdivision type roads.

264.42 miles of local roads.

2 - What was the final budget amount for the 2015 Fiscal Year for the above roads maintenance?

FY 2015 estimate for maintenance only- \$463,000 (does not include drains maintenance or snow, very similar to what I gave you for gravel roads)

3 - Also, does the above amount cover just the concrete roads maintenance (hole patching, etc) or does it also include the concrete reconstruction/replacement projects (like it was the case on Highbury and other secondary streets this Summer) ? Would you happen to know the figure for the road reconstruction projects for secondary roads (i.e. Subdivision type roads) - if it's easy to isolate/ separate from the actual budget ?

The \$463,000 is just maintenance. The replacement or reconstruction of concrete local roads is roughly \$2.5 million a year.

4 - And finally, what was the actual versus projected TPW budget ? I can see what the 2015 estimated amount is (\$5.76 mil) vs 2015 amended (\$6.273 mil).

Not sure what document you are looking at but actual 2015 numbers is what we expensed. Since or fiscal year ended June 30 it takes several months to process all the invoices and expenses so an official number is not available until fall. Estimated amounts are where I project we will be at on June 30. Sometimes this takes requires an amendment to the budget if I am over budget. Projections are made 4-5 months before June 30 so it can be a little off.

Thank you very much for your help,

Semida Fratila
2192 Tucker Dr.

William J Huotari

From: Leo <lmurskyj@aol.com>
Sent: Monday, July 13, 2015 9:35 AM
To: William J Huotari
Subject: Fwd: Tucker barricade

Sent from my iPhone

Begin forwarded message:

From: Leo <lmurskyj@aol.com>
Date: July 13, 2015 at 9:32:56 AM EDT
To: "houtariwj@troymi.gov" <houtariwj@troymi.gov>
Subject: **Tucker barricade**

I am in favor of removing the barricade at Tucker. This removal would be cost effective and not overly disrupt the character of the current neighborhood. Traffic pattern would be minimally disrupted as only residents would use this. Also this would alleviate some of the traffic on Saffron Dr.

Sincerely,
Leo R. Murskyj
5115 Saffron Dr
Troy, Mi

Sent from my iPhone

William J Huotari

From: Marco Cercone <mrcdc2004@sbcglobal.net>
Sent: Monday, July 13, 2015 6:51 AM
To: William J Huotari
Subject: Re: Barricade on Tucker East of John R

Good morning Bill,

After searching most of my home this past weekend I am un-able to find any documents that reference the barricade on Tucker. All that I can think is that I must have had a conversation with the late John Saylor of Saylor Building on the subject. I'm not sure if Saylor Building still exists to see if they have any documents on the removal of the barricade. I apologize for any inconvenience that I may have caused you in miss speaking. I still plan on being present Tuesday to voice my concerns.

Regards,
Marco Cercone

On Friday, July 10, 2015 8:10 PM, William J Huotari wrote:

Mr. Cercone, if you do have a copy of the bylaws you reference can you forward a copy to me. It would be information that should be included in the discussion of the item.

Thanks, Bill

Sent from my iPhone 6+

On Jul 10, 2015, at 7:41 AM, Marco Cercone <mrcdc2004@sbcglobal.net> wrote:

Mr. Huotari,

My name is Marco Cercone and I am a resident of Troy, Mi. I live at 2349 Tucker in the Long Lake Meadows Sub.

I moved into my home in April of 1991. At that time the builder (Saylor Building) had put into our by laws a provision to not take down the barricade unless the homeowner's on Tucker between John R. and west of the barricade were willing to have paved concrete roads, paved concrete sidewalks, and the proper storm drains installed at their expense. If these homeowners do not agree to this then the barricade cannot come down.

The only other option is to have the person requesting to take this barricade down pay for the cost of paved concrete roads, paved concrete sidewalks, and proper storm drains installed. I will fight this request if anything short of the proper material were to be used (i.e. asphalt roads and sidewalks).

I look forward to the meeting on July 15, 2015 @ 7:30 P.M. to voice my concerns in person. Thank you in advance for reading this e-mail.

Best Regards,
Marco Cercone
2349 Tucker
Troy, MI.

William J Huotari

From: Genevieve Murskyj <ZhenyaM@aol.com>
Sent: Sunday, July 12, 2015 2:09 PM
To: William J Huotari; Dane Slater
Cc: Brian M Kischnick; Jim Campbell; Steve Gottlieb; Dave Henderson; Ellen C Hodorek; Ed Pennington; Doug Tietz
Subject: Fwd: Troy - Tucker decision

Attention: Mr. Huotari, Traffic Engineer

I have been a resident in the Long Lake Meadows subdivision for over twenty years and our street intersects with Tucker.

In response to your notice of June 30th, I have waited a very long time and would sincerely appreciate the simple barrier removal on Tucker. This would effectively enhance response time for all emergency vehicles, greatly remove resident concerns and is the most economical method for resolving this issue.

Please open up Tucker and keep me updated with any future developments.

Thanks,

Genevieve J. Murskyj

William J Huotari

From: Monica Hausner <mhausner2@gmail.com>
Sent: Saturday, July 11, 2015 3:31 PM
To: William J Huotari
Subject: Traffice Committee Meeting July 15, 2015 - Request to Remove Barricade On Tucker

Dear Mr. Huotari,

I am writing to express my objection to remove the barricade on Tucker for the following reasons:

- 1) Tucker will not be handle the additional traffic flow. It could take me up to 5 minutes to exit Tucker onto John R southbound during rush hour traffic (morning & afternoon). When I take my children to school in the morning, it is very difficult to turn southbound onto John R. My relatives and friends also often express how difficult it is to exit Tucker onto John R southbound. As soon as the northbound traffic clears, then the southbound traffic backs up.
- 2) In addition to the traffic flow issue, the visibility is poor (short distance) while turning left from Tucker onto John R southbound. While I am very cautious and familiar with the road structure/conditions, this could cause a major safety concern with additional traffic flow.
- 3) The potholes in the Spring are horrendous on Tucker. Additional traffic flow will make matters worse.
- 4) Drake street is not comparable to Tucker Dr.
- 5) Additional cost to Tucker residents to pave the road. Leaving the dirt road is not an option due to the potholes every Spring.

Thank you for your consideration.

Kind regards,
Monica Hausner
Tucker Resident

William J Huotari

From: Frank Faron <ffaron@wideopenwest.com>
Sent: Saturday, July 11, 2015 8:12 AM
To: William J Huotari
Subject: Re: Request to Remove Barricade on Tucker

Dear Mr. Huotari,

My name is Frank Faron and I reside at 2317 Tucker Drive, East of the existing barricade.

In response to the notice that I received regarding the proposal to remove the barricade on Tucker Drive, East of Standish Drive, I want to inform you that I am strongly against this for the following reasons:

1. Turning south from the intersection of Tucker and John R will be a safety concern because of the restricted visibility at the intersection, close proximity to the Long Lake/John R intersection, and amount of traffic on John R in the morning and evening rush hours.
2. To safely use the intersection, the city would probably need to install a traffic light at considerable expense and disrupting traffic flow during non-busy period.
3. This would result in a very long perfectly straight stretch of road which will promote people driving above the speed limit and detract from the appearance and therefore property values for the nearby homeowners. There are already enough motorcycles and cars exceeding the speed limit within Long Lake Meadows subdivision without providing a natural drag strip.
4. I assume that it would be necessary to pave the section of Tucker west of the barricade at considerable cost to the city or homeowners.

If this is absolutely necessary for fire and safety reasons, I would think there are less expensive barriers that could be installed that could be removed in case of an emergency.

I will not be able to attend the meeting on July 15 and wanted to make certain my concerns were on record.

Feel free to contact me at 248-703-3912 or by email should you have any questions.

Sincerely,

Frank Faron

William J Huotari

From: William J Huotari
Sent: Friday, July 10, 2015 9:43 PM
To: Christopher Hausner
Cc: Brian M Kischnick; Mark F Miller; Lori G Bluhm; Brent Savidant; Steven J Vandette; Kurt Bovensiep; Timothy L Richnak
Subject: Re: Opposition to opening Tucker Drive in Troy

Thank you for your email.

I will provide a copy of the same when this item is discussed.

The item will be discussed by the Traffic Committee at the meeting of July 15.

The item will also be on the agenda at the Planning Commission meeting of Aug 11.

Recommendations from the Traffic Committee and the Planning Commission will be forwarded to City Council at their meeting of August 24 for their consideration.

You may contact the City Attorney or City Clerks office regarding the process for a recall petition process.

Sincerely,

Bill Huotari, PE
Deputy City Engineer/Traffic Engineer
City of Troy

Sent from my iPhone 6+

On Jul 10, 2015, at 9:33 PM, Christopher Hausner <chausner@gmail.com> wrote:

Good evening,

As a long standing resident of Troy, I want to express my concerns and opposition to opening Tucker Drive to through traffic.

1. A resident who lives on a road more the 1/4 mile from our street is complaining. Drake does not connect or intersect Tucker. How can a resident in a non-connected street force changes in our sub and push a tremendous tax burden on the current residents? A resident on a non-connected street does not have standing to make the request.
2. There has been zero construction of any new homes on this street. So why is there a need to open the street all of a sudden. It makes not sense at all.
3. The existing dirt road can not handle the large volume of added traffic that would result from opening the road. Large pot holes would be created at an even greater frequency.
4. Regarding the fire/police access. Two new access points through Mayflower and Drake will be in place so why do we need a third access point. If an EVA entrance is needed one could be put in to permit fire/police in the event they need to get through without opening the road to traffic.

5. Entering John R from Tucker is already difficult with the heavy North/South traffic. With only a doze residents it often takes 5 minutes just to turn on to John R during rush hours. Imagine the bottle neck with 40-50 cars. Moreover, a traffic light would be needed. Otherwise a potential safety hazard will be created
6. The site lines from Tucker flowing into John R are very poor. Which was one of the original reasons Tucker was not opened.
7. The original city resolution stated the road would be paved when new construction occurred on Tucker which has not happened.

Finally, I would appreciate you passing the information along to the entire city council. I would also like to understand how to create a recall petition for council member supporting a frivolous, costly and unnecessary tax on the hard working families living on Tucker Drive. Have a nice day.

--

Christopher K. Hausner

Kaizen Sensei, Master Black Belt, Data Analysis SME, and Engineer

William J Huotari

From: William J Huotari
Sent: Thursday, July 09, 2015 3:17 PM
To: 'semidaf@yahoo.com'
Subject: RE: EVAs and collapsible barricades in the City or Troy

Semida, I am not aware of any of those types of barriers in Troy.

The EVA's that we install are of the wood post variety with holes drilled for breakaway.

Thanks, Bill

-----Original Message-----

From: semidaf@yahoo.com [mailto:semidaf@yahoo.com]
Sent: Thursday, July 09, 2015 3:16 PM
To: William J Huotari
Cc: semidaf@yahoo.com
Subject: Re: EVAs and collapsible barricades in the City or Troy

Bill,

Thank you very much for the information you have provided. It is perfect.

In regards to the collapsible barrier subject:

- is there any barrier within the city of Troy that looks like a permanent barrier that is actually collapsible when you push on the barrier and try to drive over it. It can either be pylons with collapsible shear plates at the bottom or a continuous solid barrier - guard rail type - that has the legs collapsible and you can push the whole barrier down and drive over it.

These are in fact EVA type barriers that look like permanent/ solid ones.

City of Troy might not have any barriers of this type,

Thanks again,

Semida

William J Huotari

From: semidaf@yahoo.com
Sent: Thursday, July 09, 2015 3:16 PM
To: William J Huotari
Cc: semidaf@yahoo.com
Subject: Re: EVAs and collapsible barricades in the City or Troy

Bill,
Thank you very much for the information you have provided. It is perfect.

In regards to the collapsible barrier subject:

- is there any barrier within the city of Troy that looks like a permanent barrier that is actually collapsible when you push on the barrier and try to drive over it. It can either be pylons with collapsible shear plates at the bottom or a continuous solid barrier - guard rail type - that has the legs collapsible and you can push the whole barrier down and drive over it.

These are in fact EVA type barriers that look like permanent/ solid ones.

City of Troy might not have any barriers of this type,

Thanks again,

Semida

William J Huotari

From: William J Huotari
Sent: Thursday, July 09, 2015 10:19 AM
To: 'semidaf@yahoo.com'
Subject: RE: EVAs and collapsible barricades in the City or Troy

Semida, I received the following information on EVA's from our Fire Department:

There are EVAs at the following locations:

- Wabash Lane
- Doral / Rochester
- Paragon
- Boyd
- Harmony
- Parkton
- Starr
- Raleigh Lane
- Grand Haven / N. Lovington
- 600 Wilshire (Grass pavers along north side of building) •Oakland-Troy Airport (Coolidge Hwy gate & Equity Dr. gate)

I'm not sure what "collapsible barriers" are in reference to?

Thanks, Bill

-----Original Message-----

From: semidaf@yahoo.com [mailto:semidaf@yahoo.com]
Sent: Wednesday, July 08, 2015 4:02 PM
To: William J Huotari
Subject: EVAs and collapsible barricades in the City or Troy

Dear Mr Huotari,

Thank you for taking the time and talking with me yesterday and answering my questions regarding the Tucker Dr barricade removal hearing that will happen on Wednesday, July 15. I would like to ask you two more additional questions on the same subject:

- 1 - can you please send me the EVA locations (addresses) that are available in the City of Troy.
- 2 - locations of collapsible barriers within the City of Troy - location/address if you have available.

Once again thank you very much for all your help.

Sincerely,

Semida Fratila
2192 Tucker Dr.

William J Huotari

From: semidaf@yahoo.com
Sent: Wednesday, July 08, 2015 4:02 PM
To: William J Huotari
Subject: EVAs and collapsible barricades in the City or Troy

Dear Mr Huotari,

Thank you for taking the time and talking with me yesterday and answering my questions regarding the Tucker Dr barricade removal hearing that will happen on Wednesday, July 15. I would like to ask you two more additional questions on the same subject:

- 1 - can you please send me the EVA locations (addresses) that are available in the City of Troy.
- 2 - locations of collapsible barriers within the City of Troy - location/address if you have available.

Once again thank you very much for all your help.

Sincerely,

Semida Fratila
2192 Tucker Dr.

William J Huotari

From: Rick and Beth Churay <churay21@gmail.com>
Sent: Wednesday, July 08, 2015 12:58 PM
To: William J Huotari
Subject: Opening of Tucker Rd. to John R.

I live at 2338 Tucker and would be affected by opening the barrier to John R. My preference is to keep the barrier in place and not open the road to John R. Even though I would benefit by having access to John R., I do not want to see the increased traffic.

In the event the that it is decided more access is required, I feel Drake street should also be opened between Long Lake Meadows and the new subdivision adjacent to it to provide full access and eliminate all barriers

Thank You.

William J Huotari

From: S Sukhi <bombaywala75@gmail.com>
Sent: Wednesday, July 08, 2015 1:49 PM
To: William J Huotari
Subject: Subject: Remove the barricade on Tucker East of John R

Sir/Madam:

I live at 5316 Standish Drive (248 879 6274). I support removal of the barricade on Tucker East of John R. Since some 20+ houses were built after clearing the woods and Standish Drive was connected to John R via newly built Mayflower Drive , we have noticed increased vehicular traffic on Standish Drive . After Standish is connected to John R via Drake Drive , upon completion of houses currently being built , traffic will substantially increase on Standish Drive. To reduce the congestion , it is imperative that the City of Troy , remove the barricade on Tucker East of John R.

Sincerely,

Suren Sukhtankar (248 879 6274)

P. S . I am likely to attend the traffic committee meeting on 15 July.

William J Huotari

From: Smith, Patrick (Detroit, MI) <patrick.smith@hp.com>
Sent: Tuesday, July 07, 2015 7:49 AM
To: William J Huotari
Cc: Brian Smith
Subject: Traffic Committee Meeting July 15, 2015 - Request to Remove Barricade on Tucker

William,

Just writing to express my concern re: the possibility of removing the barricade on Tucker which is right at my house; I'm at 2234 Tucker.

I am very much against removing the barricade. The thought of thru traffic driving down the dirt road on to the paved road would result in a lot of dirt being kicked up into the air and into my yard and home. Not to mention that with the barricade our little part of Tucker is a peaceful safe place for children to play.

Sincerely,

Pat

PATRICK L. SMITH
Technical Consulting
Application Development Services
HP Enterprise Services
Mobile +1 248.941.5451
PC Phone +1 404.648.7363
Email patrick.smith@hp.com



William J Huotari

From: JEE E <jee_0303@yahoo.com>
Sent: Monday, July 06, 2015 7:40 PM
To: William J Huotari
Subject: removal of barricade on Tucker, east of John R

Since I live on the street that will be most affected by the removal of this barricade, I am totally against removal of it. The people want to avoid the traffic light on John R during the commuting hours. This means more people will be going much too fast down my street. We already have much too much cut through traffic. The mothers driving their kids to and from school and those going to work speed down our street going far beyond the posted limit of 25.

Joyce Entrekin
5176 Saffron Drive
Troy, Mi 48085

William J Huotari

From: Alina <alinamocon@hotmail.com>
Sent: Sunday, July 05, 2015 1:29 PM
To: William J Huotari
Subject: Request to Remove Barricade on Tucker

Dear Mr. Huotari,

I am writing to express my opposition regarding the request to remove the barricade on Tucker.

My family has resided at 2227 Tucker in Troy for the last twenty years. We built our house on an empty lot at the end of Tucker so that our two sons, both of whom were attending middle and elementary school at the time, could have a safe environment to play in around our home. Our neighbors also had young children at that time and we were always comforted by the fact that our kids could play in front of our house without having to worry about traffic going by at dangerous speeds.

Today, we've enjoyed seeing growing families move into the neighborhood with young children of their own. My son's friend, who now has two small children and grew up down the street from us, is seen daily walking past our house with her children and with her parents who still live in this subdivision. They enjoy the peace and safety of this part of our neighborhood. Not only Tucker, but the immediate connecting streets (with minimal traffic) are used by the entire subdivision because of the peace and security the closed off portions provide.

Troy is looking to be more pedestrian-friendly with projects like "Move Across Troy" to promote pedestrian safety. Opening up Tucker would diminish the overall enjoyment of our neighborhood, be less appealing to future families who wish to move to Troy and our subdivision, and it raises the potential for serious injuries as cars and trucks would begin to use this street as a "shortcut" on a daily basis.

I urge you to reject the request to remove the barricade on Tucker. Thank you for taking the time to read this email since I am unable to attend the meeting on July 15. Please contact me if you have any questions.

Sincerely,

Alina Mocon
2227 Tucker

William J Huotari

From: Asaro, Dominick <DAsaro@troy.k12.mi.us>
Sent: Wednesday, June 24, 2015 10:39 AM
To: Brent Savidant; Dziatczak, Mark C
Cc: William J Huotari
Subject: RE: Tucker Street - Troy, MI

This should have no impact on our transportation routes. We don't plan on changing routes with the barrier down.

Thanks,
Dominick Asaro
Troy School District
Transportation Liaison
248-823-4056

From: Brent Savidant [mailto:SavidantB@troymi.gov]
Sent: Wednesday, June 17, 2015 8:34 AM
To: Asaro, Dominick; Dziatczak, Mark C
Cc: William J Huotari
Subject: Tucker Street - Troy, MI

Gentlemen:

The City of Troy has received a request from a resident to remove the existing barricade on Tucker. This barricade is located east of John R and north of Long Lake Road. It appears that the barricade may be impacting access to and from Wass Elementary for Troy families.

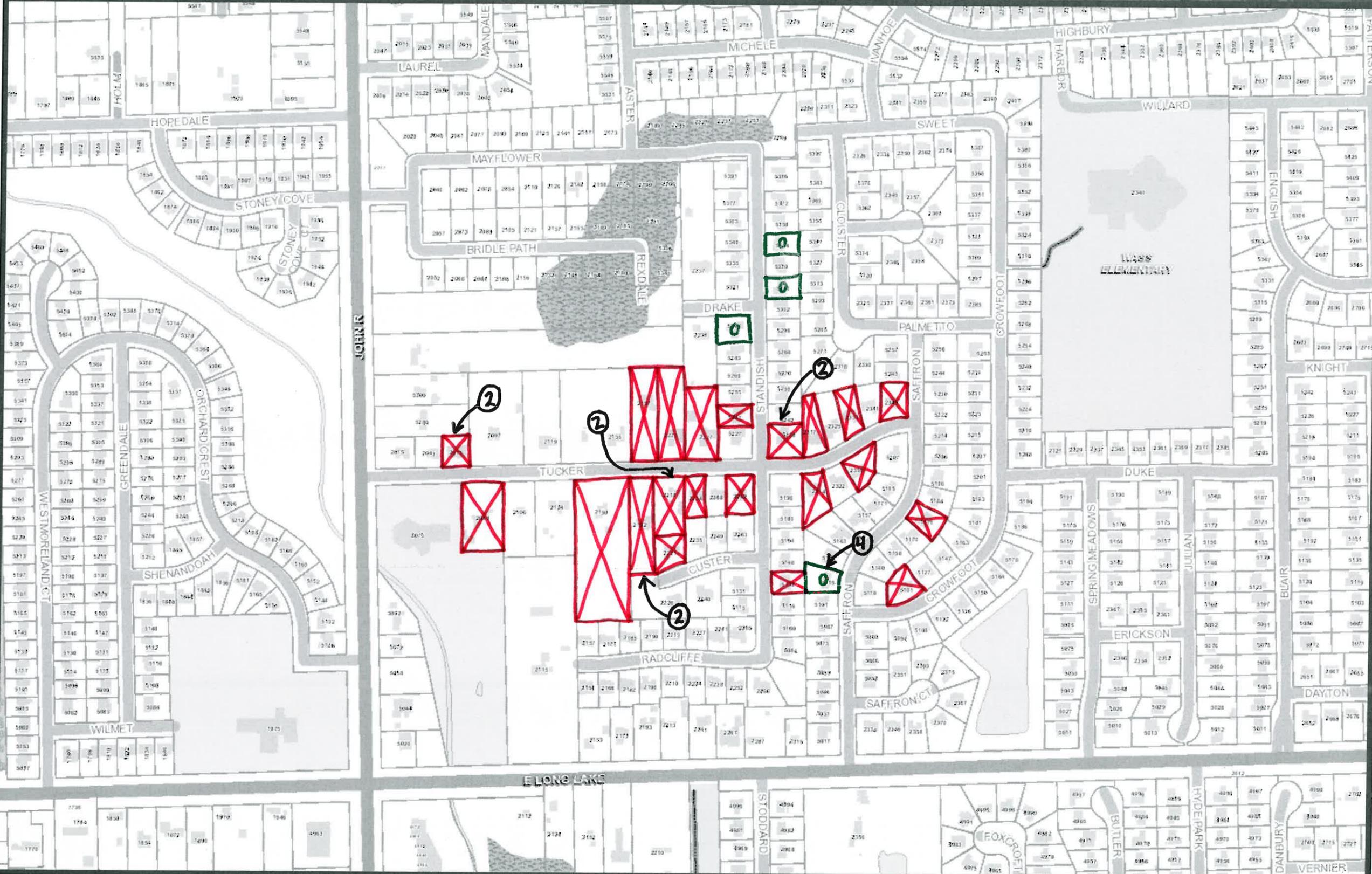
We seek input on this potential action from Troy School District, particularly someone familiar with transportation. Please provide us with a brief statement related to the potential removal of the Tucker Street barricade.

