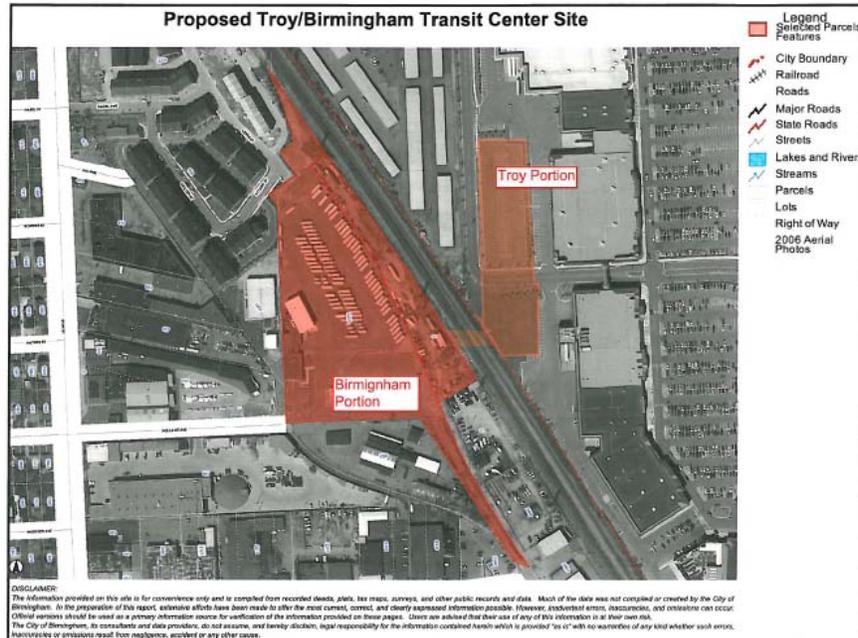


**SPECIAL JOINT MEETING OF THE
TROY PLANNING COMMISSION &
BIRMINGHAM PLANNING BOARD
WEDNESDAY, SEPTEMBER 8, 2010
7:00PM**

CONFERENCE ROOM, DEPARTMENT OF PUBLIC SERVICES
BUILDING, 851 S. ETON, BIRMINGHAM, MICHIGAN

- A. Roll Call –Troy Planning Commission
Birmingham Planning Board
- B. Chairpersons' Comments & Introduction of Guests
- C. Review & Approval of the Minutes of the Joint Meeting of **July 14, 2010**
- D. Review & Approval of the Agenda
- E. Meeting Open to the Public for Items not on the Agenda
- F. Preliminary Site Plan Review
 - 1. **See map below with legal descriptions* for property within Birmingham, MI:** Construction of Amtrak platform, public plaza, parking, pedestrian tunnel and sidewalks to access the rail platform in Birmingham and to link to the Transit Center building in Troy.



Notice: Due to Building Security, public entrance during non-business hours is through the Police Department—Pierce St. Entrance only. Individuals with disabilities requiring assistance to enter the building should request aid via the intercom system at the parking lot entrance gate on Henrietta St.

People with disabilities needing accommodations for effective participation in this meeting should contact the City Clerk's Office at (248) 644-1800 ext. 282 (voice) or (248) 644-5115 (TDD) at least one day in advance to request mobility, visual, hearing or other assistance.

***Property within Birmingham:**

(A) PROPOSED NORTHERN APPENDAGE PARCEL, TO BE SPLIT OFF OF THE BIRMINGHAM SCHOOL PROPERTY AND PURCHASED BY CITY. (AS SURVEYED)

PART OF LOT 164, BIRMINGHAM GARDENS, AS RECORDED IN LIBER 31 ON PAGE 38 OF PLATS, OAKLAND COUNTY RECORDS, AND ALSO PART OF THE NORTHEAST ¼ OF SECTION 31, TOWN 2 NORTH, RANGE 11 EAST, CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: BEGINNING AT THE NORTH ¼ CORNER OF SAID SECTION 31; THENCE S. 01° 59' 10" W. 701.82 FEET; THENCE S 88° 11' 20" E. 36.00 FEET TO THE NORTHWEST CORNER OF LOT 1 OF "ASSESSOR'S PLAT NO. 28, BEING A REPLAT OF PART OF LOT 169 OF BIRMINGHAM GARDENS AND A PLAT OF PART OF THE N.E. ¼ OF SECTION 31, T.2N., R.11E., CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN", LIBER 43, PAGE 50, OAKLAND COUNTY RECORDS; THENCE S. 01° 59' 10" W. 1,278.38 FEET ALONG THE EASTERLY RIGHT-OF-WAY LINE OF ETON ROAD (WIDTH VARIES) TO A POINT AT THE NORTHEAST INTERSECTION OF SAID ETON ROAD AND HOLLAND AVENUE (50 FEET WIDE),

ALSO BEING THE SOUTHWEST CORNER OF LOT 9 OF SAID ASSESSOR'S PLAT NO. 28; THENCE S. 88° 15' 29" E. 604.19 FEET ALONG THE NORTH RIGHT OF WAY LINE OF HOLLAND AVENUE TO THE SOUTHEAST CORNER OF LOT 9 OF SAID ASSESSOR'S PLAT NO. 28; THENCE N. 01° 53' 01" E. 621.11 FEET TO THE NORTHEAST CORNER OF LOT 1 OF SAID ASSESSOR'S PLAT NO. 28, SAID POINT ALSO BEING A CORNER OF ETON STREET STATION II CONDOMINIUM PLAN NO. 1678, LIBER 34405, PAGE 578 - 665, OAKLAND COUNTY RECORDS; THENCE ALONG THE PERIMETER OF SAID ETON STREET STATION II FOR TWO (2) COURSES: 1). 44.04 FEET ALONG A NON-TANGENT CURVE TO THE LEFT, SAID CURVE HAVING A RADIUS OF 22,661.83 FEET, A CENTRAL ANGLE OF 00° 06' 41", A CHORD LENGTH OF 44.04 FEET AND A CHORD BEARING OF N. 31° 39' 31" W.; 2). N. 58° 13' 52" E. 99.79 FEET TO THE POINT OF BEGINNING, SAID POINT ALSO BEING A CORNER OF SAID ETON STREET STATION II; THENCE ALONG THE PERIMETER OF SAID ETON STREET STATION II FOR FOUR (4) COURSES: 1). N. 31° 31' 34" W. 80.22 FEET, 2). N. 57° 32' 30" E. 52.11 FEET; 3). N. 11° 54' 39" W. 114.49 FEET; 4). N. 19° 00' 45" W. 116.00 FEET TO A POINT ON THE RIGHT-OF-WAY LINE OF GRAND TRUNK WESTERN RAILROAD; THENCE ALONG SAID RIGHT-OF-WAY LINE S. 30° 43' 58" E. 304.96 FEET; THENCE S. 59° 24' 46" W. 57.75 FEET; THENCE S. 60° 25' 16" W. 53.74 FEET TO THE POINT OF BEGINNING. CONTAINING 15,111 SQUARE FEET OR 0.35 ACRES, MORE OR LESS

(B) PROPOSED SOUTHERN APPENDAGE PARCEL TO BE SPLIT OFF OF THE BIRMINGHAM SCHOOL PROPERTY AND PURCHASED BY CITY. (AS SURVEYED)

A PARCEL OF LAND LOCATED IN THE NORTHEAST ¼ OF SECTION 31, TOWN 2 NORTH, RANGE 11 EAST, CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: BEGINNING AT THE NORTH ¼ CORNER OF SAID SECTION 31; THENCE S. 01° 59' 10" W. 701.82 FEET; THENCE S 88° 11' 20" E. 36.00 FEET TO THE NORTHWEST CORNER OF LOT 1 OF "ASSESSOR'S PLAT NO. 28 BEING A REPLAT OF PART OF LOT 169 OF BIRMINGHAM GARDENS AND A PLAT OF PART OF THE N.E. ¼ OF SECTION 31, T.2N., R.77 E., CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN", LIBER 43 PAGE 50, OAKLAND COUNTY RECORDS; THENCE S. 01° 59' 10" W. 1,278.38 FEET ALONG THE EASTERLY RIGHT-OF-WAY LINE OF ETON ROAD (WIDTH VARIES) TO A POINT AT THE INTERSECTION SAID ETON ROAD AND HOLLAND AVENUE (50 FEET WIDE), SAID POINT ALSO BEING THE SOUTHWEST CORNER OF LOT 9 OF SAID ASSESSOR'S PLAT NO. 28; THENCE S. 88° 15' 29" E. 604.19 FEET TO THE SOUTHEAST CORNER OF LOT 9 OF SAID ASSESSOR'S PLAT NO. 28; THENCE S. 02° 29' 25" W. 16.90 FEET; THENCE S. 87° 53' 30" E. 396.57 FEET ALONG THE NORTH LINE OF "ASSESSOR'S PLAT NO. 28-B, BEING A REPLAT OF LOTS 11 & 12 OF ASSESSOR'S PLAT NO. 28, BEING A REPLAT OF PART OF LOT 169 OF BIRMINGHAM GARDENS AND A PLAT OF PART OF THE NE. 1/4 OF SECTION 31, T.2N., R.77 E., CITY OF BIRMINGHAM, OAKLAND COUNTY MICHIGAN," LIBER 64, PAGE 10, OAKLAND COUNTY RECORDS, TO THE POINT OF BEGINNING; THENCE N. 15° 59' 00" E 117.21 FEET; THENCE S. 18° 59' 54" EAST 182.57 FEET; THENCE ALONG A CURVE TO THE LEFT 239.09 FEET, SAID CURVE HAVING A RADIUS OF 1,564.10 FEET, A CENTRAL ANGLE OF 08° 45' 30"; A CHORD LENGTH OF 238.86 FEET AND A CHORD BEARING OF S. 23° 22' 39" E.; THENCE S. 27° 47' 20" E. 218.21 FEET; THENCE N. 67° 00' 00" W. 47.75 FEET; THENCE ALONG A CURVE TO THE LEFT 515.19 FEET,