Thank you.

R. Brent Savidant | Planning Director

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3364 | Cell: 248.943.0821 troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."



Legend

- ✕ - KEEP BARRICADE
- - REMOVE BARRICADE
- Ⓜ - MULTIPLE EMAILS FROM SAME ADDRESS

1:5,741



Notes

JULY 16, 2015

956.8 0 478.42 956.8 Feet

WGS_1984_Web_Mercator_Auxiliary_Sphere

Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

TRAFFIC COMMITTEE MEETING SIGN-IN SHEET

July 15, 2015

NAME (PLEASE PRINT)	ADDRESS	PHONE #	Email Address	ITEM # or AREA OF CONCERN
PAUL TURNER	3899 SPRUCE RD	810-878-0204	TURNERPAUL@ATT.NET	SIDEWALK WAIVER
CYTHIA FEDAK	5227 STANLEY	248-819-3666		STANLEY & TUCKER
MIKE LANHAM, SR.	2124 TUCKER, DR.	248-689-8511		TUCKER, DR.
MARCO CERLONE	2349 TUCKER DR	248 879 8256	MRCDC2004@sbcglobal.net	TUCKER BARRICADE
Sandra Paci	5045 Saffron Dr	248-813-1611		
ROBERT RAYMONT	2700 SPARTA DRIVE	248-417-0972	RRAYMONT2001@YAHOO.COM	STOP SIGNS
LORETTA RAYMONT	"	"	"	"
Deb Tooch	2088 Tucker	248-524-9329	MST6AEDEN@gmail.com	Tucker barricade
MICHAEL DRAPER	328 KRAVINK	248-957-4623		SIDEWALK
GARY COPLEY	5171 SAFFRON	248-250-9012		BARRICADE
Ken Trastum	5158 Saffron	248-819-5111		
CHRIS HAUSNER	2071 TUCKER	248-743-1338	chausner@gmail.com	TUCKER
Monica Hausner	2071 Tucker	248-743-1338	mhausner@gmail.com	Tucker
Tim Woodin	4222 Parkside Cr ST	586-228-4221		Close Closing
Ollie Apahidean	2223 Tucker Dr.	586-604-8554		Tucker barricade
Bob Welch	1244 ALMOND	248-879-0796		

FOR 4177 BEACH RD

TRAFFIC COMMITTEE MEETING SIGN-IN SHEET

July 15, 2015

NAME (PLEASE PRINT)	ADDRESS	PHONE #	Email Address	ITEM # or AREA OF CONCERN
ELIZABETH GRAMER	6751 CRESTVIEW	248 840 4009	ELIZABETH@GRAMERFUNERALHOME.COM	STOP SIGN
FAROOK SALAM	2015 TUCKER	248-619-9251		STOP SIGN
DAN FRATILA	2192 TUCKER	248 619-0773	DANSEMI@YAHOO.COM	TUCKER
BARBARA NORTHAM	524 STANDISH	248-879-2775		TUCKER
DOROTHY KONIANSKE	2231 DRAKE	248-225-4971		TUCKER
SEMIDA FRATILA	2192 TUCKER	248 619-0773		TUCKER
CLAIRE WOLF	2150 TUCKER	248-835-8580		
Mihaela Dancea	5302 Standish	248 828 1112		"
Horatio Dancea	"	"		"
Daniel Murza	2218 TUCKER	248 835 6457		TUCKER
Semida Fratila	2192 TUCKER	248 275 5330	Semida@yahoos.com	Tucker Barrier Angel
Linia MURZA	2218 TUCKER DR	249-703-8298	CAFELUTSA@YAHOO.COM	TUCKER BARRIER
PETRU LUPAS	2194 TUCKER RD	248-835-3884	PETRULUPAS@YAHOO.COM	
CORNELIA LUPAS	2197 TUCKER AVE	248-219-4377	Lupascornelia@yahoo.com	
GENEVIEVE MURSKYJ	5115 SAFFRON	248-231-1902	G.MURSKYJ@AOL.COM	PLEASE OPEN UP TUCKER!
Leo Murskyj	5115 Saffron	248 828 7371		Tucker

Petition opposing the removal of the barrier on Tucker Street

We, the undersigned property owners or current occupants, do hereby respectfully petition against the proposed REMOVAL of the barrier on Tucker Street, east of John R Road. The barrier on Tucker Drive should remain in place consistent with City council Resolution # 87-1086.

Printed Name	Address	Phone #	Signature
DAN FRATILA	2192 TUCKER DR., TROY, 48085	248 619-0773	<i>[Signature]</i>
Suzanne Monck	5228 STANDISH DR. 48085	248 535-1237	<i>[Signature]</i>
TIM MAJICK	5228 STANDISH DRIVE TROY	248 535 1237	<i>[Signature]</i>
CYNTHIA FEDAK	5227 STANDISH 48085	248 879 3666	<i>[Signature]</i>
BARBARA J NORTHAM	5241 STANDISH 48085	248 539-2775	<i>[Signature]</i>
Tracy Scheve	2248 Tucker Troy 48085	248 379-1203	<i>[Signature]</i>
Rachelle Lyngk	2262 Tucker Dr Troy 48085	248 506 0578	<i>[Signature]</i>
PATRICK SMITH	2234 TUCKER TROY MI 48085	248 941 5957	<i>[Signature]</i>
LICIA MUZZA	2218 TUCKER TROY MI 48085	248.203.8878	<i>[Signature]</i>
MIKE LANHAM JR	2124 TUCKER TROY, MI 48085	248 691 9111	<i>[Signature]</i>
John Whelan	2043 Tucker Drive, Troy, MI 48085	(413) 862-0824	<i>[Signature]</i>
Kristen Whitney	2043 Tucker Dr Troy, MI 48085	412-780-3718	<i>[Signature]</i>
Daniel Muzace	2218 Tucker Dr, Troy, MI 48085	248-835-6977	<i>[Signature]</i>
CYNTHIA VEGGIAN	2155 TUCKER DR TROY 48085	248-659-8523	<i>[Signature]</i>
BRUNO VEGGIAN	2155 TUCKER DR TROY MI, 48085	248 699 5523	<i>[Signature]</i>
CHRIS HACKNER	2071 TUCKER DR TROY MI 48085	248-743-1338	<i>[Signature]</i>
SANDRA ANDREONI	2077 TUCKER DR TROY MI 48085	248 689 4423	<i>[Signature]</i>
Brian Anthony	2119 Tucker Dr Troy 48085	248-731-7305	<i>[Signature]</i>
Cornelia Lupas	2197 Tucker Dr Troy 48085	(248) 219-4377	<i>[Signature]</i>
PETRU LUPTAS	2197 TUCKER DR TROY 48085	248-335-3844	<i>[Signature]</i>
MARLO CERONE	2349 TUCKER DR. TROY 48085	248 879 8256	<i>[Signature]</i>
Shadee Cerone	2349 Tucker Dr Troy 48085	248 879 8256	<i>[Signature]</i>
FRANK FARON	2317 TUCKER DR TROY 48085	248 879 1703	<i>[Signature]</i>
Jean Maikozek	2322 Tucker Troy 48085	248-828-0121	<i>[Signature]</i>
MIKE MAIKOZEK	2322 TUCKER TROY 48085	248-828-0121	<i>[Signature]</i>
MARYLOU FARON	2317 TUCKER TROY 48085	248-879-1703	<i>[Signature]</i>

Petition opposing the removal of the barrier on Tucker Street

We, the undersigned property owners or current occupants, do hereby respectfully petition against the proposed REMOVAL of the barrier on Tucker Street, east of John R Road. The barrier on Tucker Drive should remain in place consistent with City council Resolution # 87-1086.

Printed Name	Address	Phone #	Signature
Carol Petty	5180 Standish Troy MI 48065	877-9362	Carol Petty
Monica Hausner	2071 Tucker Troy MI 48065	248-743-1338	Monica Hausner
ELAINE WOLF	2150 Tucker Troy MI 48065	248-835952	Elaine Wolf
Sulaiman Salem	2015 Tucker Troy	248-619-9251	Sulaiman Salem
Farooq Salem	2223 Tucker Dr Troy	586-604-1828	Farooq Salem
Nhianna Akhedeani	2227 Tucker Troy	248-619-9037	Nhianna Akhedeani
Olina Prosser	2227 Tucker Troy	248-689955	Olina Prosser
Zyq Moeun	2240 Custer Troy	248-219-5796	Zyq Moeun
TRIFIL BARTUCH	2378 Tucker Troy	248-879-6518	Trifil Bartuch
Richard	2223 Tucker Troy	586-604-8554	Richard
Ollie Ashdean	2106 Tucker Troy	(248)321-2845	Ollie Ashdean
Kwanyi Kim	2106 Tucker Troy	(248)564387	Kwanyi Kim
Sandy Kim	2088 Tucker Troy	248-524-9329	Sandy Kim
Deb Tosch	2088 TUCKER Troy	248-524-9329	Deb Tosch
John Dugan	5256 STANDISH DR, TROY	248-879-0557	John Dugan
KENNETH AUGERANI	2097 TUCKER TROY	248-68506500	Kenneth Augerani
Solomon Asmelash	2221 CURR DR, TROY, MI	248-249-5635	Solomon Asmelash
Fikri Bajajyan	2221 CUSTER DR TROY	248-7978371	Fikri Bajajyan
Thomas Schenel	2248 Tucker Dr Troy MI	248-879-1253	Thomas Schenel
Dennis Angelo	5101 Crowfoot Troy MI	248-9798300	Dennis Angelo
Philip G. Cook	5108 Crowfoot Troy	248-879-8558	Philip G. Cook
Adriana Diaz	5197 Crowfoot Troy	248-879-5870	Adriana Diaz
Howard Cain	5117 Crowfoot Troy		Howard Cain
ROBERT WYBO	5193 CROWFOOT TROY	248-879-1937	Robert Wybo
Burt Buchanan	3314 Tucker Dr. Troy	248-879-7809	Burt Buchanan
Mike Buchanan	" " "	"	Mike Buchanan



Fire Department

500 West Big Beaver Road

Troy, Michigan 48084

Phone: 248-524-3419

Fax: 248-689-7520

July 15, 2015

To: Bill Houtari, Deputy City Engineer, &
Traffic Committee Members

Re: Tucker Drive Barricade

The Fire Department is in favor of removing the barricade on Tucker Drive between John R and Standish. The removal will reduce response times and travel distance for emergency vehicles to the East end of Tucker Dr, Standish, Drake, Custer, Radcliff, Saffron and a more direct access to Long lake Meadows. Additionally, the FD will have access to additional hydrants on Tucker if the need should arise.

Although Standish is now accessible from Mayflower, the removal of the barricade from Tucker Dr will provide a more direct access to the Long Lake Meadows subdivision rather than driving through one subdivision to get to another.

If a fire were to occur on Tucker Dr between the barricade and Standish East of the barricade, additional response time could be from 30 seconds to over a minute, which is critical when dealing with life safety. Fires typically double in size every 30 seconds. The Fire Department attempts to use the most direct route whenever possible.

Time & distance survey:

Driving no faster than posted speeds from Station 5 to the barricade:

- Using Highbury, response to the East side of the Tucker barricade takes 3:18 minutes and 1.6 miles
- Using Jeffery, response to the East side of the Tucker barricade takes 3:47 minutes and 1.8 miles
- Using Mayflower, response to the East side of the Tucker barricade takes 2:40 minutes and 1.6 miles
- Using Saffron off of Long Lake, response to the East side of the Tucker barricade takes 3:42 and 2.1 miles
- Using Tucker, response to the Tucker barricade takes 2:09 and 1.4 miles
- Using Mayflower, response to the West end of Redcliff is 1.8 miles and takes an additional 38 seconds from Tucker and Standish and an extra .2 miles.

Incidents on Tucker Dr since 1992

- East side of barricade: 5 (2341, 2341, 2349, 2262, 2349)
- West side of barricade: 4 (2211, 2155, 2223, 2150)

Lt. Eric Caloia
Fire Inspector
caloie@troymt.org
248-526-2609



Harmony Dr



Harmony Dr



CITY COUNCIL REPORT

Date: July 16, 2015

To: Brian Kischnick, City Manager

From: Mark F. Miller, Director of Economic and Community Development
Steven J. Vandette, City Engineer
William J. Huotari, Deputy City Engineer/Traffic Engineer

Subject: Summary of Traffic Committee Discussion of Tucker Barricade

A Private Agreement for Hunters Park 2 Site Condominiums is on the agenda tonight for approval of municipal improvements related to the development.

An independent but parallel item regarding an existing barricade on Tucker was raised during the preliminary site plan approval for Hunters Park 2. James and Dorothy Konarske stated their opposition to the project at the Planning Commission, based primarily on the proposed vehicular connection with Drake. This connection eliminated the “dead end” status of Drake and provided a more direct vehicular connection with John R Road. The Konarkse’s submitted a request to remove the barricade on Tucker Street, east of John R.

The issue of interconnectivity and the removal of the barricade on Tucker Street was placed on the agenda at the Traffic Committee meeting of July 15, 2015. Notices were sent to properties in the SE quarter of Section 13 (322 addresses).

Twenty-five (25) residents signed in at the meeting regarding the Tucker barricade item, but many more were in attendance and the venue was moved from the Lower Level Conference Room to City Council Chambers to accommodate the public in attendance.

A petition was submitted opposed to the removal of the barricade and was signed by sixty-two (62) residents in the immediate area. Emails in opposition to removing the barricade were received from twenty-five (25) residents. Emails supporting removal were received from seven (7) residents.

A majority of residents spoke in opposition to removing the barricade, while a minority spoke in favor of removing the barricade.

A motion was made at the conclusion of public comment to “*Leave the barricade in place and encourage an Emergency Vehicle Address (EVA)*”. **This motion ended in a tie vote (3-3).**

The item will next be included on the Planning Commission agenda at their meeting of August 11. A complete report with minutes of both meetings and supporting documentation will be provided to City Council at their meeting of August 24, 2015.

A regular meeting of the Troy Traffic Committee was held Wednesday, July 15, 2015 in the Lower Level Conference Room at Troy City Hall. Pete Ziegenfelder called the meeting to order at 7:30 p.m. Due to the size of the audience, the meeting was moved to the Council Chambers.

1. Roll Call

Present: Tim Brandstetter
David Easterbrook
Richard Kilmer
Al Petrusis
Cynthia Wilsher
Pete Ziegenfelder

Absent: None

Also present: Paul Turner, 3899 Spruce
Cynthia Fedak, 5227 Standish
Mike Lanham, Sr., 2124 Tucker
Marco Cercone, 2349 Tucker
Sandra Paci, 5045 Saffron
Robert Rayment, 2700 Sparta
Loretta Rayment, 2700 Sparta
Deb Tosch, 2088 Tucker
Murray Deagle, 328 Evaline
Gary Copley, 5171 Saffron
Ken Trasleur, 5158 Saffron
Chris Hausner, 2071 Tucker
Monica Hausner, 2071 Tucker
Tina Woodin, 42322 Parkside
Ollie Apahidean, 2223 Tucker
Bob Weir, 1244 Almond
Elizabeth Gramer, 6751 Crestview
Farook Salem, 2015 Tucker
Dan Fratila, 2192 Tucker
Barbara Northam, 5241 Standish
Dorothy Konarske, 2237 Drake
Semida Fratila, 2192 Tucker
Elaine Wolf, 2150 Tucker
Mihaela Dancea, 5302 Standish
Horatio Dancea, 5302 Standish
Daniel Murza, 2218 Tucker
Liua Murza, 2218 Tucker
Petru Lupas, 2194 Tucker
Cornelia Lupas, 2197 Tucker
Genevieve Murskyj, 5115 Saffron

Leo Murskyj, 5115 Saffron
Sgt. Mike Szuminski, Police Department
Bill Huotari, Deputy City Engineer/Traffic Engineer

2. Minutes – June 17, 2015

Resolution # 2015-07-26
Moved by Kilmer
Seconded by Wilsher

To approve the June 17, 2015 minutes as printed.

Yes: Brandstetter, Easterbrook, Kilmer, Petrusis, Wilsher, Ziegenfelder
No: None
Absent: None

MOTION CARRIED

A motion was made by Mr. Kilmer and seconded by Mr. Brandstetter to move Item #7 to the front of the meeting due to the large number of residents in attendance at the meeting.

Resolution # 2015-07-27
Moved by Kilmer
Seconded by Brandstetter

To move Item #7, on the agenda, to the front of the meeting.

Yes: Brandstetter, Easterbrook, Kilmer, Petrusis, Wilsher, Ziegenfelder
No: None
Absent: None

MOTION CARRIED

7. Request to Discuss Interconnection – Tucker, John R to Standish

A petition was submitted at the meeting opposed to the removal of the barricade and was signed by sixty-two (62) residents in the immediate area. Emails in opposition to removing the barricade were received from twenty-five (25) residents. Emails supporting removal were received from seven (7) residents prior to and after the meeting.

Michael Ortmon of 5298 Standish spoke in **favor** of removing the barricade. His points were based on Planning Commission discussion of connected streets; public safety where seconds matter; if Standish were blocked in the middle, the only way to get in would be from Long Lake to Standish; Tucker would be a much more accessible road for emergency vehicles to access the area; and that traffic could be “evened out” if the barrier comes down as multiple access points would provide residents in the area multiple ways

to get in or out of their subdivisions.

Dan Fratila of 2192 Tucker spoke in **opposition** to removing the barricade. Mr. Fratila provided a Power Point presentation detailing his points. His three (3) main concerns were summarized as:

- A. Low visibility that would jeopardize the health, safety and welfare of the immediate community surrounding Tucker. Two (2) new connections are available in the immediate vicinity of Tucker Dr. (Mayflower and Drake – approved to be opened).
- B. City Council Resolution #87-1086 from 9/14/1987.
- C. City Policy on Street Interconnectivity of conflicts.

Chris Hausner of 2071 Tucker spoke in **opposition** to removing the barricade. Mr. Hausner discussed the following: an increase in the crash hazard with the Tucker connection open, both internally at Tucker/Standish as well as at Tucker/John R; difficulties making a left turn from Tucker to John R with limited traffic on Tucker; request that an EVA (Emergency Vehicle Access) be placed if the barricade were to be removed; there would be a tenfold increase in traffic on the gravel portion of Tucker and it would become a maintenance issue; the request to remove the barricade was made by a resident that does not live on Tucker; there are fourteen (14) homes on the gravel portion of Tucker that would be directly impacted; removing the barricade would create a half-mile straight shot from John R to the interior subdivisions and traffic would travel at high rates of speed; the need for a traffic signal at John R/Tucker if the barricade were removed; there have been two (2) access points added in the immediate area; and finally that no one on Tucker requested that the barricade be removed.

Deb Tosch of 2088 Tucker spoke in **opposition** to removing the barricade. Ms. Tosch discussed the following items: that she had lived on Highbury for 18 years and knows from experience that speeds increase on long, straight stretches of roads in a neighborhood and had a dog hit and killed while living on Highbury; they moved to Tucker because of the barricade and limited traffic; they are on a fixed income and could not afford a SAD (Special Assessment District) paving project; inconsistencies in the City's interconnectivity policy noting that there are three (3) locations in their section of the city where there are barricades and/or EVA's in place and those locations also have curved roads which help keep speeds down; a Wall Street Journal article that 38 counties in Michigan have turned paved roads to gravel to reduce maintenance costs (she stated that it is 3 times the cost to maintain a paved road as compared to a gravel road); if the barricade can't stay then install an EVA like what was done at Boyd and Harmony or Devonwood; be consistent in your policy.

Ken Androni 2097 Tucker spoke in **opposition** to removing the barricade. Mr. Androni has lived on Tucker for 40 years and has watched how it has changed. It has several long and hidden driveways. If the barricade has to come down, install an EVA. Access to John R is already difficult during rush hour and would only be made worse if the barricade comes down. Leave Tucker as it was intended.

Ollie Apahidean of 2223 Tucker spoke in **opposition** to removing the barricade. Tucker

was a gravel road back in 1963. By 1990 most of the homes were built along Tucker. The Barricade was placed in 1987 when Long Lake Meadows was built. Removal of the barricade creates an unnecessary hardship. Accidents will increase at Tucker and Standish. Right now, traffic is limited due to the barricade. He feels the safest solution is to leave things as is. He did note that a motorcycle driver who lives in the area drives around the existing barricade daily. The barricade has been in place for 27 years and has worked just fine for the residents, so why change it now?

Marcus Cercone of 2349 Tucker spoke in **opposition** to removing the barricade. He is opposed to the removal due to speeding that will occur on Tucker. He also stated that it would be detrimental to little kids.

A representative of the Bethesda Romanian Pentecostal Church spoke in **opposition** to removing the barricade. He stated that the church had tried to get a driveway approved from the church to John R in the past but were denied by the RCOC due to visibility issues with the existing bridge. He is also concerned that the children who play in the grassy area at the church could be in harm's way if the barricade were removed.

Monica Hausner of 2017 Tucker spoke in **opposition** to removing the barricade. She stated that there is low visibility. It can take up to five (5) minutes to get out on John R from Tucker. It would be a big mistake to open Tucker up.

Daniel Murza of 2218 Tucker spoke in **opposition** to removing the barricade. He wanted to confirm and agree with all that has been previously stated. He added that it is a hazardous situation. It takes more than 5 minutes to get onto John R in the AM peak hour.

Lee Murza of 2218 Tucker spoke in **opposition** to removing the barricade. She spoke about the safety of the children who play on the street. They drive their kids to school and don't mind driving around to get to the school.

Brian Murphy of 2119 Tucker spoke in **opposition** to removing the barricade. Mr. Murphy discussed the difficulty in southbound John R traffic trying to turn onto Tucker between 3:30 – 6:00 PM. He stated that people pass on the shoulder and that there will be more crashes if the barricade is removed.

Semida Fratila of 2192 Tucker spoke in **opposition** to removing the barricade. She stated that this is a very big safety issue. The metal embankment from the bridge blocks visibility at the Tucker intersection. There are very long driveways with obstructed views which is not an issue currently as there are lower speeds on Tucker with the barricade in place.

A resident of 5302 Standish spoke in **opposition** to removing the barricade. They spoke of the safety of children on Standish if the barricade were removed. The majority of citizens live beyond the barricade.

Gary Copely of 5171 Saffron spoke in **favor** of removing the barricade. He stated that if the barricade is removed, then there would be a safer alternative to exist the subdivision

from the east.

James Konarske of 2237 Drake spoke in **favor** of removing the barricade. He stated that the request to remove the barricade was initiated for consistency. He agrees with the safety issues but believes that the future reconstruction of John R should negate the turning issue.

Michael Ortmann of 5298 Standish spoke in **favor** of removing the barricade. He stated that drivers avoid a bottleneck and with multiple connections, traffic is spread out and gets traffic off the main road. Tucker residents would be able to safely exit the subdivision. There is an autistic child that lives on his street as well as other children.

Deb Tosch of 2088 Tucker spoke in **opposition** to removing the barricade. She stated that the people on Standish want Tucker opened so that traffic is spread evenly.

Chris Hausner of 2071 Tucker spoke in **opposition** to removing the barricade. He stated that a traffic signal would be needed at Tucker and Standish if the barricade was removed. Residents on both sides of the barricade supported leaving the barricade alone. There are more crashes on John R at Tucker than there are at Tucker at Standish. The school has a bus stop on Tucker. The gravel road cannot handle the traffic. The request to remove the barricade from a resident on Drake.

Ollie Apahidean of 2223 Tucker spoke in **opposition** to removing the barricade. John R is planned to be widened. Utility poles are being relocated. It is dangerous to exist Tucker to John R. A widened road would make more pavement to be crossed. A traffic signal [at Tucker/John R] would be too close to Long Lake to meet warrants. There would be an increase in traffic safety issues.

Dan Fratila of 2192 Tucker spoke in **opposition** to removing the barricade. He discussed a petition that was signed by every resident on Tucker. Kids walk or ride bikes to Wass Elementary. School bus stops on Tucker and picks up kids and drops them off from both sides of the barricade. He discussed installation of an EVA which would be minimal cost to install as a portion of the existing guard rail could be left in place while still providing an EVA.

Semida Fratila of 2192 Tucker spoke in **opposition** to removing the barricade. She discussed safety concerns with opening the barricade or keeping the barricade in place, but which is more dangerous?

Mr. Ziegenfelder discussed a hypothetical scenario where the church was on fire and Tucker was closed at John R. There would be no room for emergency vehicles to get through. The existing gravel road was not build to handle through traffic. He himself has pulled a vehicle from the ditch along John R near Tucker. There are no sidewalks along Tucker. If he could not make a left from Tucker onto John R, he would go back through the subdivision if the barricade were not in place. He would support the installation of an EVA.

Lt. Caloia provided a memo from the Fire Department in support of removing the barricade. He discussed the reduced response time for emergency vehicles due to the barricade and the need to access properties from other directions. He stated that 30 seconds can be the difference between life and death.

Mr. Easterbrook discussed EVA's and had questions about cars passing turning vehicles on John R along the shoulders. Sgt. Szuminski responded that it occurs frequently at many locations in the city including along John R. Mr. Easterbrook stated that he was concerned about the safety of the children, but by removing the barricade 40% of traffic would be cut from Standish.

Steve Dearing of OHM Advisors discussed the perception of hazard versus what is law. The Michigan Vehicle Code (MVC) section 257.649, paragraph 6 provides that a driver must stop at a Stop sign; they must stop at a stop bar if present; if there is a marked pedestrian crossing a driver must stop before it. If you can't see from the stopped location then the driver is obligated to again stop at a location where there is adequate sight distance to safely proceed. Mr. Dearing further stated that he did review the Tucker/John R intersection and found that from a point 15' shy of the intersecting roadway that driver's sight distance is down to the traffic signal at Long Lake.

Mr. Kilmer discussed that any subdivision in Troy has traffic and speeding issues. People on the east side have the right to use the street and you have the right to use the other streets. Traffic is bad all over Troy.

Mr. Petrulis discussed safety issues related to speed, emergency vehicles and children. He acknowledged that residents do not want the barricade removed. If they choose to add 30 seconds to a response it is their choice. An EVA is a good compromise. The safest choice may be to leave the status quo.

Ms. Wilsher drives John R on a regular basis and acknowledges that it is difficult to get out on the road. She avoids making a left turn on major roads in Troy, like UPS. She asked about the number of crashes at Tucker/John R and Sgt. Szuminski responded that he is not aware of a significant amount of crashes. Ms. Wilsher stated that if left turns to John R are that dangerous that we should not allow left turns onto John R.

Mr. Brandstetter spoke about safety and the pro's and con's for each point. Removing the barricade would provide an alternate route. Speeding is an issue throughout the city. Interconnectivity spreads the traffic load to more roads. Citizens clearly want this barricade to stay.

Mr. Kilmer discussed damage to a fire truck by running over an EVA.

Mr. Easterbrook asked about a time study completed using Drake. Lt. Caloia responded that one has not been done as the connection is not in place yet.

Mr. Brandstetter asked if Station 5 responded to this area. Lt. Caloia responded in the

affirmative. Lt. Caloia further discussed the study that he conducted reviewing response times using existing routes.

Mr. Ziegenfelder asked about snow plowing at an EVA and what is done when they plow snow up to an EVA. Mr. Bovensiep responded that they dispatch crews, after snow plowing has been completed, to clear the EVA's of snow.

Ms. Wilsher asked if there would be No Parking signs posted at an EVA. An EVA includes No Parking signs.

A motion was made by Mr. Easterbrook and seconded by Mr. Petrulis to leave the barricade in its place and explore an EVA.

Mr. Petrulis made a motion to modify the motion on the table by replacing “explore” with “encourage”. This was seconded by Mr. Easterbrook.

Resolution # 2015-07-28
Moved by Petrulis
Seconded by Easterbrook

To modify the motion by replacing “explore” with “encourage”.

Yes: Brandstetter, Easterbrook, Petrulis, Wilsher, Ziegenfelder
No: Kilmer
Absent: None

MOTION CARRIED

Resolution # 2015-07-29
Moved by Brandstetter
Seconded by Petrulis

To leave the barricade in place and encourage an EVA

Yes: Easterbrook, Petrulis, Ziegenfelder
No: Brandstetter, Kilmer, Wilsher
Absent: None

MOTION FAILED

Mr. Ziegenfelder declared a 5 minute recess until 9:26 PM.

Kathy Czarnecki

From: Adriana Apahidean <adrianadean@comcast.net>
Sent: Wednesday, July 15, 2015 11:16 AM
To: William J Huotari
Subject: Tucker Barrier- opposed to removal

Hello Mr. Huotari:

Just a short note that I too am opposed to the removal of the barrier on Tucker Dr.

This issue requires a thorough traffic study and cost assessment. I urge the Traffic Committee to make an educated and well researched decision.

Thank you kindly,

Adriana Apahidean
2223 Tucker Dr.
Troy, Mi

Sent from my iPhone

Kathy Czarnecki

From: William J Huotari
Sent: Thursday, July 16, 2015 11:29 AM
To: 'dansemi'
Cc: Kurt Bovensiep; Brent Savidant
Subject: RE: Follow up question
Attachments: RE: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Dan, in regard to your questions:

1. The wood posts are supplied by Burt Forest Products- 227 Felch St., Ann Arbor, MI 48107
2. Email is attached

Mr. Savidant has been copied on the emails I have received as of yesterday. I did receive a couple of more emails late last night and this morning, so a complete package of information received will be provided to Mr. Savidant prior to the August 11 Planning Commission meeting.

Thanks, Bill

From: dansemi [mailto:dansemi@yahoo.com]
Sent: Thursday, July 16, 2015 9:56 AM
To: William J Huotari
Cc: Dansemi
Subject: Follow up question

Hi Bill,

Thank you very much for facilitating the Traffic Committee's meeting last night and for providing the laptop for the presentation.

If you don't mind I have 2 more follow up questions and 1 request:

1 - Can you please let me know the supplier the City of Troy is using for the EVA wooden posts that are installed in the 12 EVAs currently available in the City.

2 - You mentioned something about an e-mail answer you received from RCOC regarding John R/ Tucker intersection. Can you please share that e-mail with me.

Request: Can you please forward all the e-mails that you have received regarding the "Tucker barrier removal" issue to Mr. Savidant in the Planning Department. That way we (the residents that wrote to you) don't have to start the e-mail process from scratch for the next phase which will be the Planning Commission.

Thank you again for all your help and support,

Dan

Kathy Czarnecki

From: William J Huotari
Sent: Monday, July 20, 2015 10:11 AM
To: 'dansemi'
Cc: Brent Savidant; Lori G Bluhm; Timothy L Richnak; Kurt Bovensiep; Steven J Vandette
Subject: RE: Follow up questions after the July 15 Traffic Committee Meeting
Attachments: 2000 Aerial.pdf; 2002 Aerial.pdf; 1990 Aerial.pdf

Dan, the item will be discussed at the August 11th Planning Commission meeting, so emails should be directed to the Planning Director, Brent Savidant who prepares the agenda for the Planning Commission.

Mr. Savidant has copies of the previously submitted emails.

The closest example that I recall was a “temporary” barricade on the south side of Enterprise, between Robinwood and Maple, west of Old Rochester, placed when a new subdivision was built to the north in the early 2000’s. The area to the north was a wooded area in the 1990’s. The barricade was removed after the completion of the new homes around 2002. See the attached historical aerials for reference.

I have copied others and they can add their comments if they recall the situation you are asking about.

Thanks, Bill

From: dansemi [mailto:dansemi@yahoo.com]
Sent: Monday, July 20, 2015 9:13 AM
To: William J Huotari
Cc: Dansemi
Subject: Follow up questions after the July 15 Traffic Committee Meeting

Hi Bill,

I would like to start my note by thanking you and the other City Staff members for facilitating this meeting and providing a forum where the resident’s opinions can be heard.

I have 2 follow up questions related to the Tucker barrier subject:

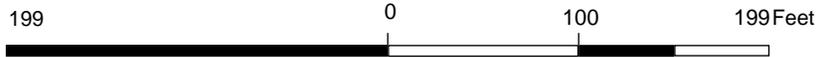
1 - Where should future e-mails be sent, if other residents would like to comment about the Tucker barrier subject ? Still to you or somebody else ?

2 - Was there a road barrier (like the one on Tucker) that separated gravel from paved street ever removed in the City of Troy, and the gravel side never paved, in the last 20-30 years ?

Thank you...Dan



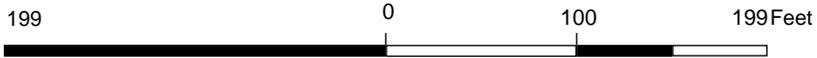
2002 Aerial



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



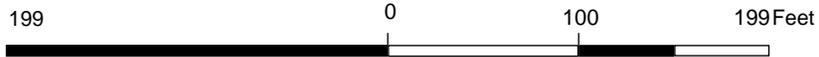
2000 Aerial



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



1990 Aerial



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Kathy Czarnecki

From: dansemi <dansemi@yahoo.com>
Sent: Thursday, August 06, 2015 3:19 PM
To: Planning
Cc: Dansemi
Subject: Tucker barrier Public Hearing, August 11, 2015 -- request to maintain the barrier and encourage the use of an EVA

Dear Planning Commission Members,

I'm writing to you about the Tucker Road barrier public hearing scheduled for August 11, 2015.

Please consider voting that the barrier will stay in place and an EVA option will be encouraged.

Here are the reasons for the above statement.

As you are aware, one (1) household from Drake started this barrier removal request, through an e-mail sent to Mr. Miller's office, stating their opposition to the Tucker barrier (see paragraph below):

*Subject: REMOVAL OF TUCKER STREET BARRICADE This **item was initiated by James and Dorothy Konarske**, Troy residents who live at 2237 Drake, who submitted a request to remove the barricade on Tucker Street, east of John R Road. This item will be forwarded to City Council for consideration. A recommendation from the Planning Commission and Traffic Committee are sought to assist City Council in this issue."*

With the development of Bridgewater Estates, Hunters Park I and II, two(2) new connections were provided for residents of Long Lake Meadows.

As you know, these two(2) new connections are Mayflower and Drake, each within 150 yards of each other and of Tucker.

A - For the last twenty(20) years the Drake household was not concerned about the neighborhood's safety. When Mayflower opened two(2) years ago, again the Drake household were not concerned. Only when Drake was opened as a results of the Bridgewater Estates, Hunters Park II approval this household became all of a sudden concerned and started the process of removing the Tucker barrier.

As per the Planning Director's report to the City Council dated April 13, 2015:

"Drake was constructed as a stub street with the intent that it would be extended to the west in the future".

Drake is a short street paved street with only 2 homes (2237 and 2238).

B - If Tucker is to be opened to traffic and paved/ not paved without providing pedestrian sidewalks, you'll have half of Tucker with sidewalks (part from the Long Meadows subdivision) and then the remaining of Tucker will have no sidewalks forcing the pedestrians to share the road with the traffic. You'll have pedestrian traffic from the subdivision walking towards/from John R and being forced to share the road with the vehicular traffic. Not very safe for anybody in the community, just a serious injury of fatality waiting to happen. *The lack of sidewalks will jeopardize the health, safety and welfare of the immediate community surrounding Tucker.*

Both Mayflower and Drake have pedestrian sidewalks available for the pedestrians to walk safely without interfering with the vehicular traffic. People with children from the entire surrounding subdivision walk on Tucker on a daily basis to the 7/11 store that is at the corner of John R and Long Lake.

C- All the Tucker residents support the installation of an EVA that would satisfy the safety requirements that were raised in conjunction with the Tucker barrier.

City of Troy has 13 EVAs already installed all over the city.

EVA option will be the most cost effective way to address the safety concerns without removing the barrier and forcing the Tucker residents into a S.A.D. (Special Assessment District).

Every Tucker resident (100% - from the gravel side) signed the petition against a S.A.D. that will be imposed by the City for the paving of Tucker.

A significant number of Tucker residents are seniors, living on a fixed income and cannot afford the high cost associated with a S.A.D.

D – Existing City Council resolution #87-1086 from 9/14/1987

“BE IT FURTHER RESOLVED, That before removal of the barrier to and from Tucker Street, said street shall be paved, a Public Hearing is to be scheduled and notice of the Public Hearing is to be sent to all property owners on Tucker Street.”

According to the Planning Department summary:

“The project file indicates that on February 10, 1987 the Planning Commission recommended a barricade on Tucker “until future development occurs in the Tucker Street area”.

- No significant development occurred in the Tucker Street area since 1987 until the approval of Bridgewater Estates, Hunters Park I and II.

- On Tucker Dr. only 3 additional homes were built in the last 18 years. Total number of residences is 14.

- With the development of Bridgewater Estates, Hunters Park I and II, **two(2)** new connections (Mayflower and Drake) were provided to the newer homes on Radcliff Street and the existing residents of Long Lake Meadows.

These 2 new connections should provide ample access (including backup access, if necessary) to all emergency vehicles to quickly and safely deploy the necessary resources to any residence in the neighborhood, therefore alleviating the concerns raised by the Police and Fire Departments.

In closing, I respectfully request that you leave the Tucker Road barrier as is, and encourage the use of an EVA to satisfy the emergency services needs and requirements.

Thank you for your time and consideration,

Dan Fratila
2192 Tucker

Kathy Czarnecki

From: Venkat Dannana <venkatdannana@gmail.com>
Sent: Wednesday, August 05, 2015 11:46 AM
To: Planning
Subject: Public Meeting - August 11,2015 Planning Commission Regular Meeting

Hello Sir,

Subject : Request to remove barricade on Tucker

I, Venkat R Dannana, (resident & owner of 2189,Mayflower Troy,MI-48085), am writing this email to show consent "**In favor of barricade removal on Tucker**". This will not only enable smooth traffic distribution in the neighborhood but also help improve safety response times for fire, police and emergency vehicles by providing access via Tucker.

As I may not be able to make it for the public meeting, please consider this email as my comments in this regard.

Thank you,

Regards,

Venkat R Dannana
2189, Mayflower Dr.
Troy, MI-48085
Ph: 313-920-4590

Kathy Czarnecki

From: Kurt Bovensiep
Sent: Friday, July 10, 2015 2:27 PM
To: Brian M Kischnick; Mark F Miller; Brent Savidant; Steven J Vandette; William J Huotari; Timothy L Richnak
Subject: Tucker- Maintenance Costs

I returned a message from Semida Fratila at 2192 Tucker inquiring about the annual cost of the gravel portion of Tucker. I explained to her that we do not record the cost for individual roads. Instead, I provided her with the following information;

FY 2015 total gravel road expense- \$70,000
4.4 miles of gravel road in Troy
\$15,909 a mile for annual maintenance in FY 2015
Tucker's gravel portion is 1,127 feet from a measurement using GIS or .22 mile
Totals \$3,500 in maintenance for FY 2015

Kurt Bovensiep | Public Works Manager

City of Troy | 4693 Rochester Rd., Troy, MI 48085 | Office: 248-524-3489 | Cell: 248-885-1953 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

Kathy Czarnecki

From: Christopher Hausner <chausner@gmail.com>
Sent: Friday, July 31, 2015 8:22 PM
To: Planning
Subject: Attention Planning Commission Members - August 11 meeting

As a Tucker Resident I am responding to the Public Hearing Notice for August 11th 2015. First, the request the commission is responding to was raised without standing or merit. As you can see in Exhibit 1 the resident who raised the concern lives on a street that is not connected nor interests Tucker Drive. The entire Tucker Neighborhood on both sides of the barricade are surprised that the City Traffic and Planning Commission moved forward without considering the merit of the request. The result has been unnecessary costs to mail notices to residents along with a significant amount of personal time each resident is investing to respond to a frivolous request.

Between 20 and 30 residents expressed their opposition in-person at the Traffic commission meeting. Moreover, the comment from the individual who raised the concern was for personal safety. Interesting that for 20+ years, prior to the opening of his street to the new subdivision (which he is upset at the commission), there was no safety issue. Next when Mayflower was built almost two years ago, creating a new shorter access route to his house, there was no complaint. Now a new subdivision is being put in. Drake is being opened up providing direct access by fire and police to the household's front door. Suddenly there is an issue on a street not even connect to the person's home.

Over 65+ residents on Tucker oppose opening the road. At the same time the residents support the installation of an EVA (Emergency Vehicle Access) point per a recommendation from public safety to make it easier in the event of an emergency and provide additional entry routes.

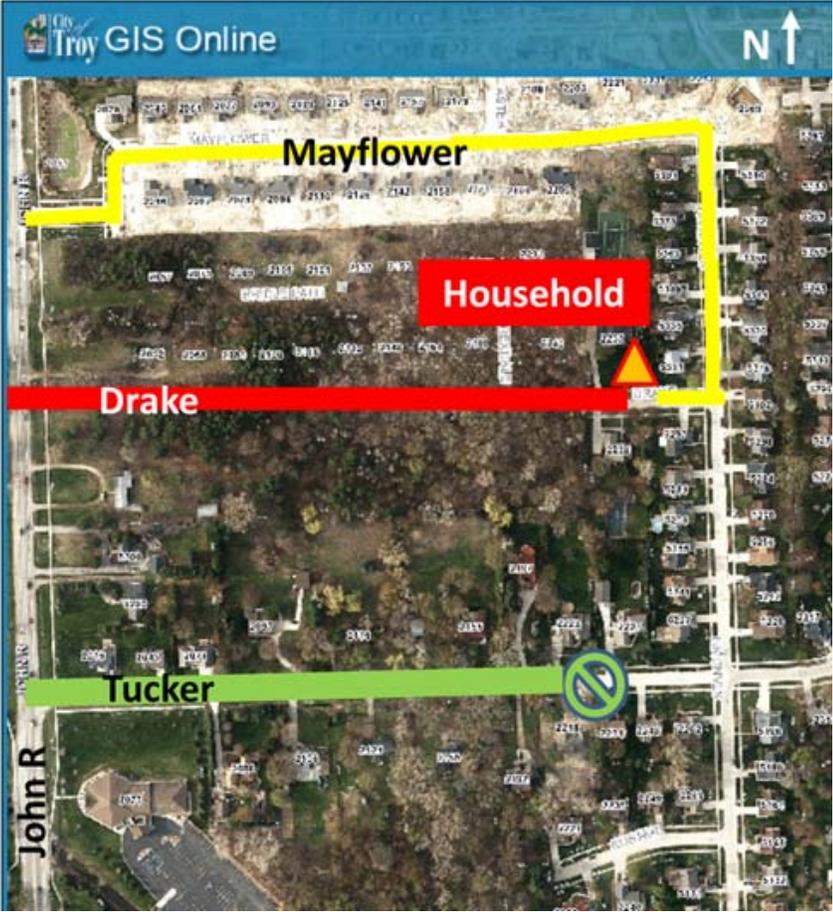
Lastly the residents cannot afford the high cost that would be unjustly forced on them by a household upset that a new subdivision is being built on their street which again is not connect to Tucker. There has been no new construction on Tucker. So what has changed? I expect the Commission meeting will be flooded by residents and potentially local news stations questioning the validity of the request before the hearing.

Thank you for your time.