SAID CURVE HAVING A RADIUS OF 22,661.83 FEET, A CENTRAL ANGLE OF 01° 18' 09" A CHORD LENGTH OF 515.17 FEET AND A CHORD BEARING OF N. 28° 18' 13" W. TO THE POINT OF BEGINNING, SAID CURVE ALSO BEING THE EASTERLY LINE OF LOT 1 OF SAID ASSESSOR'S PLAT NO. 28-B. CONTAINING 23,605 SQUARE FEET OR 0.54 ACRES, MORE OR LESS

**(C) PARENT PARCEL 2 PER TITLE INFORMATION REPORT S-378832-1-125 SU
(EDGEMERE ENTERPRISES' PROPERTY) PARCEL ID NO. 20-31-203-024**

A PART OF THE NORTHEAST ¼ OF SECTION 31, TOWN 2 NORTH, RANGE 11 EAST, IN THE CITY OF BIRMINGHAM, OAKLAND COUNTY, MICHIGAN, MORE PARTICULARLY DESCRIBED AS: COMMENCING AT A POINT ON THE NORTH LINE OF SAID SECTION 31; THENCE NORTH 88 DEGREES 12 MINUTES 00 SECONDS WEST, ALONG SAID NORTH SECTION LINE TO THE NORTH ¼ CORNER OF SAID SECTION; THENCE SOUTH 31 DEGREES 18 MINUTES 02 SECONDS EAST 1442.06 FEET TO THE POINT OF BEGINNING, SAID POINT BEING ON THE WESTERLY LINE OF THE GRAND TRUNK WESTERN RAILROAD THE FOLLOWING TWO COURSES BEING ALONG THE WESTERLY LINE OF SAID RAILROAD; 1) SOUTH 30 DEGREES 34 MINUTES 07 SECONDS EAST 416.60 FEET; AND 2) SOUTH 28 DEGREES 10 MINUTES 17 SECONDS EAST 385.25 FEET; THENCE SOUTH 62 DEGREES 42 MINUTES 03 SECONDS WEST 134.00 FEET; THENCE NORTH 18 DEGREES 59 MINUTES 17 SECONDS WEST 272.01 FEET; THENCE ALONG A CURVE TO THE LEFT 403.53 FEET, SAID CURVE HAVING A RADIUS OF 1907.31 FEET, CENTRAL ANGLE OF 12 DEGREES 07 MINUTES 20 SECONDS AND LONG CHORD BEARING OF NORTH 25 DEGREES 02 MINUTES 57 SECONDS WEST 402.78 FEET; THENCE NORTH 31 DEGREES 06 MINUTES 37 SECONDS WEST, 126.48 FEET; THENCE NORTH 59 DEGREES 25 MINUTES 23 SECONDS EAST 57.75 FEET TO THE POINT OF BEGINNING.

2. **1251 Doyle Drive, Troy, MI:** Construction of multi-modal transit center, parking facility, pedestrian tunnel and sidewalks to access the rail platform in Birmingham and to link to the Transit Center building in Troy.

G. Meeting Open to the Public for items on the Agenda

H. Adjournment

**SPECIAL JOINT MEETING OF THE CITY OF BIRMINGHAM
PLANNING BOARD AND CITY OF TROY PLANNING COMMISSION
WEDNESDAY, JULY 14, 2010**

Conference Room, Department of Public Services Building
851 S. Eton, Birmingham, Michigan

Minutes of the special joint meeting of the Birmingham Planning Board and Troy Planning Commission held July 14, 2010. Birmingham Vice-Chairperson Gillian Lazar convened the meeting at 7:07 p.m.

Birmingham Planning Board

Present: Vice-Chairperson Gillian Lazar; Board Members Scott Clein, Carroll DeWeese, Bert Koseck, Janelle Whipple-Boyce, Bryan Williams; Student Representative Aaron Walden

Absent: Chairman Robin Boyle

Birmingham Administration: Matthew Baka, Planning Intern
Jana Ecker, Planning Director
Carole Salutes, Recording Secretary

Troy Planning Commission

Present: Chairman Michael Hutson; Commission Members Donald Edmunds, Mark Maxwell, Philip Sanzica, Robert Schultz, Thomas Strat, John Tagle, Lon Ullmann

Absent: Commission Member Mark Vleck

Troy Administration: Mark Miller, Acting City Manager
Allan Motzny, Asst. City Attorney
Steve Vandette, City Engineer

07-124-10

CHAIRPERSON'S COMMENTS AND INTRODUCTION OF GUESTS

Birmingham Vice-Chairperson Lazar welcomed everyone to the joint meeting.

07-125-10

**REVIEW AND APPROVAL OF THE MINUTES OF THE JOINT MEETING OF
JANUARY 27, 2010**

**Motion by Mr. DeWeese
Seconded by Mr. Schultz to approve the Minutes of January 27, 2010.**

Motion carried, all were in favor.

07-126-10

REVIEW AND APPROVAL OF THE AGENDA (no changes)

07-127-10

MEETING OPEN TO THE PUBLIC FOR ITEMS NOT ON THE AGENDA (no one spoke)

07-128-10

PRELIMINARY SITE PLAN REVIEW

- 1. Construction of Amtrak platform, public plaza, parking, pedestrian tunnel and sidewalks to access the rail platform in Birmingham and to link to the Transit Center building in Troy**
- 2. 1251 Doyle Drive, Troy, MI: Construction of multi-modal transit center, parking facility, pedestrian tunnel and sidewalks to access the rail platform in Birmingham and to link to the Transit Center building in Troy**

Ms. Ecker offered a brief introduction. The two groups are meeting because the project stands under the jurisdictions of both the Cities of Troy and Birmingham. When it comes time for a motion to be made, the Birmingham Planning Board will make a separate motion on property within the City of Birmingham and a vote will be taken from the Birmingham Planning Board only. Then the Troy Planning Commission will have the opportunity to make a motion regarding property on the Troy side and the vote will be called for Troy.

Mr. Miller announced that the property in the City of Troy is controlled by a Consent Agreement. Therefore, the Troy Planning Commission does not have the authority to grant site plan approval; the Troy City Council does. Therefore, this evening the City of Troy Planning Commission is a recommending body to City Council.

Mr. Ecker clarified that the Birmingham Planning Board makes the final decision on Preliminary and Final Site Plan Reviews.

Hubbell, Roth & Clark Project (“HRC”) Engineer Jim Surhigh went through a PowerPoint which explained the site plan for both Birmingham and Troy parcels including:

- Approaches;
- Circulation;

- Elevators; and
- Retaining wall elevation on the Troy and Birmingham sides.

Ms. Sally Elmiger, Landscape Architect from Carlisle/Wortman, showed a slide presentation depicting site amenities and landscape for both sides and both entrances which included:

- Site finishes and furnishings;
- Bus shelters;
- Retaining walls and walkways;
- Entry signs; and
- Plant material and rain garden.

Mr. Larry Ancyra, Sr. Associate with HRC, continued with slides showing:

- LED site lighting and fixtures partially financed through a grant from the Michigan Dept. of Labor and Economic Growth;
- Photometrics of the site;
- Floor plan for the building including accommodation for traffic leading to the elevator at the south end, room for kiosks, seating and restrooms;
- Cross section of the building which is looking toward Silver LEED Certification through the use of: green roof, rainwater harvesting, geothermal heating and cooling, LED lighting, bike racks, storm water quality control, construction waste management, use of recycled materials, low-emitting adhesives and paints, community connectivity through the tunnel and eco friendly elevator;
- Outside building elevations; and
- Platform and canopy elevation including heated sidewalks for winter.

Ms. Ecker explained the two communities have now decided to put the elevators into the bidding process so they can get prices with and without them. The elevators are an alternate now and if the money is there when the final bidding and budget come in they can be added. Further, she recalled this group had talked about standards and her idea was that the consensus was to have a futuristic, modern, contemporary look for the Transit Center.

Ms. Ecker went on to respond to questions. The bulk of the lighting is on the Troy side. It would not need to meet the Birmingham lighting standards and Troy does not have lighting standards. A lighting analysis will be provided at Final Site Plan Review. She noted that light from the shopping mall bleeds over into the site.

Mr. Koseck said he is in full support of the project and applauds all the work that has been done to obtain the Federal money for it. He has not heard what the vision is for what this building should be. This should be a beautiful building that is planned to stand for the next 100 years. In his opinion, it is nowhere close to being that. He has no idea how the building relates to anything around it. He asked what they are doing relative to sustainable design relative to the site. The elevators seem like an afterthought in terms of their placement.

Ms. Ecker responded there was a vision and a steering committee comprised of representatives from both cities. The group wanted this to be a joint project that would work for both sides and both communities. They had originally talked about a larger building but they got the message at least from the Birmingham City Commission that the Commission wanted a small scale, utilitarian type center that would provide for the basic needs of travelers. They did not want a large scale building.

Mr. Koseck stated that the size of a building should be driven by the program – how many busses come, how many people come, etc. and not a desire for big or small or medium. He asked about the vision. Ms. Ecker replied that in terms of vision there was a lot of discussion over the last couple of years. The vision of the steering committee when she joined the process was utilitarian. This group wanted a green building with sustainable elements. At the charrette in June of last year the focus was on the transit oriented district that would surround the transit center. The community talked about wanting connectivity between the communities, pedestrian scale elements, buildings that allow a mix of uses, development that promotes more activity, and futuristic interactive digital display elements that look toward to the future.

Mr. Miller reminded the City of Troy Planning Commission they are attending this meeting only to make a recommendation on the Preliminary Site Plan. They will certify whether it meets the Zoning Ordinance requirements and the Consent Agreement. Their recommendation will then go to the Troy City Council.