Christopher K. Hausner

Kaizen Sensei, Master Black Belt, Data Analysis SME, and Engineer

Exhibit 1 – showing the household on Drake is not interconnected to Tucker Drive at all!



Kathy Czarnecki

From: William J Huotari
Sent: Friday, July 17, 2015 8:53 AM
To: Brent Savidant
Subject: FW: Traffic Committee Meeting July 15, 2015

From: Lori G Bluhm
Sent: Friday, July 17, 2015 8:51 AM
To: 'mhausner2@gmail.com'
Cc: William J Huotari
Subject: FW: Traffic Committee Meeting July 15, 2015

Ms. Hausner- Your e-mail was forwarded to me to address the first portion. I welcome the opportunity to clarify the discussion with Traffic Committee Member Kilmer. First, the Traffic Committee rarely has such a large audience and public participation that lasts as long as the meeting this past Wednesday night. However, the Traffic Committee's task is the same for each item where the Traffic Committee makes a recommendation to City Council. The Committee is responsible for making a record, and part of this is to make inquiries. Although this questioning is generally done when a speaker is at the microphone during the public comment portion of the meeting, there were admittedly some questions asked of audience members after the public comment portion was closed on Wednesday night. I interpreted the Chair's interruption of Mr. Kilmer's statements to be a reminder that no questions were to be asked of the audience members after the public comment period closed. As such, I must respectfully disagree with the characterization in your e-mail. In any event, a Board member's expression of opinion and subsequent vote cannot be retroactively nullified.

The City's Boards and Committees are comprised of volunteers who are appointed by the Troy City Council. Diversity of opinions is encouraged so that there is broad representation of the community. I encourage you to submit your remaining concerns to the Planning Commission, and especially to the Troy City Council, who will make the final decision as to whether or not to rescind and reconsider the resolution concerning the barricade.

Lori Grigg Bluhm | City Attorney

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3323 | Cell: 248.885.1899
Fax 248.524.3259 | Bluhmlg@troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

From: Monica Hausner [<mailto:mhausner2@gmail.com>]
Sent: Thursday, July 16, 2015 3:05 PM
To: William J Huotari <HuotariWJ@troymi.gov>
Subject: Traffic Committee Meeting July 15, 2015

Dear Mr. **Huotari**,

Thank you very much for your time yesterday to meet with the Troy residents regarding the removal of the barricade on Tucker Dr.

First, I want to express my concern over one of the Traffic Committee members, Mr. Richard Kilmer. I was greatly appalled by Mr. Kilmer's behavior and comments personally directed towards the Tucker residents. His behavior was unacceptable and I am recommending that he be removed from the Traffic Committee.

As a Troy resident, I do not believe that Mr. Kilmer should represent the Troy residents based on the behaviors that he exhibited at yesterday's meeting. The chairperson even asked him to control his comments and direct them to him and not the audience. In addition, I am requesting that his vote be null and void based on his behavior and his unwillingness to consider all of the facts including the safety of Troy residents which should be the number concern.

Second, **what is the real purpose of removing the barricade on Tucker Dr.?**

1) Inconsistency in the connectivity policy?

a. The city mailer outlined that a resident of Drake believes the city's connectivity policy is inconsistent. Why is Tucker singled out? Why hasn't the city analyzed all of the dead-end roads? This request is discriminating against the residents of Tucker if the purpose is related to the connectivity policy.

2) Fire and police access?

- a. **Solution:** The most cost effective solution for the city is to install an Emergency Vehicle Access (EVA) at the end of Tucker.
- b. Removing the barricade and keeping the gravel road is not an option. The potholes in the spring are horrendous. The current road will not be able to sustain the additional traffic flow.
- c. Paving the road will cause undue financial hardship for the residents on Tucker, who are not even requesting to open the road. Please refer to the signed petition of residents in opposition of removing the barricade.

3) Improve traffic flow?

- a. Opening up Tucker will only worsen the traffic flow for southbound John R
 - i. There is no center turn lane. When vehicles are waiting to turn left onto Tucker, southbound traffic backs up. There is no shoulder on John R for vehicles to pass. Motorists will be driving on the gravel shoulder to avoid waiting in traffic causing further safety and road concerns.
 - ii. Turning left onto John R could take me up to 5 minutes during early morning and afternoon rush hour traffic. This will result in traffic backups on Tucker. If three or four vehicles are waiting to turn left onto John R, the wait time for the other vehicles could be as high as 15 minutes. Vehicles will be backing up and turning around to avoid the wait time.

4) Safety Concerns?

- a. The safety concerns regarding removing the barricade on Tucker far outweigh any traffic and safety concerns expressed by the residents on Drake and Standish.
 - i. Poor line-of sight while turning left onto John R.
 - ii. Tucker street is not comparable to Drake or Standish.

There are no sidewalks on Tucker. Children often play and have to ride their bikes in the street. On a daily basis, children and residents from the adjacent subdivision walk down Tucker road to go to 7-Eleven. I know this first hand because I see the residents walking back with slurpees. Vehicles will have a straight path to speed down Tucker. Opening the road will result in safety

concerns for not only the children and residents of Tucker but also the residents from the adjacent subdivision because there are no sidewalks on Tucker.

In closing, for the past 30 plus years, no one in the surrounding subdivision of Tucker has complained about the barricade on Tucker Dr. Now, all of a sudden, the barricade is an issue because a resident on Drake is upset because his street is being opened due to a new subdivision being built. There are no safety concerns with opening up Drake. The resident is simply upset with the increased traffic flow.

The Tucker residents on both sides of the barricade have spoken loud and clear in opposition of removing the barricade (per the signed **petition**). If the fire and police access is the major concern, then the most cost effective way for the city to resolve the issue is to install an EVA. Spending the Troy residents hard-earned tax money should not be decided by one disgruntled resident on Drake. If the city has extra budget money, please put the money towards the Troy schools and improving the educational resources for our children.

Thank you for your time and consideration.

Kind regards,

Monica **Hausner**

Kathy Czarnecki

From: Albert Hsu <ahsu1@sbcglobal.net>
Sent: Friday, July 31, 2015 10:51 AM
To: Planning
Subject: RE: Request to remove barricade on Tucker

I'm the resident of 2157 Radcliffe Drive. Here is my inputs regarding the subject.

I disagree to remove the barricade on Tucker. The barricade has been there since my family moved in in year 2000. It has never been an issue for us. We need enough exits for the subdivision, but we don't need too many exits. Before the Mayflower sub was built, there was no need to have the Tucker exit. After the Mayflower bus was built, we have one more exit (Mayflower exit). Why do we need even more? I don't see any reason to support the barricade removal on Tucker.

Thanks,
Albert Hsu
Tel: 248-289-7012[H], 248-635-8410[Cell]; email: ahsu1@sbcglobal.net

Kathy Czarnecki

From: Brent Savidant
Sent: Wednesday, August 05, 2015 3:29 PM
To: Kathy Czarnecki
Subject: FW: TUCKER DR

Comment for Planning Commission

-----Original Message-----

From: petrulupas [mailto:petrulupas@yahoo.com]
Sent: Wednesday, August 05, 2015 2:16 PM
To: Brent Savidant <SavidantB@troymi.gov>
Subject: Re: TUCKER DR

Yes Sir

Do not ask the Lord to guide your footsteps, if you are not willing to move your feet.
Peter & Nellie Lupas

> On Aug 5, 2015, at 1:17 PM, Brent Savidant <SavidantB@troymi.gov> wrote:

>

> Mr. Lupas:

>

> Thank you for the email. It is addressed to me. Is it your intent that it be forwarded to the Planning Commission as part of the public record?

>

> R. Brent Savidant | Planning Director

> City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3364 | Cell: 248.943.0821 troymi.gov

>

> "We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

>

>

>

> -----Original Message-----

> From: petrulupas [mailto:petrulupas@yahoo.com]

> Sent: Wednesday, August 05, 2015 12:46 PM

> To: Brent Savidant <SavidantB@troymi.gov>

> Subject: TUCKER DR

>

> Dear Mr. Savidant.

>

> My name is Peter Lupas. I live at 2197 Tucker. I've lived in Troy for the past 25 years and on Tucker for the past 20. I am opposed to opening the barricade. Not only am I opposed, but dozens of other residents who live on both sides of Tucker (the paved and the gravel). How is it that you allow a resident who doesn't even live near Tucker to propose such a drastic change that will affect others and not him?

> Below is an exact quote of what you said at the March 24, 2015 planning commission meeting.

> "I don't think removing the barricade on Tucker is an option. This is an act of City Council. It was approved in 1990 as an act of City Council with the condition that it would be paved. I don't believe there is money on the budget. This has

never been discussed at a budget meeting that I'm aware of. And I think it's a bit of a red herring to talk about that as an option at this time. I don't know enough about eliminating the barricade on Tucker. It's been there for 25 years and I don't believe it's an option right now".

>

> Mr. Savidant, your comments are very clear. It's pretty obvious that you see the same thing I see: the Konarskes used Tucker barricade issue at that meeting to distract the planning commission from the real topic of discussion which was opening Drake. Mr. Savidant, we the residents on Tucker aren't blind and we're not stupid. We see clearly what this has evolved into since the March 24. It's just politics.

> The Konarskes have lived on Drake for the past 20 years. Why haven't they cared about the emergency vehicle access on Tucker before now? Why haven't they emailed the city about the city's connectivity policy until now? Why? I'll tell you why. Because I don't believe they really care about emergency vehicle access or connected streets. They are just ticked that the planning commission approved to open Drake in front of their house. And the only way they can get back at the city is by retaliating against a planning commissioner who lives on Tucker. They are turning him into a scapegoat just because he voted with the majority to open Drake. Even without his vote it would have been a 5-0 vote. If you are unwilling to acknowledge this then you are just turning a blind eye to all the evidence that points to this as being retaliation.

> One more thing I wanted to make sure is very clear for the record. In your letter to the traffic committee dated June, 30 2015 you stated very clearly in your opening sentence. "This item was initiated by James and Dorothy Konarske". Removing the barrier wasn't initiated by you, the City Manager, the city attorney or any other staff member. The Konarskes are the ones who decided that someone else's street needs to be opened. Not one single person in the City Management ever had an issue with this barricade for the past 25 years. And all of a sudden the Konarskes have enlightened you to the idea that the barricade needs to be removed to "improve our neighborhood". Really? Why haven't any of you in management had the foresight to propose opening Tucker before the Konarskes initiated this request?

> This issue is not what it appears on the surface. The residents who proposed this made it appear that their concern was safety and street connectivity. But I and other neighbors have our doubts about their real motivations. If this barricade gets removed I, along with other neighbors plan to pursue all our legal options to bring to light the real motivation behind this initiation to open Tucker.

> I respectfully request you leave Tucker as is. The neighborhood has functioned just fine like this for the past 25 years. Plus the subdivision will have three new roads on which to access to John R. Mayflower, bridal path and Kingston. This is a huge improvement for emergency vehicles and for street connectivity from what it was for the past 25 years. This is not an issue worthy of litigation. The city has bigger fish to fry and more important things to spend staff resources on.

> 08/05/2015

> Thank you

> Peter Lupas

>

>

>

Kathy Czarnecki

From: Rachele Lyngklip <lyngklipr@aol.com>
Sent: Wednesday, July 15, 2015 11:24 AM
To: William J Huotari
Subject: Tucker Dr.

I am writing to voice my opposition to the opening of the barrier on Tucker Dr in Troy Mi. There is no change that necessitates the opening of this Street. This street has been closed the entire time I have lived on Tucker without incident. I challenge the legal standing that the couple that lives on Drake to initiate this request with the city. I don't understand what interest that they have that would justify their actions in initiating this matter. There are currently two additional entryways to John R road that are open (Mayfield) and Duke that allow access from the new developments. We do not have any new building or developments that necessitates the additional access to John R. In addition, East Tucker is not paved and there is no sidewalks. Tons of kids use this street to walk or ride their bikes to 7 Eleven. It would be very dangerous to open the barrier to cars without the city also putting in sidewalks. It would be a lawsuit waiting to happen with the severe consequence being a pedestrian injury or death. In addition, the intersection of John R and Long Lake is very busy with frequent accidents. Opening Tucker (a street so close in proximity with the Long Lake and John R intersection) would simply make traffic issues and traffic safety more of a concern. For these reasons, I am strongly against the opening of the barrier on Tucker Dr. Please feel free to contact me with any additional questions.

Sincerely,

Rachele and Pete Lyngklip
2262 Tucker Dr. Troy MI

Rachele Lyngklip

CAbi Fashion Consultant

phone: (248)506-0878

email: lyngklipr@aol.com

view the current season at www.rachelelyngklip.cabionline.com

for the latest fashion fun, visit the CAbi blog at www.cabionline.com/blog



Kathy Czarnecki

From: Mykola Murskyj <mmurskyj@gmail.com>
Sent: Tuesday, July 14, 2015 8:21 PM
To: William J Huotari
Subject: Opening Tucker

Attention: Mr. Huotari, Traffic Engineer

I have been a resident in the Long Lake Meadows subdivision for over twenty years and our street intersects with Tucker.

In response to your notice of June 30th, I have waited a very long time and would sincerely appreciate the simple barrier removal on Tucker. This would effectively enhance response time for all emergency vehicles, greatly remove resident concerns and is the most economical method for resolving this issue.

Please open up Tucker and keep me updated with any future developments.

Thanks,

Mykola

Kathy Czarnecki

From: Mykola Murskyj <mmurskyj@gmail.com>
Sent: Tuesday, July 14, 2015 8:21 PM
To: William J Huotari
Subject: Opening Tucker

Attention: Mr. Huotari, Traffic Engineer

I have been a resident in the Long Lake Meadows subdivision for over twenty years and our street intersects with Tucker.

In response to your notice of June 30th, I have waited a very long time and would sincerely appreciate the simple barrier removal on Tucker. This would effectively enhance response time for all emergency vehicles, greatly remove resident concerns and is the most economical method for resolving this issue.

Please open up Tucker and keep me updated with any future developments.

Thanks,

Mykola

Kathy Czarnecki

From: barb northam <waba59@wowway.com>
Sent: Tuesday, July 14, 2015 5:01 PM
To: William J Huotari
Subject: Barricade on Tucker

My name is Barbara Northam and I live at 5241 Standish. Recently I received a notice regarding the removal of the barricade on Tucker. As long as Tucker remains a dirt road on the west side of the barricade, I feel that traffic in that area should NOT be increased by removal of the barrier. We currently have another exit from our sub on the north and I understand that a paved Drake will eventually be opened to John R. With that street then being accessible via two other streets, I see no need to open a dirt road to more traffic. Thank you for your consideration in this matter.

Barbara J. Northam

--

--

WOW! Homepage (<http://www.wowway.com>)

Kathy Czarnecki

From: Keller, Chuck <ckeller@rcoc.org>
Sent: Wednesday, July 15, 2015 12:03 PM
To: William J Huotari; Sintkowski, Scott
Subject: RE: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Bill:

I had a call from Steve Dearing, OHM, yesterday inquiring about the same thing. I searched the files we have in the Traffic-Safety Department and couldn't find the mentioned report. If my memory serves me correctly, when the church proposed a driveway connection to Tucker, the residents were against it and a driveway was proposed out to John R Road. I believe it was the driveway out to John R Road that was the concern being located so close to the bridge. This is why the church driveway connects with Long Lake Road.

Looking at this location today, it appears that the approach of Tucker to John R Road is within current RCOC Guide for Corner Sight Distance requirements.

Chuck

From: William J Huotari [mailto:HuotariWJ@troymi.gov]
Sent: Wednesday, July 15, 2015 11:13 AM
To: Sintkowski, Scott; Keller, Chuck
Subject: John R - Tucker Barricade - 2075 E. Long Lake - Bethesda Romanian Pentecostal Church

Scott & Chuck, we have an item on our Traffic Committee agenda tonight to discuss the removal of a barricade on Tucker, on the east side of John R, north of Long Lake.

I have heard reference to a "report" from the RCOC regarding limited site distance at the end of Tucker and/or related to a proposed driveway from the church to John R back in 1996/1997.

Would either of you have a copy of the report, memo, whatever that might shed some light on this claim? I have asked for a copy of the report from the resident but they have not provided anything to date.

Thanks, Bill

This was from one of the emails that I received prior to the meeting tonight:

1. There is limited site distance at the end of our street as determined by the Oakland County Road Commission report dated May 2, 1996. This is due to the concrete barrier over the Gibson Drain located south of Tucker Drive on John R.

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."

Kathy Czarnecki

From: Gerry Seip <justgerrya@yahoo.com>
Sent: Tuesday, July 14, 2015 10:10 AM
To: William J Huotari
Subject: Tucker Barricade

We are residents at 5297 Standish Dr. at the corner of Drake. We have lived in our home since it was built in 1991. We are very much in favor of the Tucker barricade being removed for the following reasons: 1) it would be consistent with the opening of all stub streets to the main roads 2) it allows a very clear access to the Long Lake Meadows subdivision for the reasons stated by the Fire Chief, Police Chief, etc. 3) it will alleviate traffic flow through Mayflower and Drake (when it is opened) and give residents a third access from John R. We are unable to attend the meeting, however, we would like you to consider our thoughts when voting on this matter. Also, it is our opinion that it would be economical for the city to pave Tucker at the same time that John R is being widened. Thank you for your consideration, Chuck & Gerry Seip

Kathy Czarnecki

From: David J Roberts
Sent: Tuesday, July 14, 2015 7:44 AM
To: Kurt Bovensiep; William J Huotari; Steven J Vandette; William S Nelson
Subject: RE: EVAs and collapsible barricades in the City or Troy

To my recollection, the first "conceptual" EVA was an access gate installed at the Oakland-Troy airport off of Coolidge Hwy, probably back in the 70's. and then later on off of Equity Drive. The next closest idea of an EVA was along the north side of 600 Wilshire when the building was constructed in the late 90's early 2000's, in the form of grass pavers with delineators. Back then, we had the idea that we needed alternate emergency vehicle access, but had no standard for design.

Over the years we came up with the red EVA delineator design in combination with a No Parking sign, and that has evolved into several different variations based on location. The FD has been somewhat lenient on the design, as long as we have access. At some point several years ago, someone (I thought from Streets) suggested the collapsible barricade, but we never adopted that as a standard.

I do not recall the FD ever having to use an EVA for an incident except at the airport a couple of times over the years. EVAs are like fire extinguishers, they're made available just in case they're needed in an emergency. ☺

Dave

From: Kurt Bovensiep
Sent: Friday, July 10, 2015 8:39 PM
To: William J Huotari; David J Roberts; Steven J Vandette
Subject: RE: EVAs and collapsible barricades in the City or Troy

Didn't we have something different then the current standard at Boyd and Hartland? Seems to me we did not have wooden posts at one time.

In regards to use; I am only aware of replacing them because of accidents or snow removal within the last three years.

Sent from my Verizon Wireless 4G LTE smartphone

----- Original message -----

From: William J Huotari
Date: 07/10/2015 7:53 PM (GMT-05:00)
To: David J Roberts ,Kurt Bovensiep ,Steven J Vandette
Subject: Fwd: EVAs and collapsible barricades in the City or Troy

Any idea on these questions?

Thanks, Bill

Sent from my iPhone 6+

Begin forwarded message:

From: semidaf@yahoo.com
Date: July 10, 2015 at 3:13:25 PM EDT
To: William J Huotari <HuotariWJ@troymi.gov>
Subject: Re: EVAs and collapsible barricades in the City or Troy

Hi Bill,

Thank you for your quick reply.

Hopefully my last 2 question before the Wednesday's hearing:

- when(what year) did Troy start installing EVAs;
- how many times where the EVAs actually used - let's say in the last 10-20 years. Usage defined in terms of having to replace the wooden posts.

Much appreciated,

Semida

On Jul 9, 2015, at 3:17 PM, William J Huotari <HuotariWJ@troymi.gov> wrote:

Semida, I am not aware of any of those types of barriers in Troy.

The EVA's that we install are of the wood post variety with holes drilled for breakaway.

Thanks, Bill

-----Original Message-----

From: semidaf@yahoo.com [<mailto:semidaf@yahoo.com>]

Sent: Thursday, July 09, 2015 3:16 PM

To: William J Huotari

Cc: semidaf@yahoo.com

Subject: Re: EVAs and collapsible barricades in the City or Troy

Bill,

Thank you very much for the information you have provided. It is perfect.

In regards to the collapsible barrier subject:

- is there any barrier within the city of Troy that looks like a permanent barrier that is actually collapsible when you push on the barrier and try to drive over it. It can either be pylons with collapsible shear plates at the bottom or a continuous

solid barrier - guard rail type - that has the legs collapsible and you can push the whole barrier down and drive over it.

These are in fact EVA type barriers that look like permanent/ solid ones.

City of Troy might not have any barriers of this type,

Thanks again,

Semida

Kathy Czarnecki

From: William J Huotari
Sent: Friday, July 17, 2015 2:28 PM
To: Brian M Kischnick; Mark F Miller; Brent Savidant
Cc: Lori G Bluhm
Subject: FW: Traffic Committee meeting, Wednesday 7/15/2015

From: Lori G Bluhm
Sent: Friday, July 17, 2015 2:22 PM
To: William J Huotari
Subject: FW: Traffic Committee meeting, Wednesday 7/15/2015

Lori Grigg Bluhm | City Attorney

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3323 | Cell: 248.885.1899

Fax 248.524.3259 | BluhmLG@troymi.gov  

“We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best.”

From: semidaf@yahoo.com [<mailto:semidaf@yahoo.com>]
Sent: Friday, July 17, 2015 2:17 PM
To: Lori G Bluhm <BluhmLG@troymi.gov>
Subject: Traffic Committee meeting, Wednesday 7/15/2015

Dear Ms. Bluhms,

I was in attendance at the Traffic Committee meeting , on Wednesday, July 15 2015.

I would like to start my note by thanking Mr. Huotari and the other City Staff members for facilitating this meeting and providing a forum where the resident’s opinions can be heard.

However, I cannot have the same words of appreciation for Mr. Richard Kilmer, member on the Traffic Committee board. I am still shocked and in disbelief of what I’ve witnessed at this meeting.

By participating and listening to the comments Mr Kilmer was making it was very obvious that he was strongly biased against the Tucker residents (which 100% signed the petition against the barrier removal) and had a personal agenda that he was trying to get across. He didn’t seem very interested with following proper meeting procedures (that the Chairman explained on multiple counts).