Mr. Schultz recalled that early on it was the consensus of this group to go with an arched roof, a clock tower, and an echoing arched roof on the platform so that this is a cohesive development on both sides of the tracks.

Mr. Williams noted ways in which the building has changed since the previous meeting and he had several comments:

- The building has been moved;
- The building laid out E/W and now it lays out N/S. The glass is now more exposed to the sun in the morning and afternoon. As a result, heating and cooling costs may increase.
- What is the status of control of the road from Cole on the south end to the entrance or exit point at the north end;
- Troy has jurisdiction and their Planning Commission has no authority;
- Birmingham has no jurisdiction over the building but has authority on its side.

He still believes in the entire process; however, the building itself is unremarkable.

Ms. Ecker explained why the building has moved. The Deed Restrictions and the Consent Judgment say that Grand Sakwa does not need to grant any easements for encroachment onto their property. In the previous plan there was an access road coming off of Doyle Dr. to the back of the Kroger site. Grand Sakwa was concerned that Kroger would have objections to this. Therefore, the site plan has been changed so there are no encroachments onto the Grand Sakwa property. The building was re-

oriented because without the access road it would look at the back of the shopping center. Everyone on the steering committee agreed on the re-orientation. They like the fact that it now creates more of a public plaza space out front and opens up the view out onto the train tracks. She added that they do have control of the property and/or consent of all property owners on which this project is located on the Birmingham side to proceed with Preliminary and Final Site Plan Approval.

Mr. Ullmann received confirmation that the platforms are enclosed on four sides but they don't have closable doors. He thought the platform should be enclosed and that heat should be incorporated into the design. His problem about sustainability is that nothing is colder than aluminum and glass. He wanted to know if there is some sort of an estimate of what the operational costs for this will be. The grass roof seems to be the driving cost for this \$2 million building. A simple building would probably operate at 20 percent of the cost of this building and only cost 20 percent as much. If they want LEED certification for the building, build it out of reclaimed materials and materials that are grown in Michigan. Elevators have been added at the last minute and they are not blended into the design.

Mr. Motzny reiterated that any recommendation from the Troy Planning Commission has to be in accordance with the City of Troy Zoning Ordinance. Denial can only be based upon a provision of that law. If the site plan meets the Zoning Ordinance, it should be approved.

Mr. Strat said he is disappointed to hear that the Troy Planning Commission is forced to approve the drawings because they meet the ordinance requirements. Secondly he thinks the City Council will be basing their judgment on the recommendations of the Planning Commission and not necessarily on what they see. Also, he noted that the City of Troy is going through difficult economic times and wondered how they could afford to maintain all of the energy efficient products and electronics and what the return is on the investment.

Mr. Williams asked if there is an agreement in place between Troy and Birmingham on sharing the maintenance and operating costs. Ms. Ecker said the two communities have discussed how they would proceed but there is no formal agreement. Mr. Williams said it is conceivable that Birmingham will share a significant portion of the increased operating costs. If that is so, he objects. Ms. Ecker said that estimates for the operations and maintenance costs are not higher for this building compared to average construction. In fact, consumption of utilities is significantly lower for this project than for an average building of this size.

It was discussed by Mr. Motzny that the Troy Planning Commission can make recommendations to the plan but they would be recommendations only.

Mr. Tagle said tonight was the first time he heard that the building would be totally utilitarian. The Troy Planning Commission would be derelict in its duty not to make

recommendations as part of discussion. It would be prudent for this group to have in hand a construction budget for this project.

Ms. Ecker explained that the ramps are needed should the elevator fail to operate. Mr. Tagle observed that the elevators could go down because they are unprotected.

Mr. Sonia spoke about the urgency of making decisions on the Preliminary Site Plan. He would hate to lose the grant money. Ms. Ecker agreed that time is of the essence on every single grant agreement. The economic stimulus packages that have been approved by Congress all have detailed time limitations.

Ms. Therese Cody from the Michigan Dept. of Transportation, their direct contact with the State of Michigan, spoke. She is acting as liaison with the Federal Railroad Administration ("FRA") with regard to the \$8.4 million grant that has been received. She gave background on how much work has been done. The FRA does not care what the building looks like; however they look very closely at how any changes to the original application are presented. Specifics have to be documented as to why and what the cost difference is. If changes are to be made she suggested they should be completed within a short time.

Mr. Koseck thought all of the issues could be addressed by the architects within a month. Mr. Surhigh said the project could take a year to build.

Mr. Williams said that right now he is not comfortable with the design elements of the project because some of his fellow members from Troy are not comfortable. That causes him concern because this is a joint project. It was noted that two changes to the original application are the repositioning of the building and the addition of elevators.

Mr. Edmunds asked if the changes that are seen tonight saved any money. Mr. Ancypa responded they have been trade-offs. Mr. Edmunds thought it is essential to have the cost estimates. This is a public project using tax dollars and in the end the cities will have to answer to the public. Discussion contemplated whether the project may be exceeding its funding.

Ms. Ecker agreed to check with the Birmingham City Attorney if the Planning Board could grant Preliminary and Final Site Plan Approval on the same night. She believes it would have to be noticed to that effect.

Mr. Miller explained that Troy has an Administrative Final Site Plan Approval that considers all of the check points and it happens just prior to construction.

It was determined that the two planning entities should conduct all of their meetings jointly because this a mutual project and it is important for everyone to stick together and to come to consensus as a group even though they will vote individually.

Vice-Chairperson Lazar invited public comments at 9:10 p.m.

Mr. Alan Green, Counsel for Grand Sakwa Properties, asked if there have been any impact assessments such as an updated traffic report, and the basis of the design. As an adjacent property owner, Grand Sakwa is concerned about how the Transit Center will affect their property. He suggested that the joint planning bodies table this matter so those issues can be addressed.

Mr. Green pointed out an issue that Grand Sakwa has with the City of Troy. The City's title to the property results from a Consent Judgment that was entered into about ten years ago. There were conditions that had to be satisfied in that Consent Judgment that need to happen by June 2. It is Grand Sakwa's contention that those conditions were not satisfied and therefore they are entitled to legal title to the property. That matter is pending now before the Oakland County Circuit Court. Therefore, they think it is premature for this body to go forth with a project.

Ms. Dorothy Conrad from Birmingham expressed her concern that the "sheds" that are being proposed on the Birmingham side don't do anything to keep out the elements for people waiting for the train. Further, she was concerned about the design of the building.

Mr. Michael Poris from Birmingham said he would hate to see them lose the Transportation Center because of a lot of issues concerning the process that could be resolved with some design revisions.

Mr. Ullmann offered a resolution that the Troy Planning Commission postpone this item. He thinks the Planning Commission should meet as soon as it legally can and then have a joint meeting with the Birmingham Planning Board August 4.

Mr. Tagle maintained that the boards need to keep this as a joint effort.

Mr. Edmunds pointed out that the Troy Planning Commission members have great reservations about the project; particularly because the current plan with the elevators is only an alternate, and because of the cost of the project.

Mr. Ullmann rephrased his motion as follows:

Motion by Mr. Ullmann

Seconded by Mr. Tagle to postpone the hearing on the Preliminary Site Plan for the Transit Center and hold a joint meeting of the Troy Planning Commission and the Birmingham Planning Board on July 27 in Troy at 7 p.m. to continue discussion on the Preliminary Site Plan.

Motion carried, 8-0.

ROLLCALL VOTE:

Yeas: Ullmann, Tagle, Edmunds, Hutson, Maxwell, Sanzica, Schultz, Strat

Nays: None
Absent: Vleck

Motion by Mr. Williams

Seconded by Mr. Koseck that the Birmingham Planning Board postpone the Special Joint Meeting to consider the Preliminary Site Plan for the Transit Center to July 27 at 7 p.m. in the City of Troy.

Motion carried, 6-0.

There were no comments from the public at 9:35 p.m.

ROLLCALL VOTE:

Yeas: Williams, Koseck, Clein, DeWeese, Lazar, Whipple-Boyce

Nays: None

Absent: Boyle

07-129-10

MEETING OPEN TO THE PUBLIC FOR ITEMS ON THE AGENDA (no one spoke)

07-130-10

ADJOURNMENT

No further business being evident, meeting adjourned at 9:40 p.m.

Respectfully submitted,

Jana Ecker
Planning Director
City of Birmingham

Mark Miller
Acting City Manager
City of Troy

The Joint Meeting of the Birmingham Planning Board and Troy Planning Commission was called to order by Troy Planning Commission Chair Hutson at 7:00 p.m. on July 27, 2010 in the Lower Level Conference Room of the Troy City Hall.

1. ROLL CALL

Troy Planning Commission

Present:

Michael W. Hutson, Chair
Donald Edmunds
Philip Sanzica
Robert M. Schultz
Thomas Strat
John J. Tagle
Lon M. Ullmann
Mark J. Vleck

Absent:

Mark Maxwell

Troy Administration

Mark F. Miller, Acting Assistant City Manager/Economic Development Services
R. Brent Savidant, Acting Planning Director
Allan Motzny, Assistant City Attorney
William Huotari Assistant City Engineer
Wanda Norman, Planning Intern
Kathy L. Czarnecki, Recording Secretary

Birmingham Planning Board

Present:

Robin Boyle, Chair
Scott Clein
Bert Koseck
Gillian Lazar
Janelle Whipple-Boyce
Bryan Williams
Aaron Walden, Student Representative

Absent:

Carroll DeWeese

Birmingham Administration

Jana Ecker, Planning Director
Matthew Baka, Planning Intern

Also present:

Sally Elmiger, Carlisle/Wortman Associates, Inc.
Walter Alix, Hubbell, Roth & Clark
Michael McDonald, Hubbell, Roth & Clark
James Surhigh, Hubbell, Roth & Clark

2. APPROVAL OF AGENDA

Resolution # PC-2010-07-052

Moved by: Schultz
Seconded by: Edmunds

RESOLVED, To approve the Agenda as prepared.

Yes: All present (14)
Absent: DeWeese, Maxwell

MOTION CARRIED

3. APPROVAL OF MINUTES

Mr. Ullmann requested to revise page 6, 1st paragraph, last sentence, to read: *“In fact, consumption of utilities is approximately 20% lower for this project.”*

Mr. Edmunds requested to revise page 3, 2nd paragraph, correct Mr. Ecker to read Ms. Ecker; further, to globally search and correct spelling of Grand Sacqua to *Grand Sakwa*.

Resolution # PC-2010-07-053

Moved by: Sanzica
Seconded by: Edmunds

RESOLVED, To approve the minutes of the July 14, 2010 Special Joint Meeting as revised.

Yes: All present (14)
Absent: DeWeese, Maxwell

MOTION CARRIED

4. PUBLIC COMMENT

There was no one present who wished to speak.

5. TRANSIT FACILITY SYNOPSIS – 2006 TO 2010

Mark Miller, Acting Assistant City Manager/Economic Development Services, gave a PowerPoint presentation on the Transit Center Synopsis from the years 2006 to 2010. He addressed the following:

- Design and Site Plan Development.
- Major Building and Site Design Features.
- Considerations Incorporated into Current Site Plan.
- Sustainability.
- Transit Center Planning.
- Public and Private Investments.

6. ALTERNATIVE TRANSIT CENTER LAYOUTS

Jana Ecker, Birmingham Planning Director, presented the item.

Ms. Ecker listed items that would not change with the Transit Center layout.

- Land on the Troy side.
- Parking lot.
- Need for tunnel.
- Doyle Drive.

Ms. Ecker announced items of focus are:

- Building location.
- Elevators.
- Platform.

Ms. Ecker addressed the design details of four alternative site plans (Site Plans A, B, C and D) and two canopy elevations. She stated that all four site plans comply with the American Disabilities Act (ADA).

Mr. Savidant stated that members of the Troy's Advisory Committee for Persons with Disabilities reviewed the site plans for insight.

Discussion followed on:

- ADA compliance; slopes, heated sidewalks.
- Cost estimates.
- Elevators; in building or stand-alone, operation and maintenance plan, security, capacity.
- Building location/potential expansion.

With a hand count, it was the consensus of the Troy Planning Commission members to incorporate an enclosed elevator on the Troy side. With that consensus, it was determined that it would be necessary to change the building location.

With a hand count, it was the consensus of the Birmingham Planning Board members to incorporate a stand-alone elevator on the Birmingham side.

Ms. Ecker addressed the canopy and platform.

It was the consensus of the Birmingham Planning Board members to focus on the canopy and platform after the overall Transit Center design is determined.

7. TRANSIT CENTER CONSENSUS BUILDING EXERCISE

Sally Elmiger of Carlisle/Wortman Associates, Inc. conducted a consensus building exercise to determine various design elements of the Transit Center.

Design element panels displayed:

Building	Bus Amenities
Train Platform/Canopy	Bicycle Amenities
Elevator Design	Wayfinding
Tunnel	Landscaping
Pedestrian Circulation	Lighting
Plazas	Other

Each Planning Commissioner was asked to write the three most important ideas/comments they had about the Transit Center site design and place on the relevant design element panel.

After a five-minute break, Ms. Elmiger facilitated discussion of the ideas on each design element panel. The design team will provide results of the exercise to the Planning Commissioners at the next joint meeting.

There was discussion around the table, with consensus on the following:

- New building design.
 - Scale of other buildings.
 - Building relationship to train track.
 - Expert logic/analysis on design.
 - View from Doyle Drive.
 - View from train tracks.
 - Image at nighttime.
 - Original “vision” (February 2008 report).
 - Consider users, security and vision.
- Platform/canopy.
 - Eliminate “bus stop image”.
- Elevator design.
 - Integrate with building.
 - Security (24 hour operation).
- Pedestrian circulation.
 - Revamp/rework sloped ramp; potential to eliminate.
 - Reduce concrete, heating elements, distance of walkways.
- LEED (status).

- Costs.
 - Major concern.
 - Justification of numbers to City Council(s).
- Funding.
 - Time limitation.
 - MDOT 20% match.
 - \$1.3 M federal appropriation.
 - City(s) contributions.

8. SCHEDULE NEXT MEETING

Chair Hutson announced the next joint meeting is scheduled on Wednesday, September 8, 2010 at the Birmingham Department of Public Services Building.

OTHER BUSINESS

9. PUBLIC COMMENTS – Items on Current Agenda

Pam Brady – Ms. Brady addressed accommodations for luggage and lockers.

Chris Corden of Grand Sakwa – Mr. Corden addressed the vested interest of Grand Sakwa in the Transit Center and the company's association and history with the City of Troy. He indicated a detailed letter was sent to the City of Troy outlining issues and concerns relating to parking (ratio, overflow), utilization of center (bus, train, taxi, sedan), among other concerns. Mr. Corden said he believes the issues and concerns of Grand Sakwa have not been shared with the Planning Boards, and addressed the Consent Judgment in terms of design materials used in the Transit Center.

Ms. Ecker responded that Consent Judgment issues are best handled by legal representatives of the City(s).

Mr. Miller responded that Troy's City Attorney replied to the letter from Grand Sakwa.

Barb Quincy – Ms. Quincy addressed concerns with overflow parking in the Mid-Town Square Condominiums. She also questioned the ownership of Doyle Drive.

Mr. Savidant responded that Doyle Drive is a private road.

Chair Hutson responded that Doyle Drive ownership and Consent Judgment concerns are matters best handled by the legal representatives of the City(s).

ADJOURN

The Joint Meeting of the Birmingham Planning Board and Troy Planning Commission adjourned at 9:58 p.m.

Respectfully submitted,

Michael W. Hutson, Troy Planning Commission Chair

Kathy L. Czarnecki, Recording Secretary

G:\Planning Commission Minutes\2010 PC Minutes\Draft\07-27-10 Joint Meeting Birmingham and Troy_Draft.doc

DATE: August 27, 2010

TO: Troy Planning Commission

FROM: R. Brent Savidant, Acting Planning Director

SUBJECT: PRELIMINARY SITE PLAN APPROVAL (File Number SP 957) - Troy/Birmingham Intermodal Transit Facility – South of Maple Road, West of Coolidge, Section 31, Zoned M-1 (Light Industrial) - Controlled by Consent Judgment

GENERAL INFORMATION

A revised Preliminary Site Plan has been submitted for Planning Commission consideration. The transit center building and Amtrak platform were revised based on comments generated at the July 27, 2010 Joint meeting and previous meetings.

Name of Owner / Applicant:

The applicant is the City of Troy and the City of Birmingham. The cities entered into a joint venture to develop the Intermodal Transit Facility, which sits on both sides of the C.N. right-of-way. The Troy site was deeded to the City of Troy as part of the Consent Judgment, with the condition that it be developed as a transit center.

Location of Subject Property:

The property is located south of Maple Road, west of Coolidge. The transit center building is in Troy, on the east side of the C.N. Railroad right-of-way, in section 31. The Amtrak platform is in Birmingham, on the west side of the tracks.

Size of Subject Parcel:

The Troy site is approximately 2.71 acres in size. The Birmingham site is approximately 1.51 acres in size. The total site area is 4.22 acres.

Current Use of Subject Property:

The 2.71-acre Troy site is currently comprised of a triangular grassy area, drive and parking area. The site abuts the Midtown Square shopping center to the east and the C.N. Railroad right-of-way to the west. The right-of-way also serves as the boundary between the cities of Troy and Birmingham. There is currently an Amtrak platform and shelter located in Birmingham, on the west side of the tracks.

Current Zoning Classification:

The Troy site is zoned M-1 Light Industrial (controlled by Consent Judgment).

Proposed Uses and Buildings on Subject Parcel:

A 2,400 square foot building is proposed for the site. The building will include a public waiting/seating area, restroom facilities, drinking fountains, mechanical/electrical room, storage room, and future kiosk space for transit service providers. A plaza area in front of the building features a brick paver design and vertical element. The design and funding source for the vertical element will be determined in the future.

The Amtrak platform (Birmingham) and the Transit Center building (Troy) will be linked by a tunnel under the tracks. Besides offering a connection between the Transit Center and the Amtrak platform, the tunnel also serves as a non-motorized link between Troy and Birmingham.

Access to the tunnel from both sides of the tracks will be provided by barrier-free ramps and stairs, as well as an elevator on the Troy side of the tracks. Design elements intended to improve accessibility include pedestrian scale lighting, hand rails, horizontal landing areas, benches, and radiant heat under the ramp to melt ice and snow during winter months. The ramp/stair area will be landscaped to improve aesthetics, and reduce soil erosion and storm water runoff.

The Amtrak platform in Birmingham will be enhanced by the addition of a large canopy, shielded on four sides to protect users from the elements. Access to the site will be improved with the addition of a new public street, connecting the site to Eton Street in two locations. Additional off-street parking spaces will be provided, including handicapped spaces, to provide convenient access for train users. Sidewalks connecting the site to adjacent neighborhoods and commercial areas will also be provided.

A drop-off area with waiting slips for four (4) SMART buses is proposed next to the transit center building.

Current Use of Adjacent Parcels:

North: Midtown Square shopping center.
South: Midtown Square shopping center.
East: Midtown Square shopping center.
West: Mini storage facility and C.N. right-of-way.

Zoning Classification of Adjacent Parcels:

North: M-1 Light Industrial.
South: M-1 Light Industrial.
East: M-1 Light Industrial.
West: No zoning (C.N. right-of-way).