Mr. Richard Kilmer was speaking with a raised voice (almost approaching yelling levels) addressing (looking and pointing in the direction of) the Tucker residents that voiced their concerns about removing the barrier and making denigratory remarks like:

"I'll give you a shovel to dig the wooden post out of the ground in case that the fire department engine will ever have to use the EVA (Emergency Vehicle Access)".

Mr. Kilmer also commented in a negative way about the road maintenance yearly cost that was mentioned by Mr. Bovensiep . It sounded like the hard working Tucker residents are not paying their taxes are just taking advantage of the services that the Troy DPW has to offer.

It is also my understanding that a student representative (Katie Regan) also participates at these Traffic Committee meetings. She wasn't present at the one last Wednesday. It was probably for her own good that she didn't witness first hand Mr. Kilmer's behavior which would not qualify as "democracy at work".

In my opinion Mr. Richard Kilmer exhibited the behavior of a "bully", trying to intimidate the hard working Tucker residents that had the right to present their view (even if not in agreement with Mr. Kilmer's preconceived agenda).

We, as parents, and the Troy schools, that our kids proudly attend, are teaching them from an early age that bullying is not a behavior that should be condoned, nor encouraged. Even at the school level the reprimands for bullying are very severe.

When the discussion came to the point of amending one of the motions that were presented for the Traffic Committee to vote on ("adding the words -- "encourage the installation of an EVA barrier") Mr. Kilmer started yelling "No,No,No" without even listening to what was being proposed.

Even the Committee chairman (Mr.Ziegenfelder) had to directly address and caution him, on multiple times, to come to order and follow proper procedure (i.e. listen to what he's supposed to Vote on before saying a straight NO).

Based on the above evidence, which you also witnessed first hand, I would like to respectfully request that Mr. Kilmer's vote be null and void. Mr. Kilmer's disregard of the Traffic Committee procedures (which he should be very familiar with) should not allow his vote to be counted for this Agenda item.

I would also respectfully request that Mr Kilmer be removed from the traffic committee. I understand that he's volunteering on the Traffic Committee, like any other members on the other committees. However the committee members represent the citizens of Troy and should listen and take into consideration all the evidence presented. If we cannot trust our fellow neighbors, what confidence can we have that fair and proper procedures are followed and we're not just dragged through these Committees to satisfy somebody's hidden agenda.

The residents of Tucker Road have spoken loud and clear when ALL of them (both sides of the barrier, paved or not paved) signed the petition, and a very large number of them were present in the audience, that the barrier should stay in place and an EVA be installed for safety and emergency vehicles access.

In conclusion I would like to ask your opinion, as the head of the Legal Department for the City of Troy, that was consulted before this request was allowed to pursue further (since it was initiated by the City of Troy Manager's Office based on one e-mail received from Mr Konarske, resident of Drake).

Why are we still moving ahead with this issue? Isn't the loud and clear voice of ALL (100%) of Tucker residents enough reason to conclude that the MAJORITY has spoken and that the MINORITY shouldn't decide/ impose their will.

In my opinion that would be very much in line with "*we believe doing government at best*" motto that the City of Troy has enthusiastically embraced.

I'm sure that the City, including your Department's resources, would be better used on more pressing issues that the City is experiencing.

Thank you for your time and attention,

Sincerely,

Semida Fratila

Kathy Czarnecki

From: Deb Tosch <mstgarden@gmail.com>
Sent: Thursday, August 06, 2015 10:14 AM
To: Planning
Cc: John Tosch; Ellen C Hodorek; Dane Slater; Jim Campbell; Dave Henderson; Ed Pennington; Doug Tietz; Steve Gottlieb; chausner@gmail.com; dansemi@yahoo.com
Subject: Request to Remove Barricade on Tucker
Attachments: Ruby-Colleen.JPG; Devonwood Troy Mi.JPG; Beech Lane Dr..JPG; Troywood EVA.JPG; Boyd Street EVA 1.JPG

Follow Up Flag: Follow up
Due By: Thursday, August 06, 2015 2:00 PM
Flag Status: Flagged

We are Deb and John Tosch from 2088 Tucker Drive and we have lived in Troy since 1978. We are adamantly against the removal of the barricade separating us from Spring Meadows subdivision.

Our reasons are as follows:

1. The City Council already reviewed this issue in 1987 and determined Tucker Drive should remain closed until such time as further development on Tucker occurs. There has been no further development to warrant the removal of the barrier.
2. The City's Policy of interconnectivity is very inconsistent. We have three such inconsistencies in this developed one square mile area alone.
 - The first is Endicott to Oakwood in the northwest corner. This was probably thought best because the traffic to bypass the light at Square Lake would have been severe. However, the streets are curved which would have slowed the traffic considerably.
 - The second is Ruby to Colleen in the northeast corner. These two streets were not connected but these two streets also have curves which would have slowed traffic as well. Instead of connecting, this is an EVA only.
 - Third is Sweet to Mayflower which is part of the Bridgewater Estates development. Instead of opening Sweet to Mayflower as well as to Standish, this connection was not made. Why?

Another question is why is Mr. Miller involved with this issues? He is in charge of economic development. What does the removal of the Tucker barricade have to do with economic development for Tucker or for that matter Troy, MI? Wouldn't his time be better spent filling the empty office space in Troy? We have been told we cannot speak with the police or fire officials only Mr. Miller. However, the one person who has filed a complaint has been allowed that opportunity. Why? Does Mr. Miller and the complainant have a personal relationship or is some financial gain to be realized? Is this how decisions are made in Troy, based on relationships, and not the voice of the taxpayers? Mr. Miller is not even a resident of Troy and not a tax payer. All of the tax-paying residents on Tucker Drive oppose the removal of the barricade.

Tucker Drive is a quarter mile from a major intersection. Traffic going north during rush hour is tremendous. Opening up Tucker Drive will provide **over a quarter mile of straight road** and the first opportunity for the entire square mile of homes to exit John R and head home more quickly than driving up to the 4 and soon to be 5 access points. All of these access points were designed with curves to slow traffic with the exception of Highbury. When my husband and I lived on Highbury and were fighting for a stop sign at Endicott (which was denied), the city traffic engineer stated that Highbury was not designed correctly and newly developed streets in Troy are no longer designed with long straightaways.

We would like to address another issue that has been brought up and that refers to the comment that gravel roads are costly to maintain. According to a study published by NPR News on October 26, 2010, the cost to maintain a paved road over the entire life cycle is about 3 times more than maintaining a gravel road. Looking at the cost on a year to year basis is short sighted. "Over a 40 year lifespan, a low-volume paved road will need to be chip sealed twice, undergo overlay once and then reclaim/overlay. The yearly maintenance costs of gravel roads make them appear inefficient until you consider the capital improvement costs associated with bituminous roads at the middle and end of their lives, which isn't always reflected in yearly maintenance figures." The study was for streets in Minnesota north of the Twin Cities. Their climate is similar with cold winters; however, they do not have the freeze-thaw cycles that we have that shortens the lifespan of the roads in Southeast Michigan. Many communities in the upper portion of the United States, Michigan included, are turning paved roads back to gravel as the cost of maintaining roads skyrockets (Wall Street Journal, July 10, 2010)

The last issue is the comment that Tucker is a public road and everyone should have access. Yes Tucker is a public road; however there are numerous examples of public roads in Troy that are blocked from all drivers. A few examples:

1. Colleen to Ruby (attachment #1) an EVA blocks access. Why?
2. Devonwood (attachment #2). Subdivision built up to the largest portion of gravel road in Troy. These were not connect and two barriers block the two sections. Why?
3. Beech Lane Drive (attachment #3) has a barricade in the middle of the street. Why?
4. Troywood (attachment #4). A new sub was built and instead of interconnectivity, an EVA was approved. Why?
5. Boyd-Harmony (attachment #5, #6). This is interesting when you view the picture. Why?

There are a total of 13 EVAs in Troy. There is a precedent now for public roads remaining blocked for obviously various reasons. We have not been able to find any criteria for decision making on an EVA.

This is a **personal vendetta** to the residents of Tucker. For over 27 years the residents of Drake and Standish lived on dead end streets. At no time during this period was there a concern about the response time from police and fire. We were told that the opening of Tucker would increase the response time by 30 seconds now that the Mayflower/Standish access was made. The response time during the 27 years was much longer, but obviously this was not a problem for them. Therefore the only reason this request is being made is to divert the traffic from Standish/Drake to Tucker. This was confirmed to me personally by a resident of Standish who resides at the connection point at Mayflower.

The opening will also inflict a financial burden on the 14 residences who would have to bear the burden of a tax levy to pave the road. A levy that as yet the City of Troy has never pushed. All of the requests to pave the gravel roads have come from the residents and then 60% have to agree before the road is paved.

If fire and public safety is at issue, then the best solution would be to make the barricade into an EVA.

Sincerely,

John and Deb Tosch

DO NOT
E.V.A.
ENTER

NO
PARKING
FIRE
LANE

DO NOT
E.V.A.
ENTER

NO
PARKING
FIRE
LANE

E
V
A



ROAD
ENDS



ROAD
ENDS



RESERVED
VEHICLES
ONLY

NO
PARKING
FIRE
LANE

E
V
A

E
V
A



RESERVED
VEHICLES
ONLY

NO
PARKING
FIRE
LANE

ywood Dr



Boyd St

14.56016

Kathy Czarnecki

From: William J Huotari
Sent: Wednesday, July 15, 2015 3:27 PM
To: 'Al Petrusis (Traffic Comm)'; 'David Easterbrook (Traffic Committee)'; 'Katie Regan (Traffic Comm - Student Rep)'; 'Pete Ziegenfelder (GM)'; 'Tim Brandstetter (Kimley-Horn)'
Cc: Michael D Szuminski; Eric Caloia; Lori G Bluhm; 'Steve Dearing'; Mark F Miller; Brent Savidant; Brian M Kischnick
Subject: RE: Tucker Barricade - Map Showing Locations of Emails Received
Attachments: Email Results.pdf

Attached is a depiction of the locations of residents that want to KEEP the barricade (red box with an X) and those that want to REMOVE the barricade and OPEN Tucker (green box with an O).

Where a number is shown in a circle is an address where more than one email was received from the same address.

If you remove the multiple responses from the same address, the tally becomes:

- 19 oppose removing the barricade
- 4 support removing the barricade

I have been told that there will also be a petition submitted tonight that has 62 signatures (as of earlier today) of residents that OPPOSE removing the barricade.

Thanks, Bill

From: William J Huotari
Sent: Wednesday, July 15, 2015 11:48 AM
To: 'Al Petrusis (Traffic Comm)'; 'David Easterbrook (Traffic Committee)'; Katie Regan (Traffic Comm - Student Rep); 'Pete Ziegenfelder (GM)'; 'Tim Brandstetter (Kimley-Horn)'
Cc: Michael D Szuminski; Eric Caloia; Lori G Bluhm; 'Steve Dearing'; Mark F Miller; Brent Savidant; Brian M Kischnick
Subject: RE: Tucker Barricade - Emails Received

Attached are additional emails received after Tuesday, July 14, 2015 8:09 AM to today.

Current tally is 28 emails received:

- 21 oppose removing the barricade
- 7 support removing the barricade (although 4 emails are from the Murskyj's, all with different email addresses)

Thanks, Bill

From: William J Huotari
Sent: Tuesday, July 14, 2015 8:09 AM
To: Al Petrusis (Traffic Comm); David Easterbrook (Traffic Committee); Katie Regan (Traffic Comm - Student Rep); Pete Ziegenfelder (GM); Tim Brandstetter (Kimley-Horn)

Cc: Michael D Szuminski; Eric Caloia; Lori G Bluhm; Steve Dearing; Mark F Miller; Brent Savidant; Brian M Kischnick

Subject: Tucker Barricade - Emails Received

In order that you have some background prior to the meeting, attached are emails regarding the Tucker Barricade that I have received as of this morning. I will have hard copies for you at the meeting, so you don't have to print them out.

I will also provide copies of additional emails that I receive between now and the meeting tomorrow night at the meeting.

I will call Richard and Cynthia so that they can have copies before the meeting.

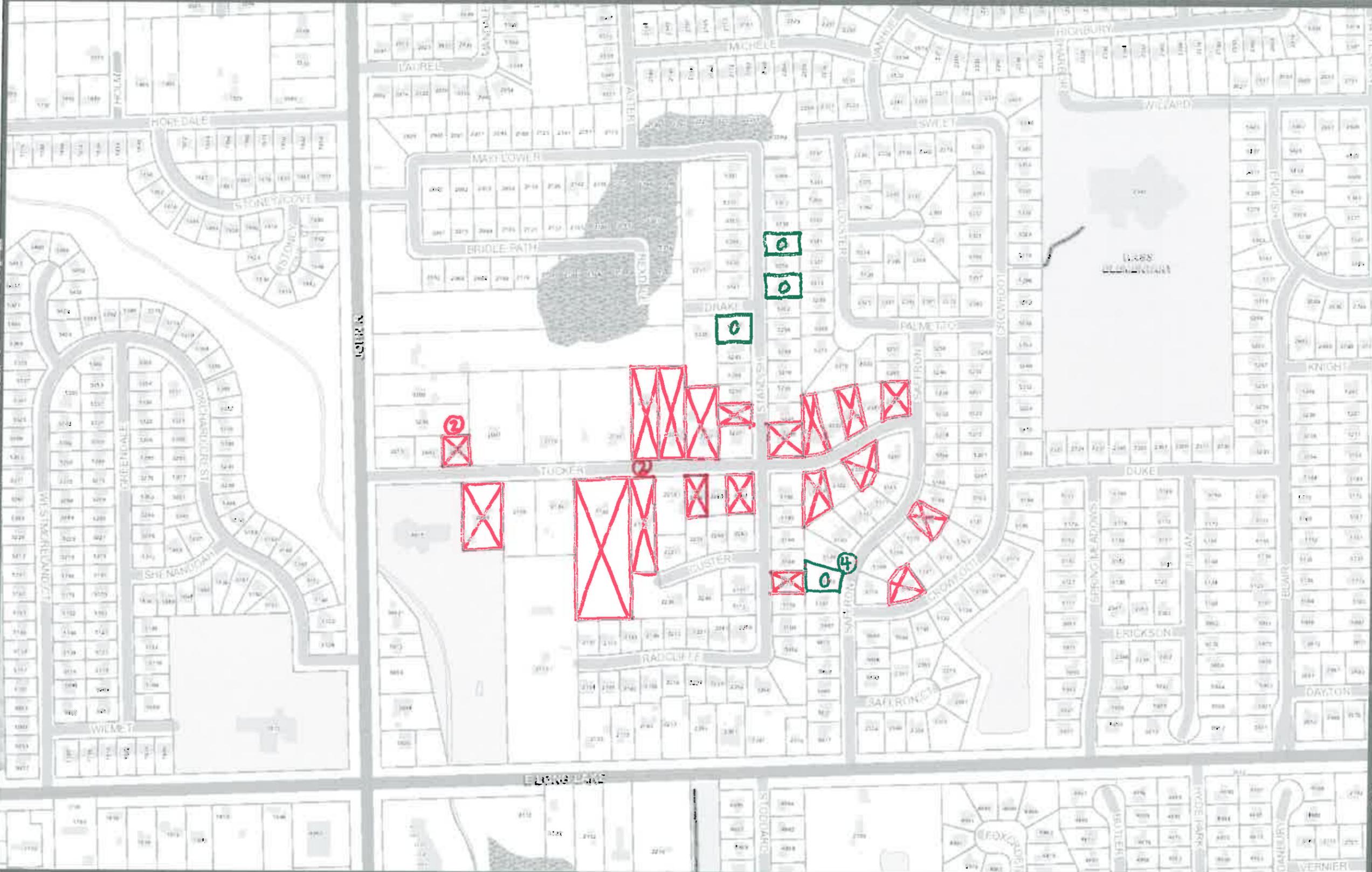
Please DO NOT "Reply All" if you have a question or a comment. Just reply to me directly to avoid any unintended violations of the Open Meetings Act.

Thanks, Bill

William J. Huotari, P.E. | Deputy City Engineer/Traffic Engineer

City of Troy | 500 W. Big Beaver, Troy, MI 48084 | Office: 248.524.3387 | troymi.gov  

"We believe a strong community embraces diversity, promotes innovation, and encourages collaboration. We strive to lead by example within the region. We do this because we want everyone to choose Troy as their community for life. We believe in doing government the best."



Legend

- ✘ - KEEP BARRICADE
- - OPEN BARRICADE
- ② - MULTIPLE EMAILS FROM SAME ADDRESS

1:5,741



Notes

956.8 0 478.42 956.8 Feet

WGS_1984_Web_Mercator_Auxiliary_Sphere

Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

7/15/15 - 3:07 PM

Kathy Czarnecki

From: Nick Vendittelli <nvendittelli@wowway.com>
Sent: Tuesday, July 14, 2015 10:12 AM
To: William J Huotari
Subject: July 15, 2015 Meeting Agenda item Remove Barricade on Tucker

Thank you for the notice asking for our input in regards to removing the barricade on Tucker. I will not be able to attend the meeting, but would like to provide my comments. I am not in favor of removing the barricade. We have live on Standish for over 24 years, one of the original owners. For all these years we have not had Tucker open, and we are used to it. We now have Mayflower open to John R and my understanding is that Drake will also open on to John R. So we had no access to John R for 24 years and now will have two roads connecting to John R, which is more than enough for the amount of traffic and makes it unnecessary to have Tucker opened as well. From a financial point the added expense to the residences on Tucker if the road were to be paved or to the city for the multiple regrading and gravel of Tucker during a year which will be required if it were to remain a gravel road is a waste of money, with Mayflower and then Drake, both paved, being opened. To me the expense either way is not justified over a perceived traffic flow issue.

Sincerely

Nick Vendittelli

5132 Standish, Troy

DATE: August 7, 2015

TO: Planning Commission

FROM: R. Brent Savidant, Planning Director

SUBJECT: PRELIMINARY SITE PLAN REVIEW (File Number SP 1009) – Proposed Sedona Taphouse Restaurant, South side of Big Beaver, East of Livernois (198 E Big Beaver), Section 27, Currently Zoned BB (Big Beaver) District

The petitioner Knollenberg Hospitality LLC submitted the above referenced Preliminary Site Plan application for a proposed restaurant with rooftop seating at 198 E. Big Beaver.

The Planning Commission considered this item at the July 28, 2015 Regular meeting and postponed the item to provide the applicant with an opportunity to respond to comments made during the meeting and in the report.

The property is currently zoned BB (Big Beaver) Zoning District. The Planning Commission is responsible for granting Preliminary Site Plan approval for this item.

The attached report prepared by Carlisle/Wortman Associates, Inc. (CWA), the City's Planning Consultant, summarizes the project. CWA prepared the report with input from various City departments including Planning, Engineering, Public Works and Fire. City Management supports the findings of fact contained in the report and the recommendations included therein.

Attachments:

1. Maps
2. Report prepared by Carlisle/Wortman Associates, Inc.
3. Minutes (draft) from July 28, 2015 Planning Commission Regular meeting (excerpt).
4. Public Comment

G:\SITE PLANS\SP 1009 Sedona Taphouse Restaurant Sec 27\SP-1009 PC Memo 2015 08 07.docx

PROPOSED RESOLUTION

PRELIMINARY SITE PLAN REVIEW (File Number SP 1009) – Proposed Sedona Taphouse Restaurant, South side of Big Beaver, East of Livernois (198 E Big Beaver), Section 27, Currently Zoned BB (Big Beaver) District

Resolution # PC-2015-08-

Moved by:

Seconded by:

RESOLVED, That Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Sedona Taphouse Restaurant, located on the south side of Big Beaver and east of Livernois (198 E. Big Beaver), Section 27, within the BB (Big Beaver) District, be granted, subject to the following:

1. Indicate material use of retaining wall along eastern property line prior to Final Site Plan approval.
2. Indicate trash enclosure material screening prior to Final Site Plan approval.
3. Provide photometric plan and lighting fixtures in compliance with Article 13 prior to Final Site Plan approval.

_____) or

(denied, for the following reasons: _____) or

(postponed, for the following reasons: _____)

Yes:

No:

MOTION CARRIED/FAILED



Legend:

Aerial

- Red: Band_1
- Green: Band_2
- Blue: Band_3

200 0 100 200Feet

Scale 1: 1,200



CARLISLE

WORTMAN
associates, inc.

605 S. Main Street, Ste. 1
Ann Arbor, MI 48104

(734) 662-2200
(734) 662-1935 Fax

August 7, 2015

Preliminary Site Plan Review For City of Troy, Michigan

Applicant:	Knollenberg Hospitality LLC
Project Name:	Sedona Taproom
Location:	Southside of Big Beaver, east of Livernois
Zoning:	BB, Big Beaver Form-based district
Action Requested:	Preliminary Site Plan Approval

PROJECT AND SITE DESCRIPTION

We received a site plan and accompanying documents for a proposed commercial development on the southside of Big Beaver Road, east of Livernois. The 0.83 acre parcel is currently developed as a one-story office building.

The applicant is proposing to demolish the current building and construct a 5,100 sf free-standing Sedona Tap. Sedona Tap is a beer-based restaurant from Virginia. This is the sixth restaurant nationally and the first franchise in Michigan. The restaurant is proposing a rooftop bar/patio area that fronts on Big Beaver. Access to the site will be via the existing curb cut off Big Beaver.

The property is zoned Big Beaver Form-Based District. The proposed development and use are by-right and require Site Plan Review approval from the Planning Commission.

Location of Subject Property:
Southside of Big Beaver, east of Livernois

Proposed Uses of Subject Parcel:
Sedona Taproom

Current Use of Subject Property:
Office Building

Current Zoning:
The property is currently zoned Big Beaver Form Based Code, site type A



Direction	Zoning	Use
North	BB, Big Beaver Form Based	Office
South	BB, Big Beaver Form Based	Office
East	BB, Big Beaver Form Based	Office
West	BB, Big Beaver Form Based	Office

PREVIOUS PLANNING COMMISSION DISCUSSION

The item was last reviewed by the Planning Commission at their July 28th meeting. The item was tabled to allow the applicant to address the following:

1. Provide easement document for cross-access to the property to the south.

The applicant will be required to provide all necessary cross-access easements as part of the final site plan submittal. Cross-access easement documents are not required for preliminary site plan approval.