Future Land Use Designation:

The Master Plan classifies the property as being within the Transit Center classification.

ANALYSIS

The site is located within the M-1 Light Industrial District. However, it is controlled by consent judgment.

Compliance with area and bulk requirements:

Setback	Required	Provided
Front yard	25 feet (Sec 40.10.08)	Approximately 40 feet (to Doyle Drive)
Side yard	10 feet (each) 20 feet (total)	11 feet (east), 100 feet (west)
Rear yard	20 feet	82 feet (south)
Building height	3 stories, 40 feet	1 story, 20 feet
Maximum lot coverage by buildings	40 percent	Approximately 2 percent

The site plan meets all setback and bulk requirements for the M-1 Light Industrial district. It must be noted, the site is located in close proximity to the Oakland/Troy Airport. Therefore, maximum building height is also governed by the requirements of the FAA and MDOT Bureau of Aeronautics and Freight Services.

Off-Street Parking:

The Troy site plan provides 106 conventional parking spaces, 8 barrier free spaces, and 2 additional spaces reserved for electric vehicles, for a total of 116 spaces. The Birmingham side provides 29 conventional spaces and 6 barrier free spaces, for a total of 35 spaces. Combined, there are 151 parking spaces proposed for the transit facility.

The City of Troy Zoning Ordinance does not include a parking standard for public transit facilities. The transit center itself will have minimal staffing and will therefore generate minimal to no actual parking demand. Parking demand will be generated by the transit uses on the site, not by the building. Therefore the square footage of the transit center building is irrelevant for the purpose of determining parking demand.

The parking provided at the site will be primarily used by transit system users. Most of the SMART bus users will arrive at the site by bus, foot or train. Therefore most of the demand for parking spaces will be by Amtrak users. It is anticipated that Amtrak users would continue to park on the Birmingham side, where spaces are located closer to the train platform. The 35 spaces provided on the Birmingham side represent a significant increase over the 4 spaces presently dedicated for Amtrak users. The 4 existing spaces will remain after the transit center is constructed. It is anticipated that when the Birmingham lot becomes full, Amtrak users will park in the Troy parking lot.

The proposed 116-space parking area is the maximum number of spaces given the size of the property and it is sufficient for the transit center facility at this time. The parking area should be reviewed regularly to ensure that parking demand for transit center users is being met, when transit use increases.

Vehicular and Non-motorized Access:

Vehicular access to the transit center building is provided by Doyle Drive, which intersects Maple Road to the north and Coolidge Highway to the east. This drive will be used by automobiles and

buses. On the Birmingham side, an access drive connecting to Eton Street provides access to the Amtrak platform, including a drop-off and parking area.

Non-motorized access is provided by a series of sidewalks that connect the transit center to the existing sidewalk system within the Midtown development. These in turn connect to the public sidewalks along Maple and Coolidge. The entire transit center site is of barrier-free design. A barrier-free tunnel connects the two sides of the tracks. Access to the tunnel is provided by a barrier-free ramp and an elevator on the Troy side.

Storm Water Detention:

The applicant proposes to utilize innovative techniques to store and treat storm water, including a green roof, rain gardens and the harvesting of rainwater to use for watering plants on the site.

Environmental Provisions:

The building and site will be a demonstration project for sustainable construction practices. These include a green roof, rain gardens and geothermal HVAC. The applicant intends to pursue LEED certification for the facility.

Natural Features and Floodplains:

The Natural Features Map indicates there are no significant natural features located on the property.

CITY MANAGEMENT RECOMMENDATION

Because the site is controlled by Consent Judgment, the Planning Commission is a recommending body for the Preliminary Site Plan. City Council has approval authority on the transit center elements within the City of Troy.

The Preliminary Site Plan meets Zoning Ordinance and Consent Agreement requirements. City Management recommends approval of the Preliminary Site Plan, as submitted.

Attachments:

1. Maps

cc: File/ SP 957

Prepared by RBS

G:\SITE PLANS\SP 957 Troy Birmingham Intermodal Transit Facility\PC Report 09 08 10.docx

PROPOSED RESOLUTION

PRELIMINARY SITE PLAN APPROVAL (File Number SP 957) - Troy/Birmingham Intermodal Transit Facility – South of Maple Road, West of Coolidge, Section 31, Zoned M-1 (Light Industrial) - Controlled by Consent Judgment)

Proposed Resolution # PC-2010-07-

Moved by:
Seconded by:

RESOLVED, The Planning Commission recommends that Preliminary Site Plan Approval, as requested for the proposed Troy/Birmingham Intermodal Transit Facility, located south of Maple Road and west of Coolidge, in Section 31, within the M-1 zoning district, and controlled by consent judgment, be (granted, subject to the following design considerations):

_____) or

(denied, for the following reasons: _____) or

(postponed, for the following reasons: _____)

Yes:
No:
Absent:

MOTION CARRIED / DENIED

Intermodal Transit Facility

City of Troy Planning Department



Legend

-  I-75
-  Road Centerline
-  Major Road
-  Industrial Road
-  Local Road
-  Hydrography Poly
-  Hydrography Arc
-  Parcels
- Aerial Photos - 2008**
-  Red: Band_1
-  Green: Band_2
-  Blue: Band_3

826 0 413 826Feet

Scale 1: 4,956

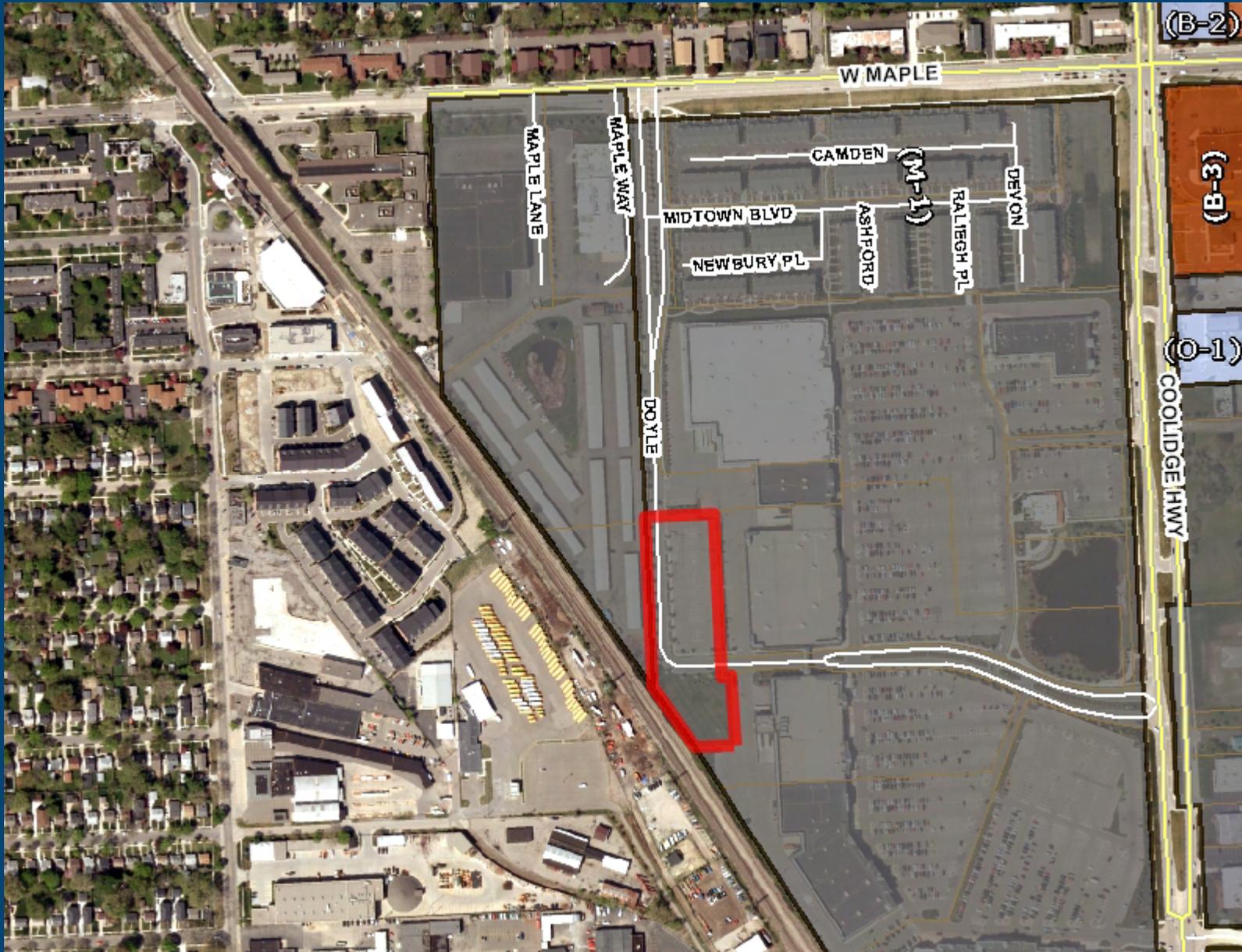


Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Printed: 7/19/2010

Intermodal Transit Facility

City of Troy Planning Department



Legend

- I-75
- Road Centerline**
 - Major Road
 - Industrial Road
 - Local Road
- Zoning**
 - (PUD) Planned Unit Development
 - (B-1) Local Business District
 - (B-2) Community Business District
 - (B-3) General Business District
 - (R-C) Research Center District
 - (C-F) Community Facilities District
 - (C-J) Consent Judgment
 - (E-P) Environmental Protection District
 - (R-EC) Residential Elder Care
 - (P-1) Vehicular Parking District
 - (H-S) Highway Service District
 - (M-1) Light Industrial District
 - (O-1) Office Building District
 - (O-M) Office Mid-Rise District
 - (OSC) Office Service Commercial District
 - (CR-1) One Family Residential Cluster District
 - (R-1A) One Family Residential District
 - (R-1B) One Family Residential District
 - (R-1C) One Family Residential District
 - (R-1D) One Family Residential District
 - (R-1E) One Family Residential District
 - (R-1T) One Family Attached Residential Distr
 - (R-2) Two Family Residential District
 - (R-M) Multiple Family Residential Medium De
 - (RM-1) Multiple Family Residential District Lc
 - (RM-2) Multiple Family Residential District (M
 - (RM-3) Multiple Family Residential District (Hi
- Hydrography Poly
- Hydrography Arc
- Parcels

Aerial Photos - 2008

- Red: Band_1
- Green: Band 2

Printed: 7/19/2010

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Scale 1: 4,956



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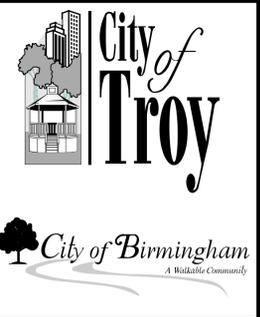
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QUEUE - V:\b\w\B\Water\Access\21.dwg

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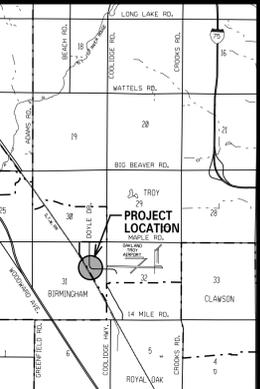


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 Consulting Engineers

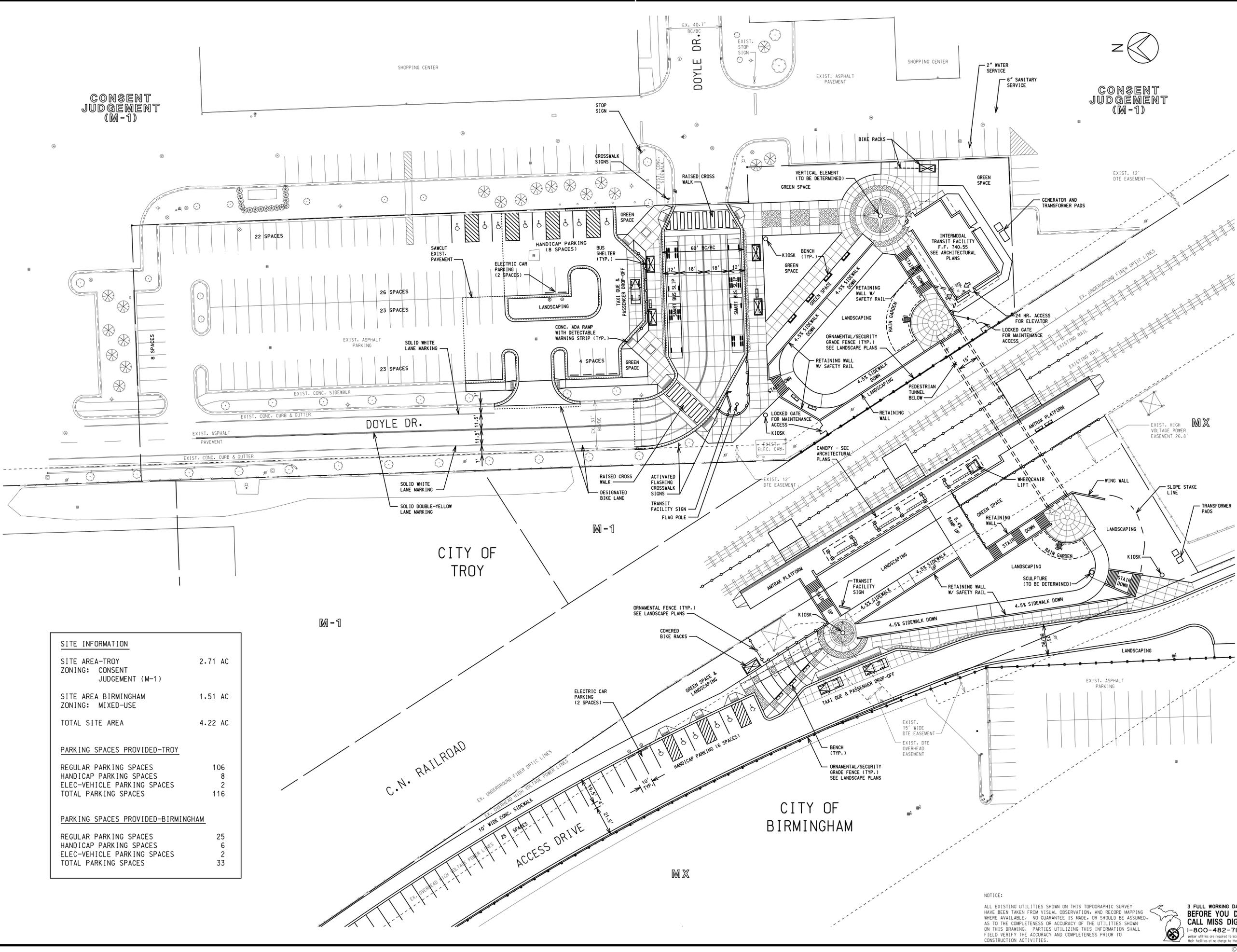
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 48303 - 0824

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8-27-10	PRELIMINARY SITE PLAN SUBMITTAL
7-8-10	PRELIMINARY SITE PLAN SUBMITTAL
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1-11-10	PRELIMINARY SITE PLAN SUBMITTAL
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9-11-09	REVISED SITE PLAN
8-21-09	JOINT PLANNING COMM. STUDY SESSION
DATE	ADDITIONS AND/OR REVISIONS
DESIGNED	J.J.S.
DRAWN	J.A.R.
CHECKED	J.J.S.
APPROVED	W.H.A.
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CITIES OF TROY & BIRMINGHAM	
INTERMODAL TRANSIT FACILITY	
OAKLAND COUNTY	MICHIGAN
PRELIMINARY SITE PLAN	
HRC JOB NO. 20090078	SCALE 1" = 30'
DATE AUGUST 2009	SHEET NO. C-01

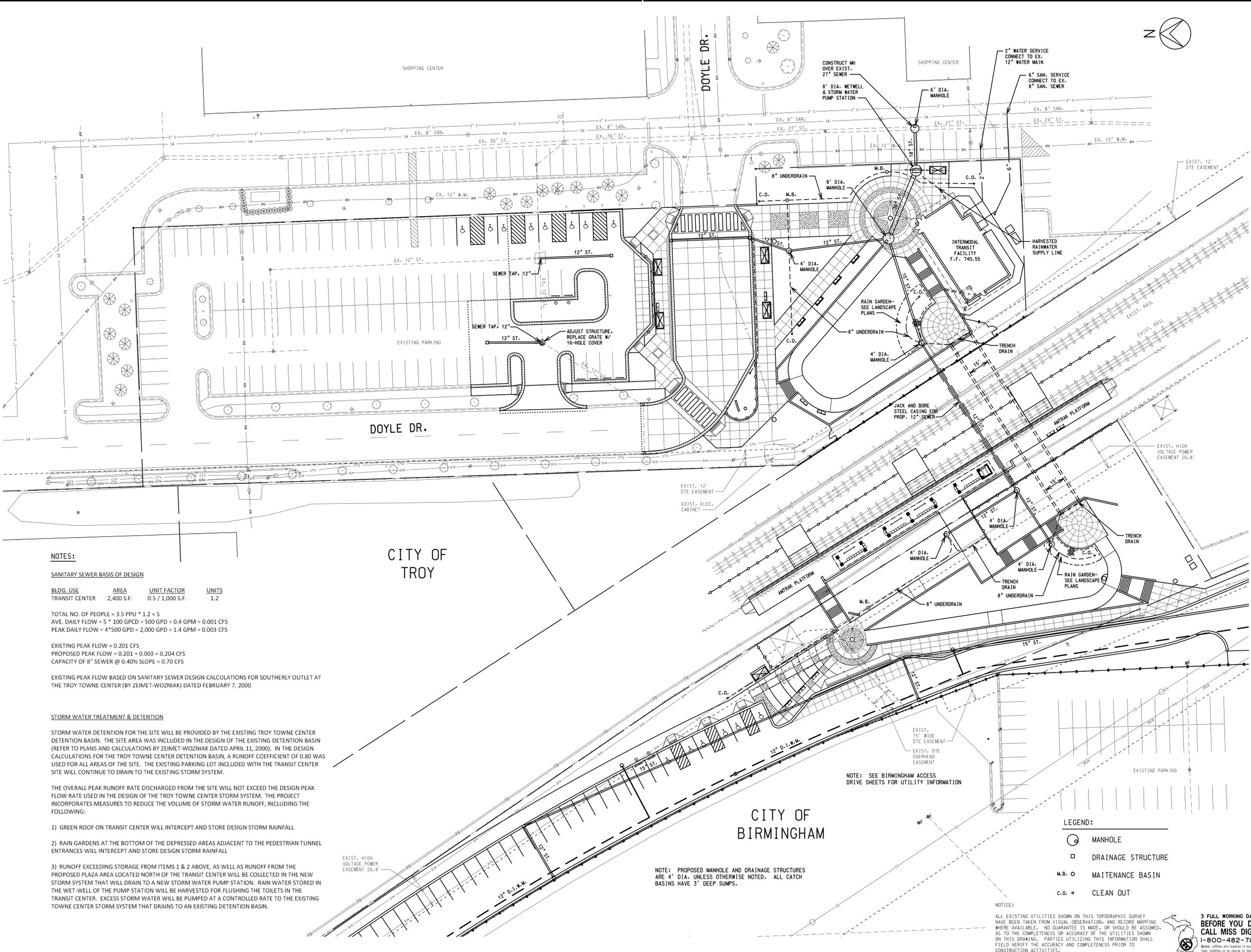


SITE INFORMATION	
SITE AREA-TROY	2.71 AC
ZONING: CONSENT JUDGEMENT (M-1)	
SITE AREA BIRMINGHAM	1.51 AC
ZONING: MIXED-USE	
TOTAL SITE AREA	4.22 AC
PARKING SPACES PROVIDED-TROY	
REGULAR PARKING SPACES	106
HANDICAP PARKING SPACES	8
ELEC-VEHICLE PARKING SPACES	2
TOTAL PARKING SPACES	116
PARKING SPACES PROVIDED-BIRMINGHAM	
REGULAR PARKING SPACES	25
HANDICAP PARKING SPACES	6
ELEC-VEHICLE PARKING SPACES	2
TOTAL PARKING SPACES	33