2. Reconsider the layout of greenbelts, drive-aisles, and pedestrian walkway within the parking lot including reducing greenbelts, increase the westernmost drive-aisle width, adding a minimum 5-foot wide pedestrian spine between the row of parking, and increasing westernmost stall length from 18 to 19 feet.

The applicant has amended their interior lot layout as recommended. The following changes have been made:

- Added additional landscaping buffer along western property line to address concerns raised by the adjacent owner at the previous planning commission meeting. The greenbelt is now six-feet in width between the edge of the parking lot and the western property line.
- Increased all drive aisle lengths to 15-feet in width to accommodate easier site circulation.
- Added a 7-foot wide pedestrian spine connecting the building to the parking lot to the south.
- Added a 0.5 to 2.5 foot high retaining wall along eastern property line. Material selection of wall has not been indicated.

3. Add a striped crosswalk across the drive-aisle from the pedestrian spine to the sidewalk along the rear of the building.

The applicant has added a striped crosswalk across the drive-aisle that connects the 7-foot wide pedestrian spine to the sidewalk along the rear of the building.

4. Provide site landscaping calculation.

	Greenscape	Hardscape	Total	Compliance
<u>Site landscaping:</u>	4,068 sq/ft required	1,355 sq/ft required	5,423 sq/ft required	Complies
A minimum of fifteen percent (15%) of the site area shall be comprised of landscape material. Up to twenty-five percent (25%) of the required	4,532 sq/ft provided as indicated on landscape	2,115 sq/ft provided as indicated on landscape plan	6,647 sq/ft provided as indicated on landscape plan	

landscape area may be brick, stone, pavers, or other public plaza elements, but shall not include any parking area or required sidewalks.	plan			
---	------	--	--	--

5. Indicate trash enclosure screening.

The applicant indicated that the trash enclosure will be screened with materials that are consistent with the building. The materials should be indicated on the final site plan submittal.

6. Provide samples, swatches, or manufacturer's specification sheets of the predominant proposed exterior materials and colors of all buildings and permanent structures, including walls and fences.

The applicant has been asked to provide material samples at the meeting.

7. Submit a lighting plan that complies with Article 13 of the Zoning Ordinance.

The applicant has not provided fixture cut sheets or a photometric plan. Compliance with lighting cannot be determined at this time. All lighting will be required to comply with Article 13.

SUMMARY OF FINDINGS

We support the development of this site, and find that the plan and development details are consistent with the vision along Big Beaver and the requirements of Big Beaver Form Based District. We recommend preliminary site plan approval with the following conditions to be addressed in the final site plan application:

1. Indicate material use of retaining wall along eastern property line.
2. Indicate trash enclosure material screening.
3. Provide photometric plan and lighting fixtures in compliance with Article 13.



CARLISLE/WORTMAN ASSOC., INC.
Benjamin R. Carlisle, LEED AP, AICP

9. PRELIMINARY SITE PLAN REVIEW (File Number SP 1009) – Proposed Sedona Taphouse Restaurant, South side of Big Beaver, East of Livernois (198 E Big Beaver), Section 27, Currently Zoned BB (Big Beaver) District

Mr. Carlisle reviewed the Preliminary Site Plan application. He addressed the site access, pedestrian and vehicular circulation, lighting plan, trash enclosure screening and landscaping. Mr. Carlisle recommended approval with the conditions as identified in his report dated July 21, 2015.

Mr. Savidant announced written comments from Ganesh Reddy of Mamta Holdings were distributed to Board members prior to the beginning of tonight's meeting.

Present were Martin Knollenberg, project architect Thomas Strat, Thomas Desmond of Thomas Strat Architects and landscape architect Brian Devlin.

Mr. Strat gave a presentation detailing the architecture of the proposed Sedona Taphouse Restaurant, featuring building materials, building transparency and rooftop outdoor seating.

There was discussion on:

- Vehicular and pedestrian circulation; narrowness of layout, drive aisle.
- Maneuverability of delivery trucks.
- Landscape requirements; hardscaping.
- Valet service.
- Hours of operation.

Chair Edmunds opened the floor for public comment.

Ganesh Reddy of Mamta Holdings, 3270 W. Big Beaver, addressed concerns with the proposed development as relates to the abutting property to the west, 100 E. Big Beaver.

Chair Edmunds closed the floor for public comment.

Discussion followed on:

- Existing cross access easement to the south; reciprocal agreement, use of property no effect on agreement.
- Required parking spaces; based on projected number of seats only, outdoor seating requires no additional parking.
- Deceleration lane; would be determined by County.
- Stormwater management; utilize regional detention or retain underground.

Resolution # PC-2015-07-049

Moved by: Tagle
Seconded by: Crusse

RESOLVED, That Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Sedona Taphouse Restaurant, located on the south side of Big Beaver and east of Livernois (198 E. Big Beaver), Section 27, within the BB (Big Beaver) District, be postponed until the applicant can respond to comments made this evening, specifically addressing conditions outlined in the proposed Resolution.

Yes: All present (7)
Absent: Sanzica

MOTION CARRIED



Hand delivered to
Planning Commission
Meeting July 28, 2015

July 27, 2015

Mr. R. Brent Savidant
Planning Director, City of Troy
500 W Big Beaver Rd
Troy, MI 48084.

Dear Brent:

We have been notified that the property located at 198 E Big Beaver is under consideration for a conditional rezoning and site plan approval for Sedona Tap House. We own the property at 100 E Big Beaver Rd and 30 E Big Beaver Rd.

We would like to raise some concerns and request modifications to the plan that will ensure that the new development does not adversely affect our property or that of other business owners in the area.

We would be grateful if you would consider the following items and modify the proposed plan accordingly:

1. **Access and Traffic Concerns.** The access from Big Beaver Rd. into the drive at 198 E. Big Beaver is insufficient to handle rush hour traffic as well regular business traffic for the proposed business use. Further, there is no room for an adequate deceleration lane after our eastern entrance. This is will be a dangerous place to exit and enter at peak times, putting drivers at unnecessary risk.
2. **Parking Concerns.** There is insufficient parking on-site at 198 E. Big Beaver, which will create an overflow parking situation on our property. We request that you require a fence and landscaping that will deter the patrons and/or employees from parking in our lot. We would reserve the right to tow cars not doing business in our building at the owner's expense.
3. **Future uses of the New Building.** Currently the proposed restaurant is expected to have 150 seats. The restaurant, however, is 5,100 square feet, leaving ample room for patrons to stand, as well as floor space to put additional tables/seating after approvals are obtained. As mentioned above, parking is insufficient to handle the expected volume of patrons (a) in the standing area, (b) people who may not be seated but who are waiting for a table, or (c) any parked demand created by the addition of unapproved seats after approvals are obtained. For reference, other restaurants in the area of the size of the proposed restaurant have 240 seats (not taking into consideration standing room patrons), which would require 120 parking spaces and additional parking for staff, considerably more than currently contemplated by the proposed plan.



4. **Storm Water Run-Off.** We are experiencing issues with the City of Troy storm sewer handling the amount of drainage that we currently have onto our property. We would like to ensure that the problem is not exacerbated by increasing the impervious area of the adjacent site. Looking at the elevations on the topographical survey and the site plan, we believe the storm water detention is likely insufficient to prevent off-site water entering onto our property. We are investing a lot to fix the existing problem and do not wish to have those efforts undermined (or have the problem exacerbated) by storm water run-off coming off their site onto ours.

5. **Future Development.** We plan to request an additional building on our site that will meet the City codes and further the creation of a cohesive development that is successful and desirable. We feel that adding a retail/restaurant to our parking area will enhance the ability to lease our existing space to qualified and upscale tenants. Any encroachment onto our property (especially of the sort contemplated by the proposed plan) will hinder our ability to create the best development possible with the highest quality of tenants.

Please take our requests into consideration prior to approving the plan as presented. We do not want to create an issue for our neighboring property and would like them to be able to use the property to its fullest extent. We simply want to ensure that such use is not at the expense of the neighboring properties.

Thank you for your consideration.

Sincerely,

Ganesh Reddy

Mamta Holdings, LLC.









KNOLLENBERG
AGENCY

198
E. BIG BEAVER

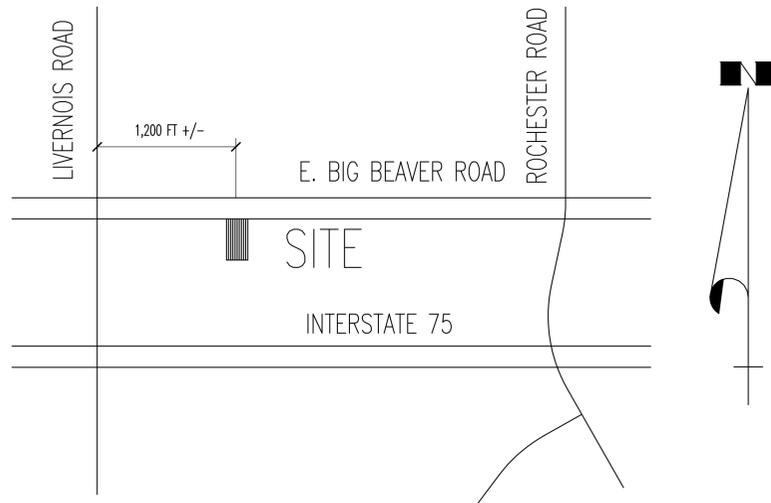


SITE PLAN

FOR REDEVELOPMENT OF

198 EAST BIG BEAVER ROAD

A PART OF THE NORTHWEST 1/4 OF SECTION 27, T-2-N, R-11-E
CITY OF TROY, OAKLAND COUNTY, MICHIGAN



LOCATION MAP

DESCRIPTION

PARCEL 20-27-101-061
(ADDRESS 197 E. BIG BEAVER ROAD, TROY, MICHIGAN)
A PART OF THE NORTHEAST 1/4 OF SECTION 27, T-2-N, R-11-E, CITY OF TROY, OAKLAND COUNTY, MICHIGAN
BEING MORE PARTICULARLY DESCRIBED AS LOT 10 EXCEPTING THE SOUTHERLY 200 FEET AND THE NORTHERLY 42
FEET FOR BIG BEAVER ROAD, OF "FROST SUNNY ACRES" SUBDIVISION AS RECORDED IN LIBER 64 OF PLATS,
PAGE 9 OAKLAND COUNTY RECORDS. CONTAINING 0.83 ACRES.

PROPRIETOR:

MARTIN J. KNOLLENBERG
198 E. BIG BEAVER ROAD
TROY, MICHIGAN 48083-1203
PH (248) 528-0200

ENGINEER/SURVEYOR:

SURVEY TECH
CIVIL ENGINEERING - LAND SURVEYING
3253 LYNHURST CT., OAKLAND MI 48306
CONTACT: MR. PAUL BOOMER (PROJECT MANAGER)
PH (248) 524-5254
MR. MAHER FAIK (PRINCIPAL)
PH (248) 670-65565
FAX (248) 377-8883

ARCHITECT

THOMAS STRAT
2605 CROOKS ROAD
TROY, MICHIGAN 48084
PH (248) 703-3361

LANDSCAPE ARCHITECT

J. BRIAN DEVLIN
31736 WEST CHICAGO AVENUE
LIVONIA, MI 48150-2830

SHEET INDEX

- 1) COVER SHEET
- 2) LANDSCAPE PLAN
- 3) EXISTING CONDITIONS PLAN
- 4) DEMOLITION PLAN
- 5) ENGINEERING SITE/PLAN
- 6)

BENCHMARKS:

CITY OF TROY BENCHMARK
BM 1295
NORTH RIM OF COMMUNICATIONS MH AT THE
NORTHEAST CORNER OF BIG BEAVER AND
HELENA ST
ELEV 671.94 (NAVD 88)
SITE BENCHMARKS
BM 1
RIM OF SANITARY SEWER MANHOLE LOCATED
55'+/- NORTHEAST OF THE NORTHWEST
PROPERTY CORNER, 5 FEET SOUTH OF THE
CURB OF BIG BEAVER ROAD
ELEV 672.00 (NAVD 88)
BM 2
P.K. NAIL IN THE SOUTHWEST FACE OF
UTILITY POLE LOCATED 35 FEET NORTH
OF THE SOUTHEAST PROPERTY CORNER
ELEV 670.15 (NAVD 88)

PROJECT MANAGER
PAUL J. BOOMER

ALL CONSTRUCTION SHALL BE IN
ACCORDANCE WITH THE MUNICIPALITY'S
CURRENT STANDARDS AND SPECIFICATIONS.

THE CONTRACTOR SHALL CONTACT
"MISS DIG", 1-800-482-7171, 72 HOURS
IN ADVANCE OF CONSTRUCTION FOR
EXISTING UNDERGROUND UTILITY
LOCATIONS.

THE CONTRACTOR SHALL NOTIFY THE
MUNICIPALITY AND/OR THE AUTHORITY
HAVING JURISDICTION, 48 HOURS PRIOR
TO THE BEGINNING OF CONSTRUCTION.

FULL TIME INSPECTION WILL BE
REQUIRED DURING THE INSTALLATION
OF SANITARY SEWERS, STORM SEWERS,
DRAINS, WATER MAINS, STREETS, AND
APPURTENANCES.

SHEET 1

NOVEMBER 2014

SURVEY TECH
CIVIL ENGINEERING AND LAND SURVEYING
3253 LYNHURST CT., OAKLAND TWP., MICHIGAN 48306
PH (248) 670-6556 FAX (248) 377-8883

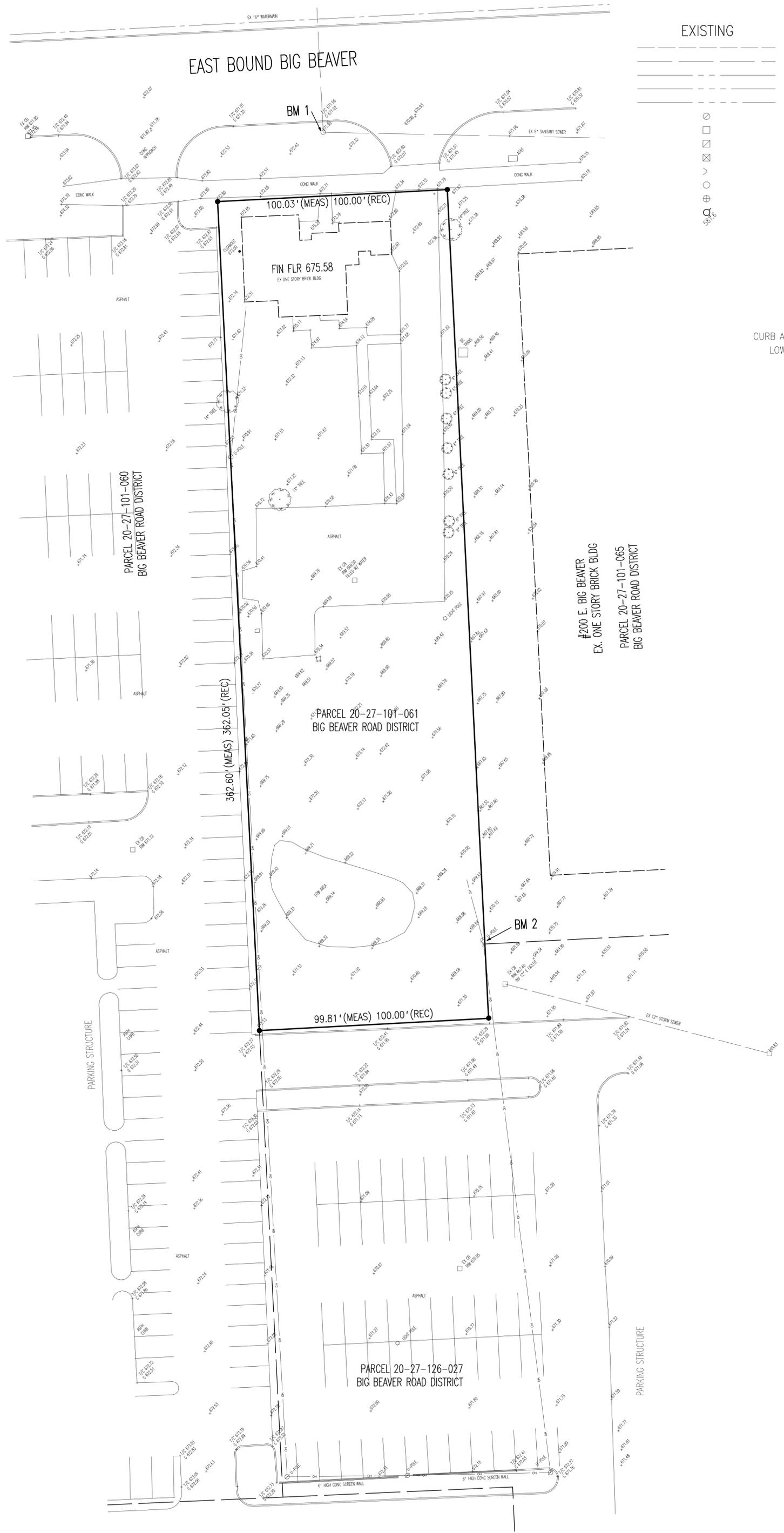
LEGEND

EXISTING

PROPOSED

- STORM SEWER
 - SANITARY SEWER
 - WATERMAIN
 - GAS MAIN
 - ELEC. TELE. CABLE
 - STORM MANHOLE
 - CATCH BASIN
 - INLET
 - REARYARD CATCH BASIN
 - END SECTION
 - SANITARY MANHOLE
 - GATE VALVE AND WELL
 - HYDRANT
 - GRADE
 - PAVEMENT GRADE
 - TOP OF CURB
 - TOP OF WALK
 - SWALE GRADE
 - SPOT GRADE/GUTTER
 - DRAINAGE AREA
-
- 91.7
 - P
 - T/C
 - T/W
 - SW
 - G
-
- CURB AND GUTTER INLET FILTER (SI-4)
 - LOW POINT INLET FILTER (SI-2)
 - SILT FENCING (SP-2)
 - GRADING LIMITS

EAST BOUND BIG BEAVER



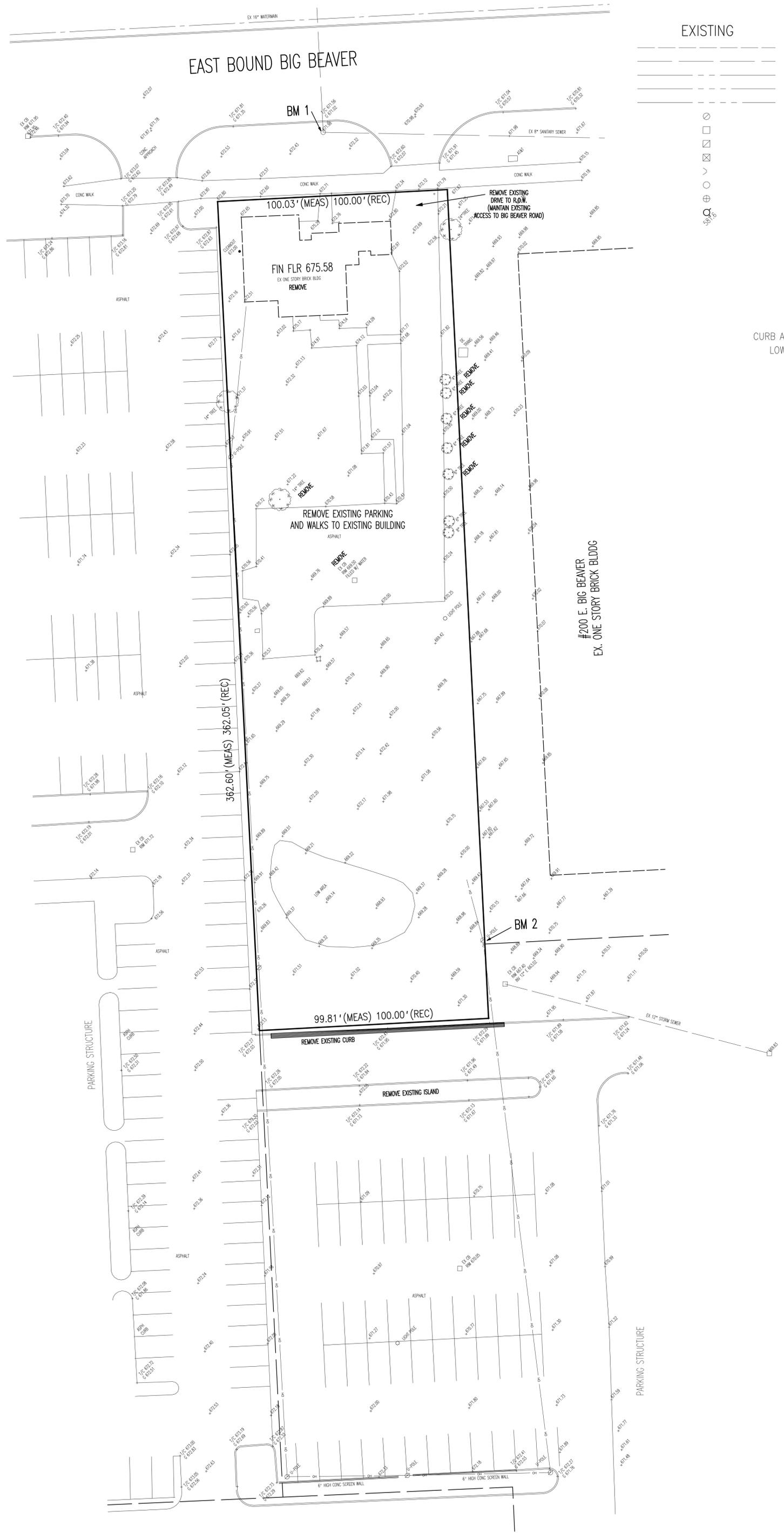
LEGEND

EXISTING

PROPOSED

- | | | |
|-----|-------------------------------------|------|
| --- | STORM SEWER | --- |
| --- | SANITARY SEWER | --- |
| --- | WATERMAIN | --- |
| --- | GAS MAIN | --- |
| --- | ELEC. TELE. CABLE | --- |
| ○ | STORM MANHOLE | ● |
| □ | CATCH BASIN | ■ |
| ⊗ | INLET | ⊗ |
| ⊕ | REARYARD CATCH BASIN | ⊕ |
| > | END SECTION | > |
| ○ | SANITARY MANHOLE | ● |
| ⊕ | GATE VALVE AND WELL | ⊕ |
| ⊙ | HYDRANT | ● |
| ⊙ | GRADE | 91.7 |
| ⊙ | PAVEMENT GRADE | P |
| ⊙ | TOP OF CURB | T/C |
| ⊙ | TOP OF WALK | T/W |
| ⊙ | SWALE GRADE | SW |
| ⊙ | SPOT GRADE/GUTTER | G |
| ⊙ | DRAINAGE AREA | |
| ○ | CURB AND GUTTER INLET FILTER (SI-4) | ○ |
| ○ | LOW POINT INLET FILTER (SI-2) | ○ |
| ○ | SILT FENCING (SP-2) | --- |
| --- | GRADING LIMITS | --- |