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NOTES:

SANITARY SEWER BASIS OF DESIGN

BLDG. USE	AREA	UNIT FACTOR	UNITS
TRANSIT CENTER	2,400 S.F.	0.5 / 1,000 S.F.	1.2

TOTAL NO. OF PEOPLE = 3.5 PPU * 1.2 = 5
 AVE. DAILY FLOW = 5 * 100 GPCD = 500 GPD = 0.4 GPM = 0.001 CFS
 PEAK DAILY FLOW = 4 * 500 GPD = 2,000 GPD = 1.4 GPM = 0.003 CFS

EXISTING PEAK FLOW = 0.201 CFS
 PROPOSED PEAK FLOW = 0.201 + 0.003 = 0.204 CFS
 CAPACITY OF 8" SEWER @ 0.40% SLOPE = 0.70 CFS

EXISTING PEAK FLOW BASED ON SANITARY SEWER DESIGN CALCULATIONS FOR SOUTHERLY OUTLET AT THE TROY TOWNE CENTER (BY ZEIMET-WOZNIAK) DATED FEBRUARY 7, 2000

STORM WATER TREATMENT & DETENTION

STORM WATER DETENTION FOR THE SITE WILL BE PROVIDED BY THE EXISTING TROY TOWNE CENTER DETENTION BASIN. THE SITE AREA WAS INCLUDED IN THE DESIGN OF THE EXISTING DETENTION BASIN (REFER TO PLANS AND CALCULATIONS BY ZEIMET-WOZNIAK DATED APRIL 11, 2000). IN THE DESIGN CALCULATIONS FOR THE TROY TOWNE CENTER DETENTION BASIN, A RUNOFF COEFFICIENT OF 0.80 WAS USED FOR ALL AREAS OF THE SITE. THE EXISTING PARKING LOT INCLUDED WITH THE TRANSIT CENTER SITE WILL CONTINUE TO DRAIN TO THE EXISTING STORM SYSTEM.

THE OVERALL PEAK RUNOFF RATE DISCHARGED FROM THE SITE WILL NOT EXCEED THE DESIGN PEAK FLOW RATE USED IN THE DESIGN OF THE TROY TOWNE CENTER STORM SYSTEM. THE PROJECT INCORPORATES MEASURES TO REDUCE THE VOLUME OF STORM WATER RUNOFF, INCLUDING THE FOLLOWING:

- 1) GREEN ROOF ON TRANSIT CENTER WILL INTERCEPT AND STORE DESIGN STORM RAINFALL
- 2) RAIN GARDENS AT THE BOTTOM OF THE DEPRESSED AREAS ADJACENT TO THE PEDESTRIAN TUNNEL ENTRANCES WILL INTERCEPT AND STORE DESIGN STORM RAINFALL
- 3) RUNOFF EXCEEDING STORAGE FROM ITEMS 1 & 2 ABOVE, AS WELL AS RUNOFF FROM THE PROPOSED PLAZA AREA LOCATED NORTH OF THE TRANSIT CENTER WILL BE COLLECTED IN THE NEW STORM SYSTEM THAT WILL DRAIN TO A NEW STORM WATER PUMP STATION. RAIN WATER STORED IN THE WET-WELL OF THE PUMP STATION WILL BE HARVESTED FOR FLUSHING THE TOILETS IN THE TRANSIT CENTER. EXCESS STORM WATER WILL BE PUMPED AT A CONTROLLED RATE TO THE EXISTING TOWNE CENTER STORM SYSTEM THAT DRAINS TO AN EXISTING DETENTION BASIN.

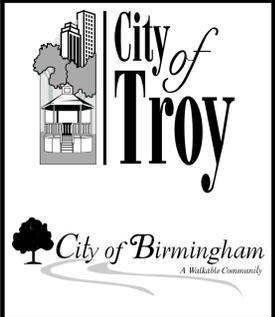
CITY OF TROY

CITY OF BIRMINGHAM

- LEGEND:**
- MANHOLE
 - DRAINAGE STRUCTURE
 - MAINTENANCE BASIN
 - CLEAN OUT

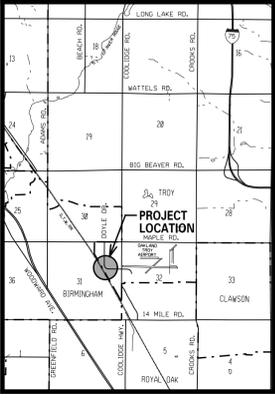
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DATE	ADDITIONS AND/OR REVISIONS	
8-27-10	PRELIMINARY SITE PLAN SUBMITTAL	
7-8-10	PRELIMINARY SITE PLAN SUBMITTAL	
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8-21-09	JOINT PLANNING COMM. STUDY SESSION	
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	CHECKED	J.J.S.
	APPROVED	W.H.A.



CITIES OF TROY & BIRMINGHAM

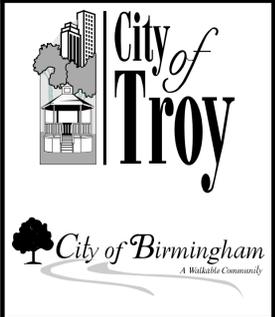
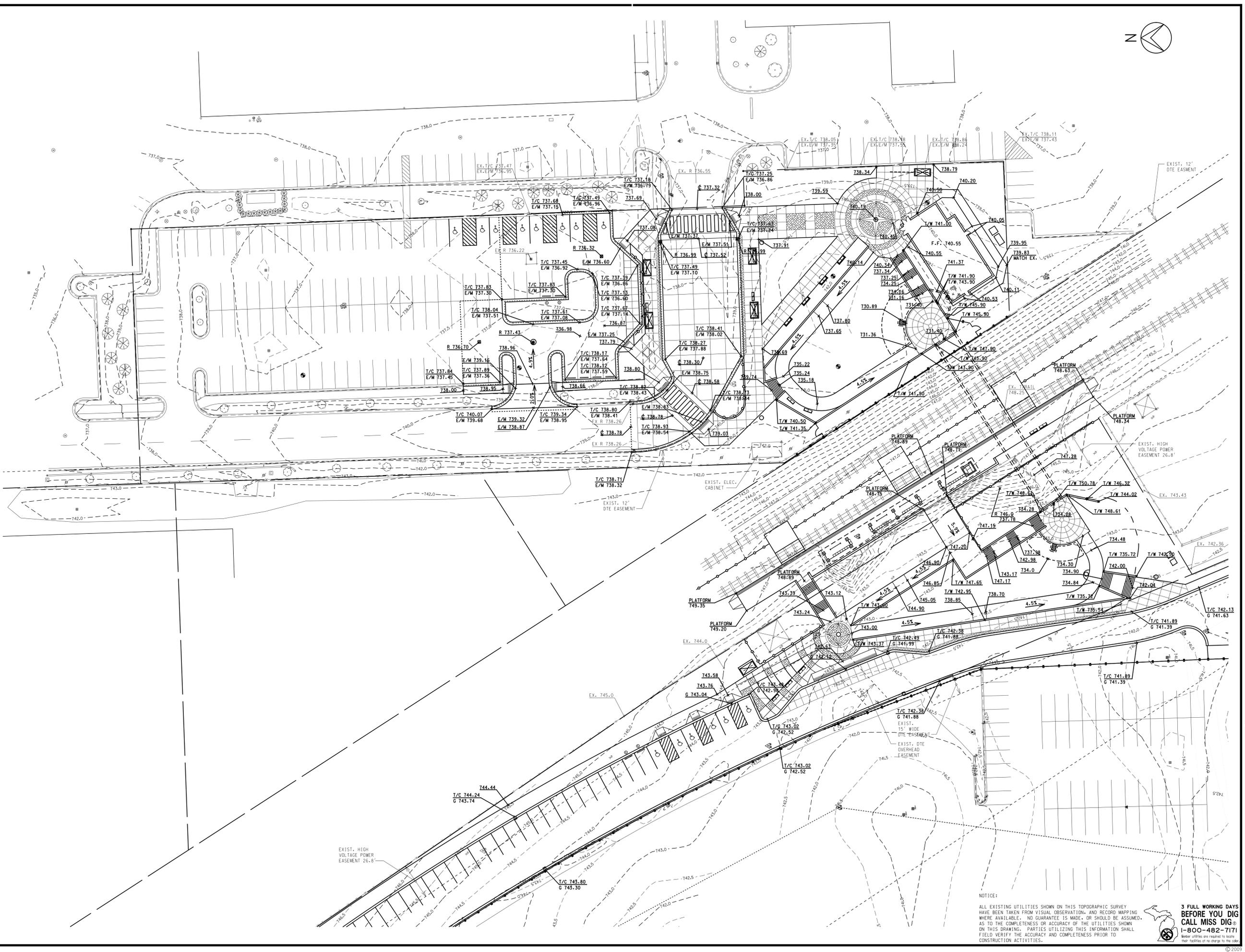
INTERMODAL TRANSIT FACILITY