EAST BOUND BIG BEAVER



SCALE 1" = 20'

LEGEND

EXISTING

PROPOSED

---	STORM SEWER	---
---	SANITARY SEWER	---
---	WATERMAIN	---
---	GAS MAIN	---
---	ELEC. TELE. CABLE	---
---	STORM MANHOLE	○
---	CATCH BASIN	■
---	INLET	□
---	REARYARD CATCH BASIN	⊗
---	END SECTION	⋈
---	SANITARY MANHOLE	●
---	GATE VALVE AND WELL	⊕
---	HYDRANT	⊙
---	GRADE	91.7
---	PAVEMENT GRADE	P
---	TOP OF CURB	T/C
---	TOP OF WALK	T/W
---	SWALE GRADE	SW
---	SPOT GRADE/GUTTER	G
---	DRAINAGE AREA	
○	CURB AND GUTTER INLET FILTER (SI-4)	○
○	LOW POINT INLET FILTER (SI-2)	○
○	SILT FENCING (SP-2)	---
---	GRADING LIMITS	---

GENERAL NOTES:

- 1) THIS IS A REDEVELOPMENT OF AN EXISTING OFFICE BUILDING IN THE FORM BASED ON THE BIG BEAVER DISTRICT (BUILDING FORM "A" - STREET TYPE "A")
- 2) ALL PLANS AND SPECIFICATIONS SHALL COMPLY WITH THE CURRENT REQUIREMENTS OF THE CITY OF TROY, OAKLAND COUNTY AND THE STATE OF MICHIGAN.
- 3) PROPOSED BUILDING WILL UTILIZE EXISTING SANITARY AND WATER SERVICE LEADS LOCATED ON THE NORTH SIDE OF THE EXISTING BUILDING. THE BUILDING CONTRACTOR SHALL VERIFY THE DEPTH AND LOCATION OF THE EXISTING SERVICE LEADS PRIOR TO THE START OF BUILDING CONSTRUCTION. THE CITY AFTER REVIEW OF THE EXISTING WATER SERVICE LEAD SIZE MAY REQUIRE A NEW WATER SERVICE LEAD TO SERVE THE PROPOSED RESTAURANT.
- 3) THIS SITE IS SERVED WITH UNDERGROUND ELECTRIC, TELEPHONE, CABLE AND GAS MAINS.
- 4) ALL PROPOSED UTILITIES WILL BE PLACED IN EASEMENTS IN ACCORDANCE WITH CURRENT TOWNSHIP AND UTILITY COMPANY STANDARDS.
- 5) SIGNAGE WILL BE BUILDING MOUNTED MEETING CURRENT CITY STANDARDS.

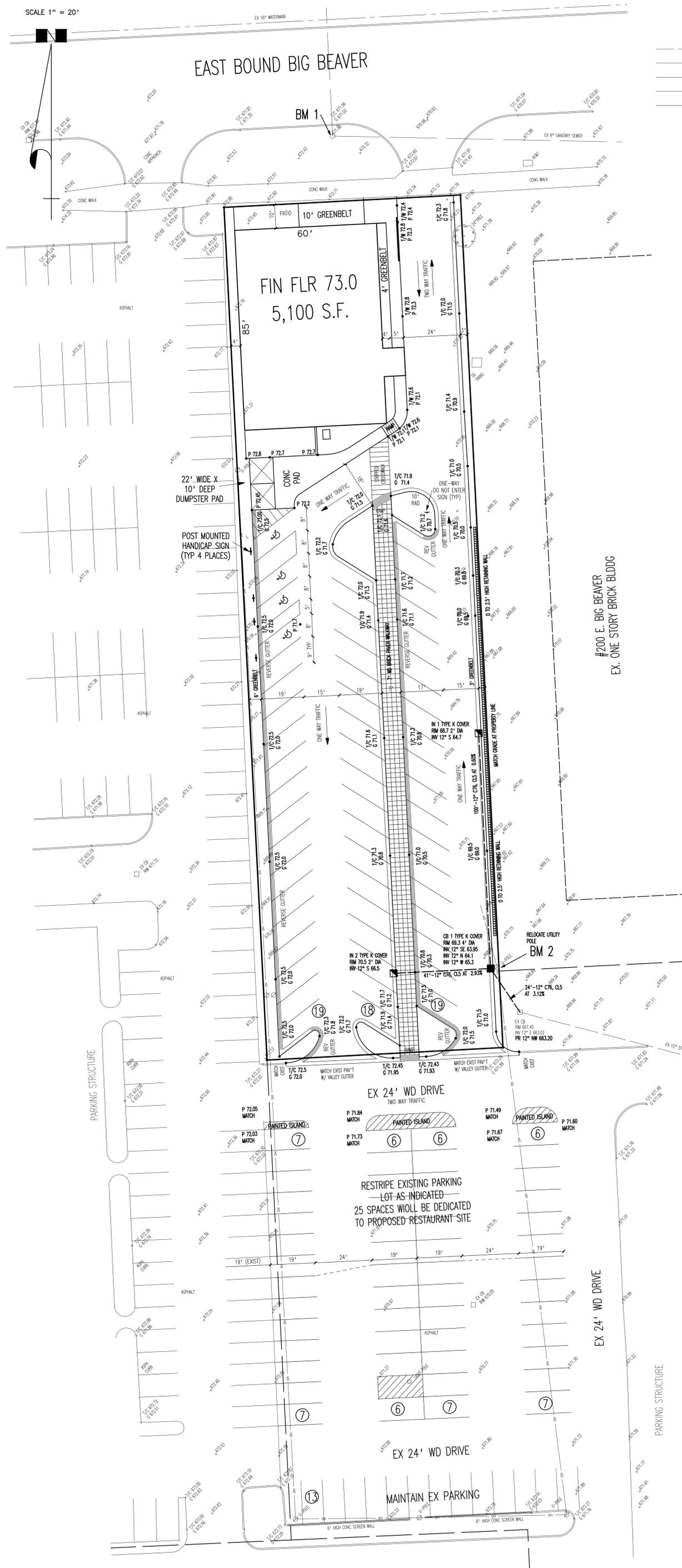
SITE DATA:

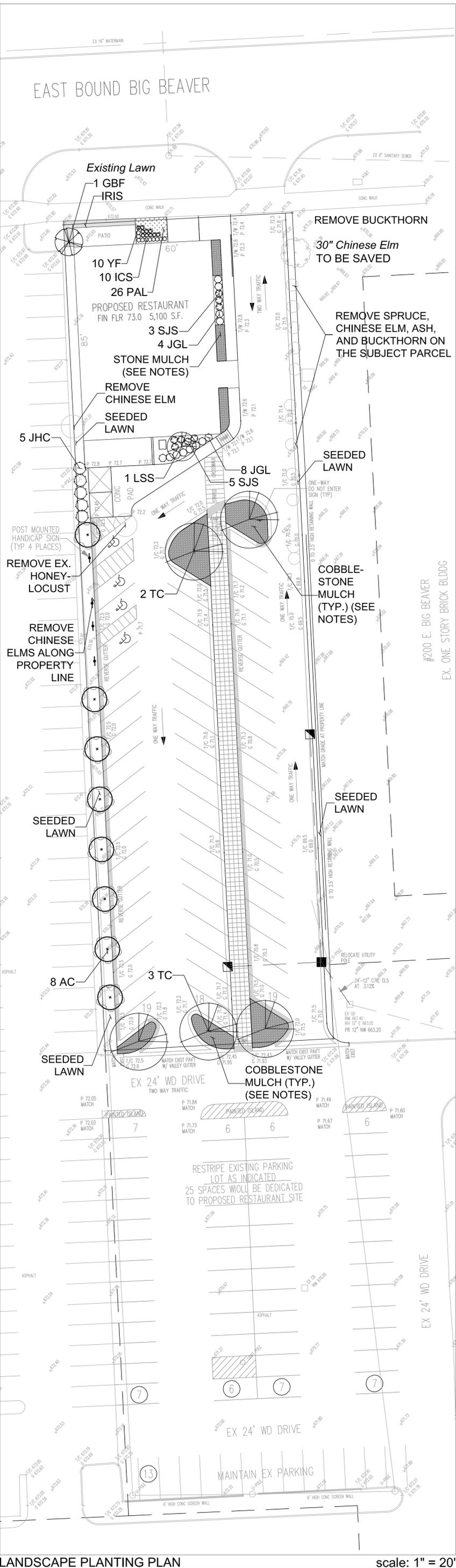
- 1) ZONING: PARCEL 20-27-101-061 (BIG BEAVER DISTRICT)
BUILDING FORM "A" - ROAD TYPE "A"
- 2) PROPOSED USE: RESTAURANT
- 3) TOTAL AREA OF PROPERTY 0.83 ACRES
- 4) SETBACKS: REQUIRED

	PROPOSED
FRONT	10'
SIDES	4' WEST - 27' EAST
REAR	30'
BUILDING HEIGHT	REFER TO ARCH PLANS
- 5) BUILDING COVERAGE: TOTAL BUILDING SIZE = 5,100 S.F. OR 0.1171 AC
0.1171/0.83 = 14.11%

PARKING REQUIREMENTS

TOTAL BUILDING AREA - 5,100 S.F.
 PARKING REQUIRED - STANDARD RESTAURANT
 - 1 SPACE PER 2 SEATS BASED ON MAXIMUM SEATING CAPACITY
 PROPOSED TOTAL SEATING - 150 SEATS
 - 150/1 SPACES FOR EACH 2 SEATS = 75 SPACES
 PARKING PROVIDED - 81 SPACES (INCL 4 HANDICAP)





LANDSCAPE PLANTING PLAN scale: 1" = 20'

GENERAL NOTES FOR ALL PLANTINGS:

- DO NOT CUT CENTRAL LEADER.
- REMOVE ALL TAGS, STRINGS, PLASTICS, AND ANY OTHER NON-BIODEGRADABLE MATERIALS (EXCEPT LABEL FOR PLANT NAME) FROM PLANT STEMS OR CROWN WHICH ARE UNSIGHTLY OR COULD CAUSE GIRDLING.
- PLANTS SHALL BEAR THE SAME RELATION TO FINISH GRADE AS IT BORE TO THE PREVIOUS GRADE IN THE NURSERY. SET THE BASE OF THE PLANT SLIGHTLY HIGHER THAN EXISTING GRADE IF PLANTING IN CLAY SOILS.
- CENTER THE ROOTBALL IN THE PLANTING HOLE. LEAVE THE BOTTOM OF THE PLANTING HOLE FIRM. USE WATER TO SETTLE THE PLANTING MIX AND REMOVE ANY AIR POCKETS AND FIRMLY SET THE TREE OR SHRUB. GENTLY TAMP IF NEEDED.

NOTE:

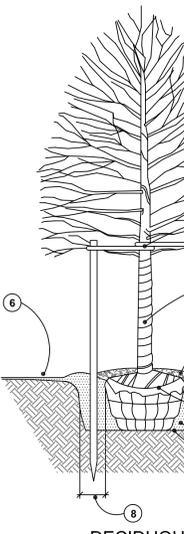
- CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION.
- SHREDDED BARK MULCH AT FOUR INCH (4") MINIMUM DEPTH. MULCH SHALL BE NATURAL IN COLOR.
- FORM A SAUCER WITH MULCH AND SOIL AROUND SHRUB BED.
- CUT AND REMOVE BURLAP AND BINDINGS FROM THE TOP ONE-THIRD (1/3) OF THE ROOTBALL.
- 3/16" x 4" ALUMINUM EDGING (OR APPROVED EQUIVALENT) OR SPADED EDGE.
- EXCAVATE PLANTING HOLE AND BACKFILL WITH PREPARED PLANTING MIX.
- UNDISTURBED SUBGRADE.
- LAWN.
- SCARIFY SUBGRADE.



SHRUB

NOTES:

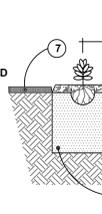
- STAKE TREES UNDER FOUR INCH (4") CALIPER.
- CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION.
- SET TOP OF BALL THREE INCHES (3") ABOVE FINISH GRADE.
- SET STAKES VERTICAL & EVENLY SPACED.
- STAKES OR GUYS TO BE SECURED ABOVE THE FIRST BRANCH.
- DO NOT PRUNE TERMINAL LEADER. PRUNE ONLY DEAD OR BROKEN BRANCHES.
- REMOVE ALL TAGS, STRING, PLASTICS, AND OTHER MATERIALS THAT ARE UNSIGHTLY OR COULD CAUSE DAMAGE.



DECIDUOUS TREE

NOTE:

- CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION.
- PERENNIALS TO BE PLANTED UP TO THE EDGE OF THE SAUCER AROUND A TREE OR SHRUB BED.
- SEE PLANT LIST FOR SPACING DISTANCE.
- SHREDDED HARDWOOD BARK OF A NATURAL COLOR MULCH AT FOUR INCH (4") MINIMUM DEPTH.
- 3/16" x 4" ALUMINUM EDGING (OR APPROVED EQUIVALENT) OR SPADED EDGE.
- EXCAVATE PLANTING BED AND BACKFILL WITH PREPARED PLANTING MIX AT A TEN INCH (10") DEPTH.
- UNDISTURBED SUBGRADE.
- PLANTING MIX TO CONSIST OF EQUAL PARTS OF SAND, LEAF COMPOST, AND NATIVE SOIL.
- LAWN.



ANNUAL / PERENNIAL / GROUNDCOVER

NOTES:

- STAKE ALL EVERGREEN TREES UNDER TWELVE FEET (12") HIGH.
- GUY ALL EVERGREEN TREES TWELVE FEET (12") HIGH AND OVER.
- CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION.
- NEVER CUT CENTRAL LEADER. PRUNE ONLY TO REMOVE DEAD OR BROKEN BRANCHES.
- SET STAKES VERTICAL AND EVENLY SPACED.
- REMOVE ALL TAGS, STRINGS, PLASTICS, AND OTHER MATERIALS THAT ARE UNSIGHTLY OR COULD CAUSE GIRDLING.



EVERGREEN TREE

not to scale

LANDSCAPE DEVELOPMENT NOTES:

- PLANTING**
- Installation of all plant material shall be in accordance with the latest edition of the American Association of Nurserymen Standards for Nursery Stock and with the specifications set forth by the City of Troy, Michigan.
 - The plant materials shall conform to the type stated on the plant list. Sizes shall be the minimum stated on the plant list or larger. All measurements shall be in accordance with the latest edition of the American Association of Nurserymen Standards for Nursery Stock.
 - The plant material shall be nursery grown and inspected by the Owner's representative before planting. The Owner's representative reserves the right to reject any plant material at any time.
 - Plants designated "B&B" shall be balled and burrapped with firm balls of earth.
 - Dig shrub pits one foot (1') larger than the shrub rootball, tree pits three (3) times the width of the tree rootball and backfill with one (1) part topsoil and one (1) part soil from excavated pit. Plant trees and shrubs at the same grade level at which they were planted at the nursery. If wet, clay soils are evident, plant trees and shrubs slightly higher.
 - The Contractor is responsible for planting the materials at the correct grades and spacing. The plants shall be oriented to give the best appearance.
 - When the plant has been properly set, the pit shall be backfilled with the topsoil mixture, gradually filling, patting, and settling with water.
 - Trees in lawn areas to have a four foot (4') circle of mulch, four inches (4") deep, and three inches (3") away from the trunk. Shrub beds are to be mulched with shredded bark mulch to a minimum depth of four inches (4"). Only natural color shredded hardwood bark mulch will be accepted.
 - Remove all twine, wire, and burlap from the top one third (1/3) of tree and shrub root balls and from tree trunks. Remove all non-biodegradable material such as plastic or nylon completely from branches and stems.
 - All plant materials shall be pruned and injuries repaired. The amount of pruning shall be limited to the removal of dead or injured limbs and to compensate for the loss of roots from transplanting. Cuts should be flush, leaving no stubs. Cuts over three quarters of an inch (3/4") shall be painted with tree paint. Shrubs along the site perimeter shall be allowed to grow together in a natural form.
 - Organic, friable topsoil shall be evenly distributed and fine graded over all areas to receive lawns at uniform depth of four inches (4") after settlement.
 - All lawn areas shall be sodded with a Grade A Kentucky Blue Grass blend over the topsoil. Peat sod is not acceptable. Existing lawn in generally good condition but with bare, sparse, or weedy areas must be renovated by filling in low areas, raking, overseeding, and top dressing all sparse and bare spots and continuing with a weed and feed program.
 - All plantings shall be completed within three (3) months, and no later than November 30, from the date of issuance of a certificate of occupancy if such certificate is issued during the April 1 thru September 30 period; if the certificate is issued during the October 1 thru March 31 period, the planting shall be completed no later than the ensuing May 31; plantings shall thereafter be reasonably maintained, including permanence and health of plant materials to provide a screen to abutting properties and including the absence of weeds and refuse.
 - Backfill directly behind all curbs and along sidewalks and compact to the top of curbs or walk to support vehicle and pedestrian weight without settling.
 - All landscape areas, especially parking lot islands and landscape beds next to buildings shall be excavated of all building materials and poor soils to a depth of twelve inches to eighteen inches (12"-18") and backfilled with good, medium-textured planting soil (loam or light yellow clay). Add four inches to six inches (4"-6") of topsoil over the fill material and crown a minimum of six inches (6") above the top of curbs and/or walks after earth settling unless otherwise noted on the landscape plan.
 - Conversion of all asphalt and gravel areas to landscape planting beds shall be done in the following manner:
 - Remove all asphalt, gravel, and compacted earth to a depth of six inches to eighteen inches (6"-18") depending on the depth of the sub base and dispose of off site;
 - Call the City for an inspection prior to backfilling;
 - Replace excavated material with good, medium-textured planting soil (loam or light yellow clay) to a minimum of two inches (2") above the top of the curb and sidewalk, add four inches to six inches (4"-6") of topsoil and crown to a minimum of six inches (6") above the adjacent curb and walk after earth settling, unless otherwise noted on the landscape plan.
 If conversion from asphalt to landscape occurs in or between an existing landscape area(s), replace excavated material from four inches to six inches (4"-6") below adjacent existing grade with good, medium-textured planting soil (loam or light yellow clay) and add four inches to six inches (4"-6") of topsoil to meet existing grades after earth settling.
 - Edging shall consist of Ryerson Steel edging or approved equivalent.
 - Elevate the rootballs of Yew shrubs to allow for better drainage.

MATERIAL

- Required landscape material shall satisfy the criteria of the American Association of Nurserymen Standards for Nursery Stock and be:
 - Nursery grown;
 - State Department of Agriculture inspected;
 - No. 1 grade material with a straight, unscarred trunk, and well-developed uniform crown (park grade trees will not be accepted);
 - Staked, wrapped, watered, and mulched according to the details provided; and
 - Guaranteed for one (1) year.
- Topsoil shall be friable, fertile soil of clayloam character containing at least five percent (5%) but not more than twenty percent (20%) by weight of organic matter with a pH range between 6.0 and 7.0. The topsoil shall be free from clay lumps, coarse sand, plant roots, sticks, and other foreign materials.
- The seed mixture shall consist of the following types and proportions: Kentucky Blue Grass blend "Baron/Sheri/Adelphi" @ sixty percent (60%), Chewing Fescue @ twenty-five percent (25%), Creeping Red Fescue @ ten percent (10%), and Perennial Rye Grass @ five percent (5%). Weed content shall not exceed one percent (1%). The mix shall be applied at a rate of 200 pounds per acre.
- Sod shall be two (2) year old "Baron/Sheri/Adelphi" Kentucky Blue Grass blend grown in a sod nursery on loam soil.
- Stone mulch at the building foundation to consist of a Western Sunrise color mix one inch to two inch (1" - 2") in diameter six inches (6") deep over geotextile fabric. Cobblestone mulch in the parking lot islands to consist of two inch to four inch (2" - 4") cobbles six inches (6") deep over geotextile fabric.

GENERAL

- Do not plant deciduous or evergreen trees directly over utility lines or under overhead wires. Maintain a six foot (6') distance from the centerline of utilities and twenty feet (20') from the centerline of overhead wires for planting holes. Call MISS DIG forty-eight (48) hours prior to landscape construction for field location of utility lines.
- The Contractor agrees to guarantee all plant material for a period of one (1) year. At that time, the Owner's representative reserves the right for a final inspection. Plant material with twenty-five percent (25%) die back, as determined by the Owner's representative shall be replaced. This guarantee includes the furnishing of new plants, labor, and materials. These new plants shall also be guaranteed for a period of one (1) year.
- The work shall consist of providing all necessary materials, labor, equipment, tools, and supervision required for the completion as indicated on the drawings.
- All landscape areas including parking lot islands shall be irrigated by an automatic underground irrigation system. Lawns and shrub/landscape areas shall be watered by separate zones to minimize overwatering.
- All written dimensions override scale dimensions on the plans.
- Report all changes, substitutions, or deletions to the Owner's representative.
- All bidders must inspect the site and report any discrepancies to the Owner's representative.
- All specifications are subject to change due to existing conditions.
- The Owner's representative reserves the right to approve all plant material.

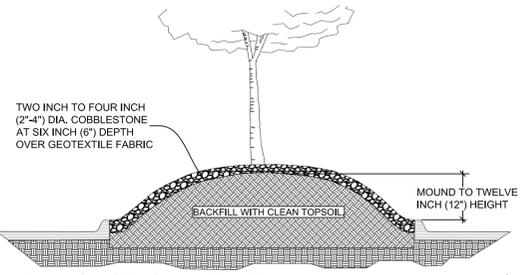
MAINTENANCE OF GENERAL LANDSCAPE AREAS

- The Owner of the landscaping shall perpetually maintain such landscaping in good condition so as to present a healthy, neat, and orderly appearance, free from refuse and debris.
- The Owner shall conduct a seasonal landscape maintenance program including regular lawn cutting (at least once per week during the growing season), pruning at appropriate times, watering, and snow removal during winter.
- All diseased and/or dead material shall be removed within sixty (60) days following notification and shall be replaced within the next appropriate planting season or within one (1) year, whichever comes first.
- Any debris such as lawn clippings, fallen leaves, fallen limbs, and litter shall be removed from the site on a weekly basis at the appropriate season.
- All planting beds shall be maintained by removing weeds, fertilizing, and replenishing mulch as needed.
- Annual beds shall be kept free of weeds and mulched with sphagnum peat of a neutral pH as needed. Perennial beds shall be kept free of weeds and mulched with fine textured shredded bark as needed. Cut spent flower stalks from perennial plants at regular intervals.

PLANT LIST

KEY QTY.	BOTANICAL NAME	SIZE	COMMON NAME
GBF 1	<i>Ginkgo biloba</i> 'Fastigiata'	Fastigiata Maidenhair Tree	2-1/2" cal. B&B
ICS 10	<i>Ilex crenata</i> 'Sky Pencil'	Sky Pencil Japanese Holly	30" ht., 5 gal. pot
PAL 26	<i>Pennisetum alopecuroides</i>	'Little Bunny'	1 gal. pot, 24" o.c.
YF 10	<i>Yucca filamentosa</i>	Yucca	24" ht., 3 gal. pot
IRIS 8	<i>Irish pallida</i> 'Aureo-Variegata'	Aureo-Variegata Sweet Iris	1 gal. pot, 24" o.c.
GENERAL SITE PLANTING			
JGL 12	<i>Juniperus chinensis</i> 'Gold Lace'	Gold Lace Juniper	24" spr., 3 gal. pot
JHC 5	<i>Juniperus chin.</i> 'Helz Columnaris'	Helz Columnar Juniper	4' - 5' ht. B&B
SJS 8	<i>Spiraea japonica</i> 'Shirobana'	Shirobana Japanese Spiraea	24" ht., 3 gal. pot
PARKING LOT LANDSCAPING			
AC 8	<i>Amelanchier canadensis</i>	Servicberry	2" cal. B&B
LSS 1	<i>Liquidambar styraciflua</i>	'Slender Silhouette'	3" cal. B&B
TC 5	<i>Tilia cordata</i> 'Greenspire'	Greenspire European Linden	3" cal. B&B
HHR 1	<i>Hamamelis sp.</i> 'Happy Returns'	Happy Returns Daylily	1 gal. pot, 18" o.c.

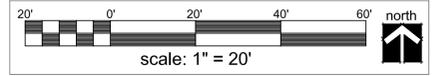
* Quantity to be determined by the Landscape Contractor in the field.



PARKING LOT ISLAND DETAIL not to scale

LANDSCAPE CALCULATIONS:

- LANDSCAPING ADJACENT TO ROADS**
East Big Beaver Road (100 l.f.)
* One (1) deciduous tree / 30 l.f. = 3.33 trees = 4 trees
- PARKING LOT LANDSCAPING** (81 spaces)
* One (1) deciduous tree for every eight (8) parking spaces equals 10.125 trees or 11 trees
SITE LANDSCAPING (Site area: 36,155 sq. ft.)
* 36,155 sq. ft. x .20 equals 7,231 sq. ft.
* Site landscaping area provided equals 4,868 sq. ft.
* Paved pedestrian use areas provided equals 2,115 sq. ft.



LANDSCAPE PLAN FOR: PROJECT LOCATION:
Mr. Martin J. Knollenberg Proposed Restaurant
198 East Big Beaver Road Development
Troy, Michigan 48083-1203 198 East Big Beaver Road
(248) 528-0200 Troy, Michigan 48083

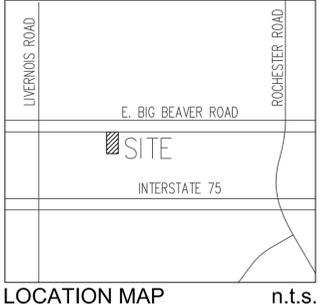
LANDSCAPE PLAN BY:
Nagy Devlin Land Design, L.L.C.
31736 West Chicago Ave.
Livonia, Michigan 48150
(734) 634-9208



J. Brian Devlin
AUTOCAD SIGNATURE
ORIGINAL IN BLUE

SHEET 2: LANDSCAPE PLANTING PLAN

* Base data provided by Creative Land, L.L.C.



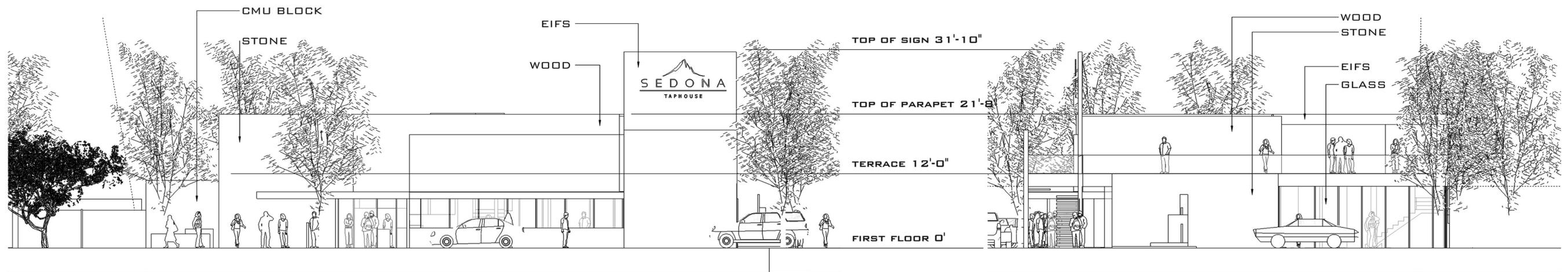
LOCATION MAP n.t.s.

date: June 22, 2015
revised:
06-25-2015 Client review.
08-06-2015 Revise for site plan changes.



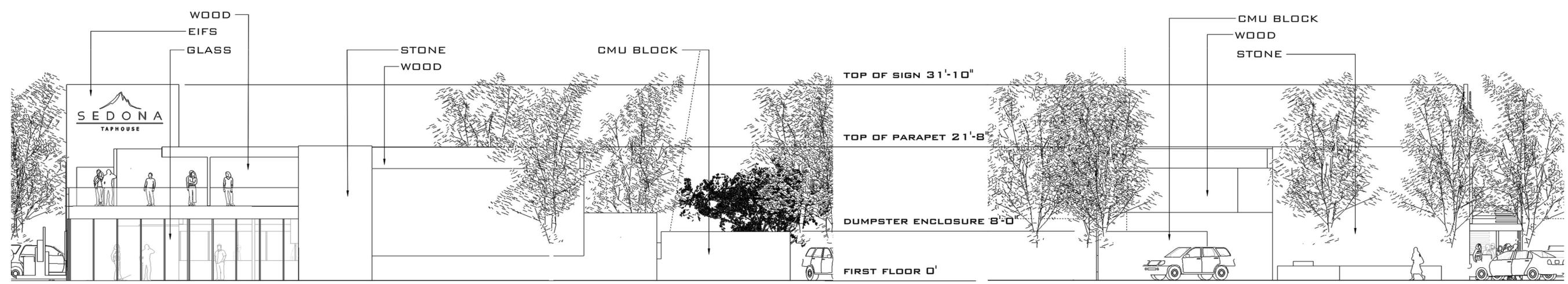
DO NOT SCALE DRAWINGS
USE FIGURED
DIMENSIONS ONLY

COPYRIGHT THOMAS
STRAT ARCHITECTS
ALL RIGHTS RESERVED.
NO PORTION OF THESE
DOCUMENTS MAY BE
REPRODUCED IN ANY
MANNER, WITHOUT PRIOR
WRITTEN PERMISSION OF
THE ARCHITECT.



WEST ELEVATION
SCALE: 1/8" = 1'-0"

NORTH ELEVATION
SCALE: 1/8" = 1'-0"



EAST ELEVATION
SCALE: 1/8" = 1'-0"

SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

THOMAS STRAT ARCHITECTS
SEDONA TAPHOUSE - TROY, MI

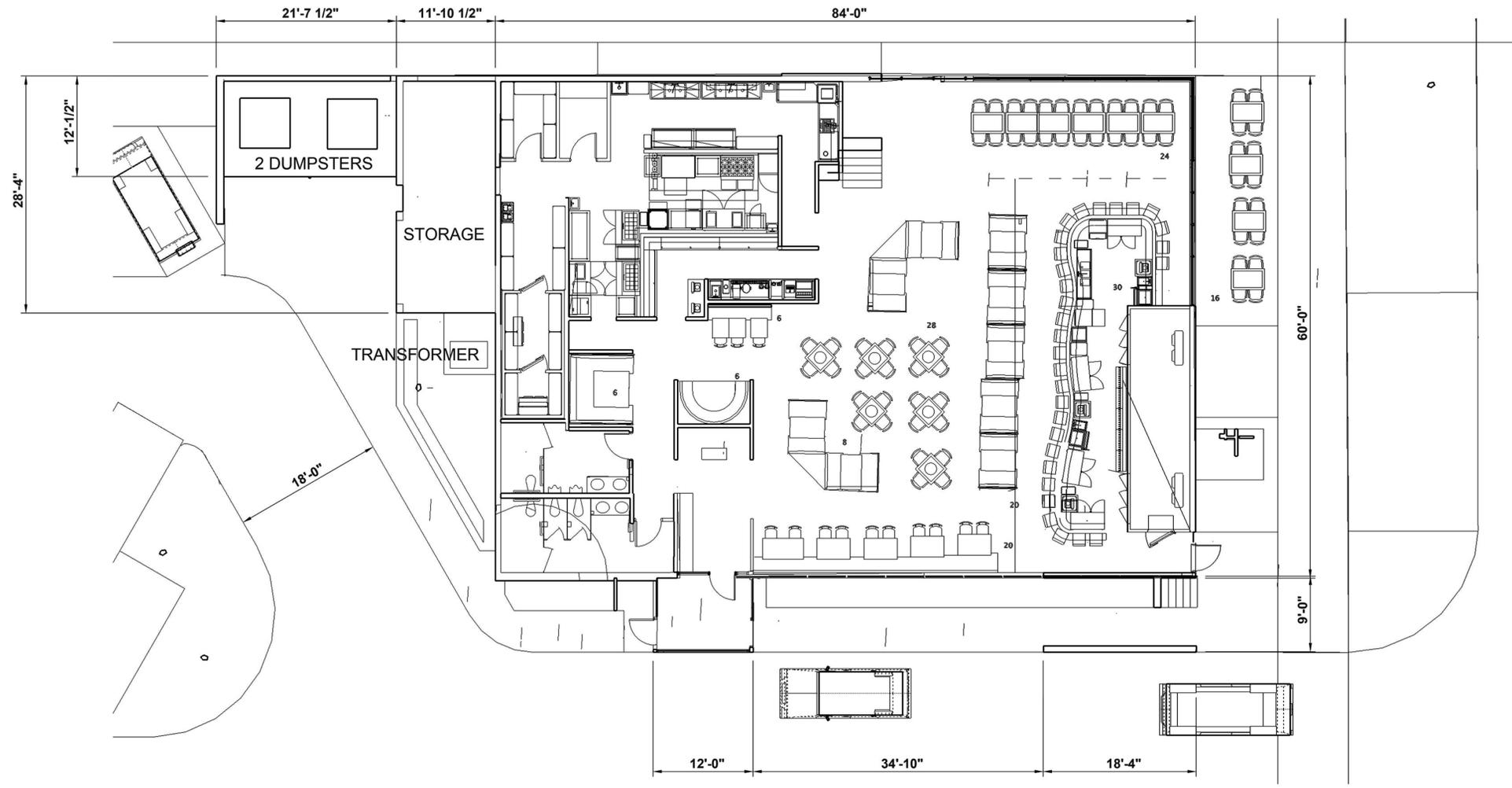
THOMAS STRAT ARCHITECTS
248.703.3361
2605 CROOKS RD
TROY, MI 48064
tstratarch@gmail.com

REVISIONS

15 JUNE, 2015
JOB NO. 1505
SHEET NO.

DO NOT SCALE DRAWINGS
USE FIGURED
DIMENSIONS ONLY

COPYRIGHT THOMAS
STRAT ARCHITECTS
ALL RIGHTS RESERVED.
NO PORTION OF THESE
DOCUMENTS MAY BE
REPRODUCED IN ANY
MANNER, WITHOUT PRIOR
WRITTEN PERMISSION OF
THE ARCHITECT.



FLOOR PLAN
SCALE: 1/8" = 1'-0"

THOMAS STRAT ARCHITECTS
SEDONA TAPHOUSE - TROY, MI

THOMAS STRAT ARCHITECTS
248.703.3361
2605 CROOKS RD
TROY, MI 48064
tstratarch@gmail.com

REVISIONS

NO.	DESCRIPTION

15 JUNE, 2015
JOB NO. 1505
SHEET NO.

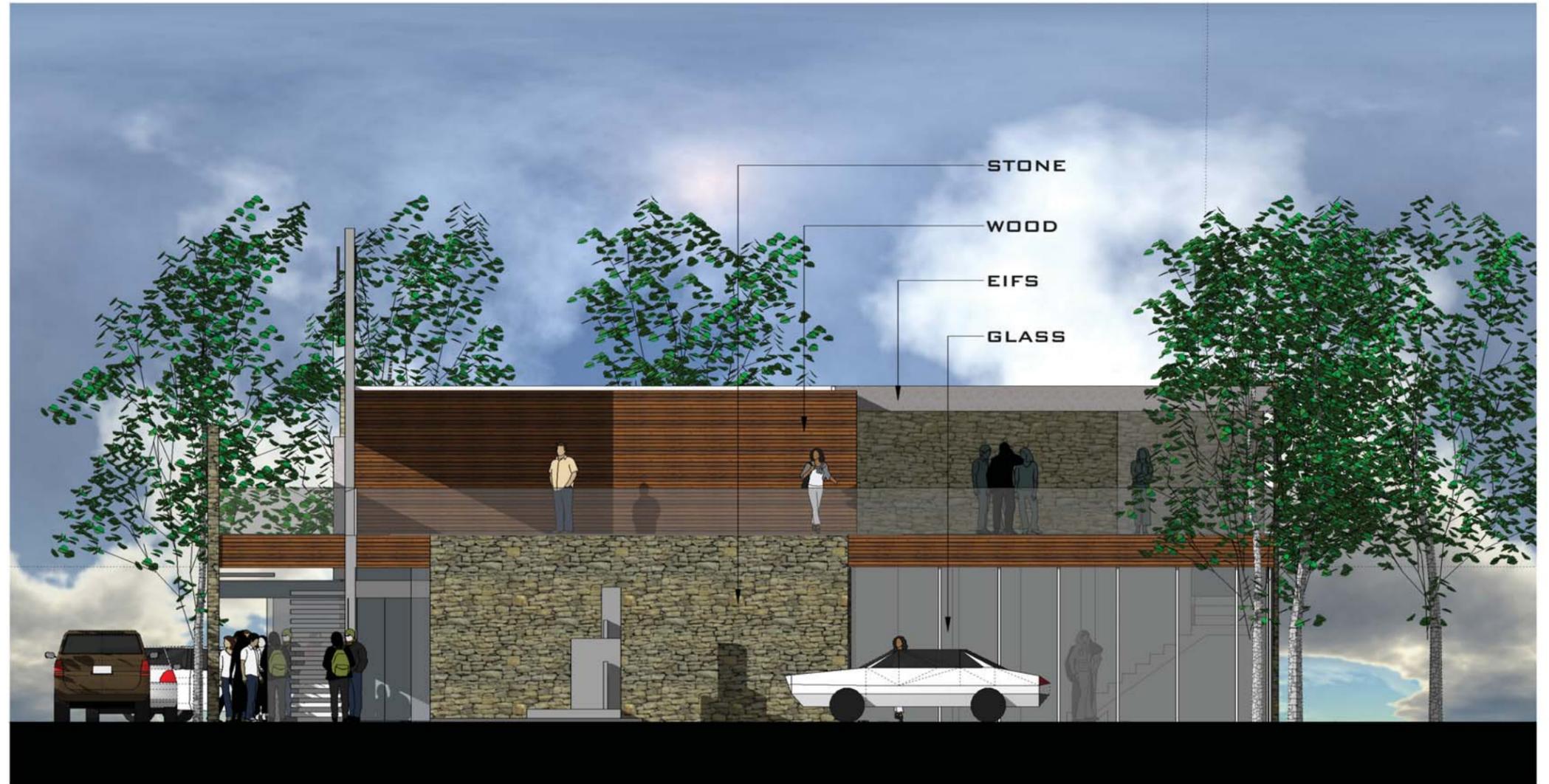


SEDONA TAPHOUSE - TROY, MI

STUDIO
OF

THOMAS **STRAT**
ARCHITECTS

2605 CROOKS ROAD TROY, MI 48064 TSTRATARCH@GMAIL.COM 248.703.3361



NORTH ELEVATION

STUDIO
OF

THOMAS **STRAT**
ARCHITECTS

2605 CROOKS ROAD TROY, MI 48064 TSTRATARCH@GMAIL.COM 248.703.3361

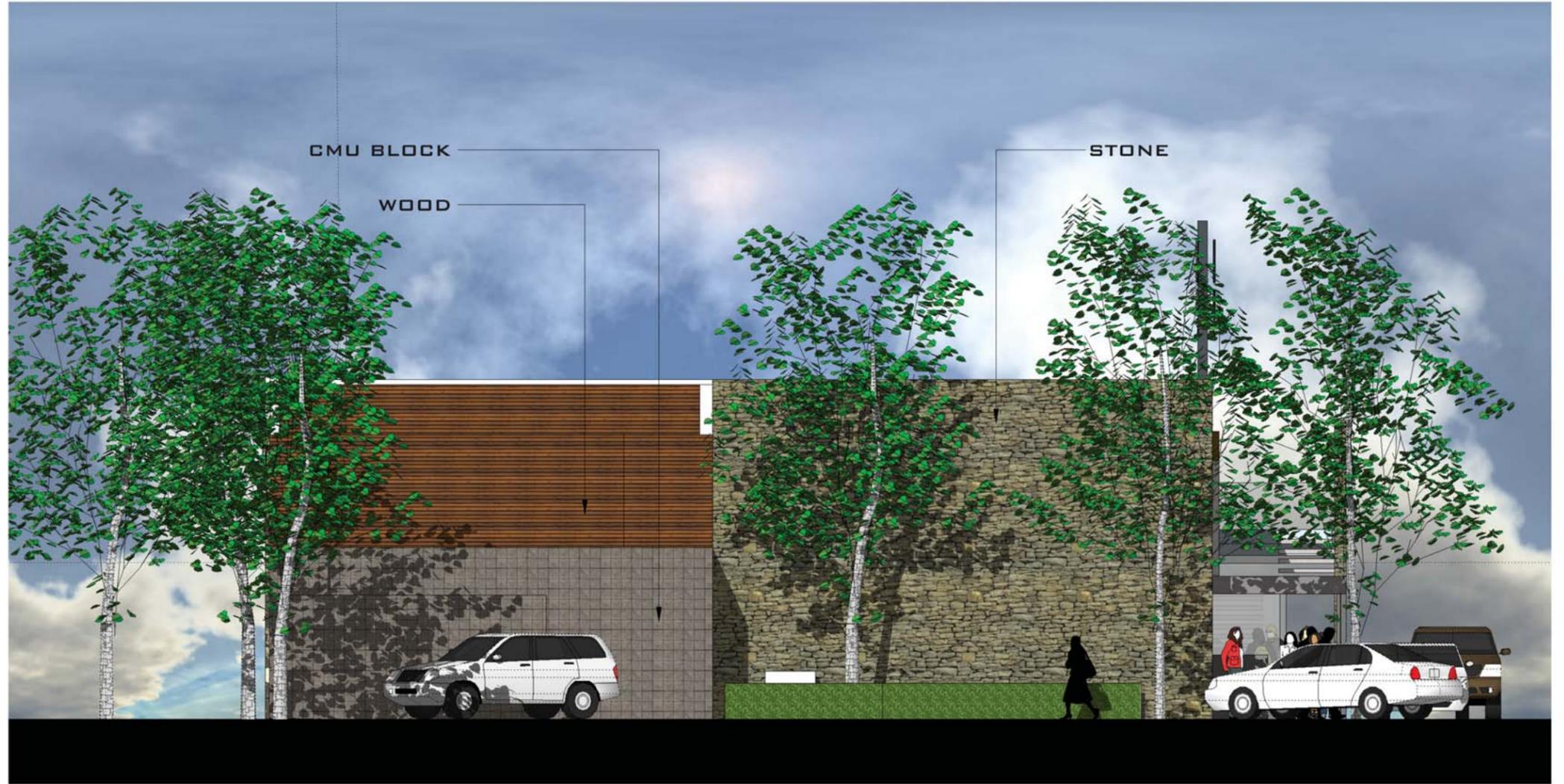


EAST ELEVATION

STUDIO
OF

THOMAS **STRAT**
ARCHITECTS

2605 CROOKS ROAD TROY, MI 48064 TSTRATARCH@GMAIL.COM 248.703.3361



SOUTH ELEVATION

STUDIO
OF

THOMAS **STRAT**
ARCHITECTS

2605 CROOKS ROAD TROY, MI 48064 TSTRATARCH@GMAIL.COM 248.703.3361



WEST ELEVATION

STUDIO
OF

THOMAS **STRAT**
ARCHITECTS

2605 CROOKS ROAD TROY, MI 48064 TSTRATARCH@GMAIL.COM 248.703.3361