OAKLAND COUNTY MICHIGAN

PRELIMINARY UTILITY PLAN

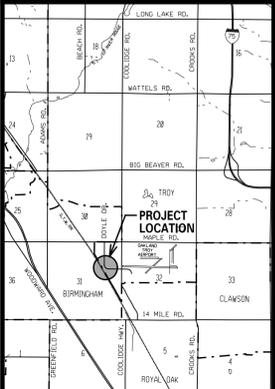
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DATE AUGUST 2009	SHEET NO. C-02 OF

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CITIES OF
 TROY & BIRMINGHAM
**INTERMODAL
 TRANSIT FACILITY**
 OAKLAND COUNTY MICHIGAN
**PRELIMINARY
 GRADING PLAN**
 HRC JOB NO. 20090078 SCALE 1"=30'
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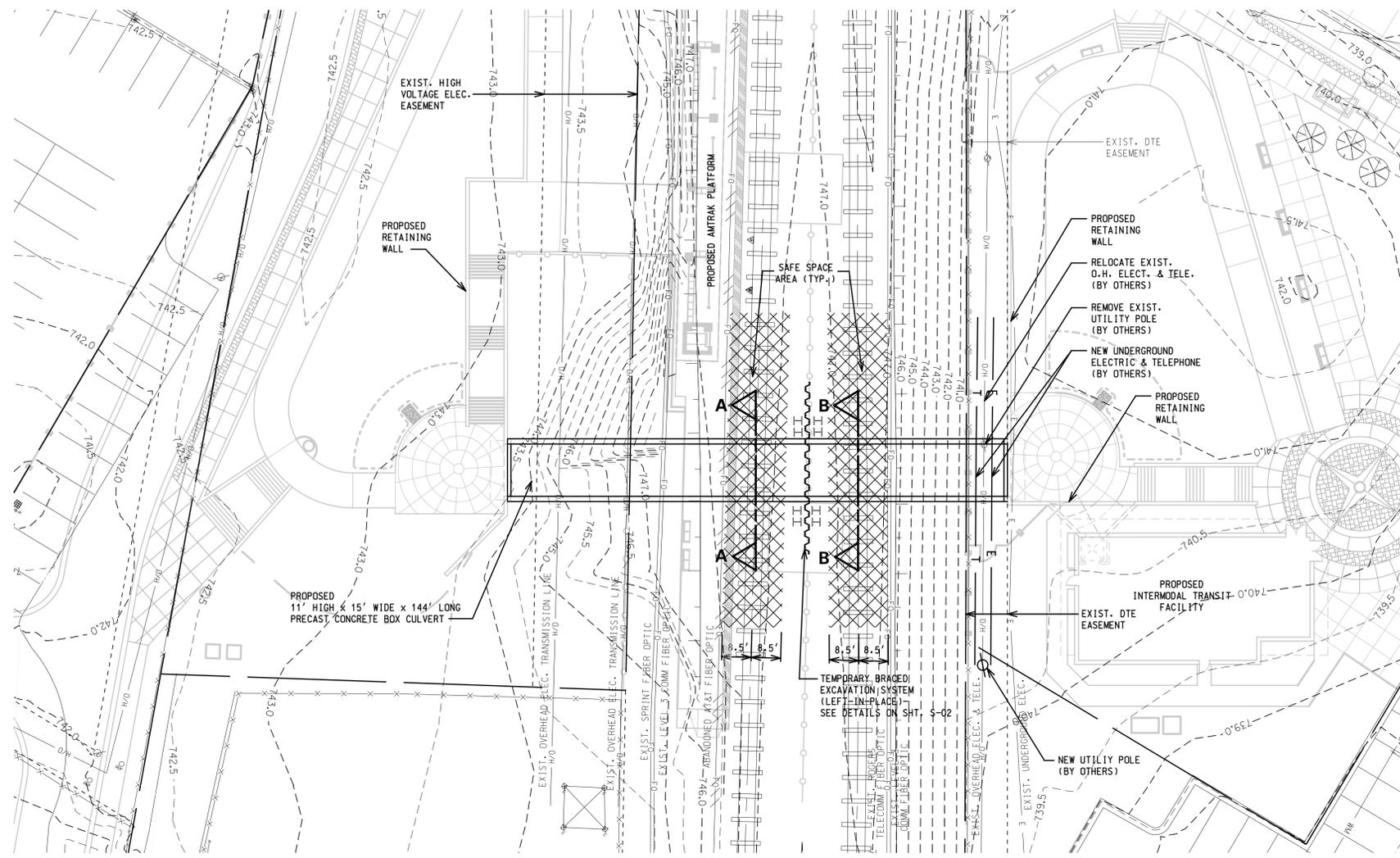
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DESIGN - 1-800-999-2211

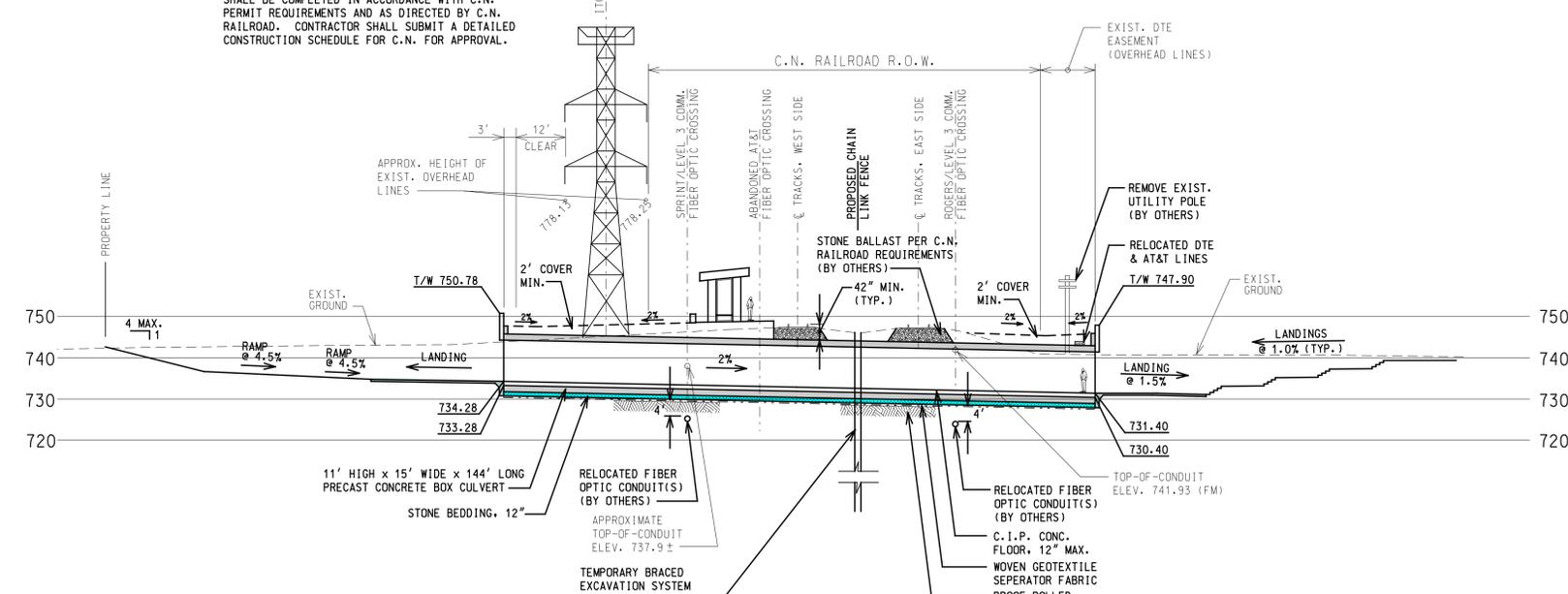
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USER NAME - Jrc



PEDESTRIAN TUNNEL CROSSING - PLAN VIEW
SCALE: 1" = 20'

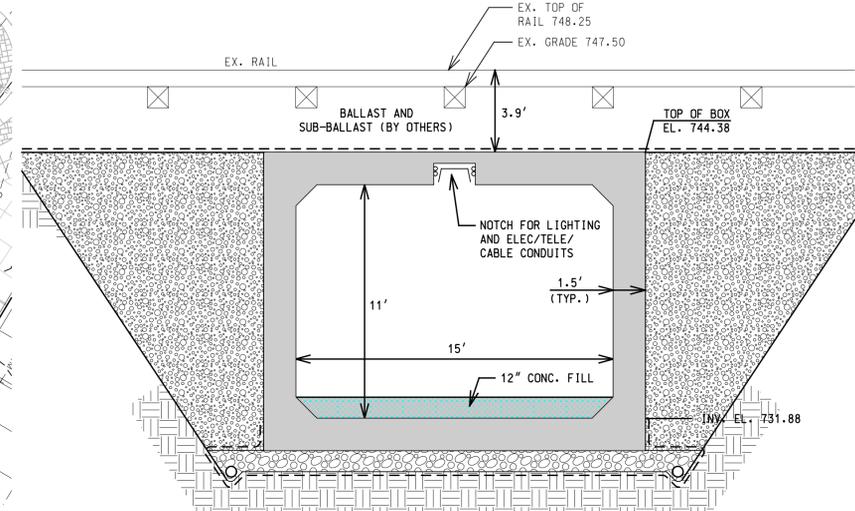
NOTE:
ALL WORK DONE IN C.N. RAILROAD RIGHT-OF-WAY SHALL BE COMPLETED IN ACCORDANCE WITH C.N. PERMIT REQUIREMENTS AND AS DIRECTED BY C.N. RAILROAD. CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE FOR C.N. FOR APPROVAL.



PEDESTRIAN TUNNEL CROSSING - PROFILE VIEW
SCALE: 1" = 20' HOR.
1" = 20' VERT.

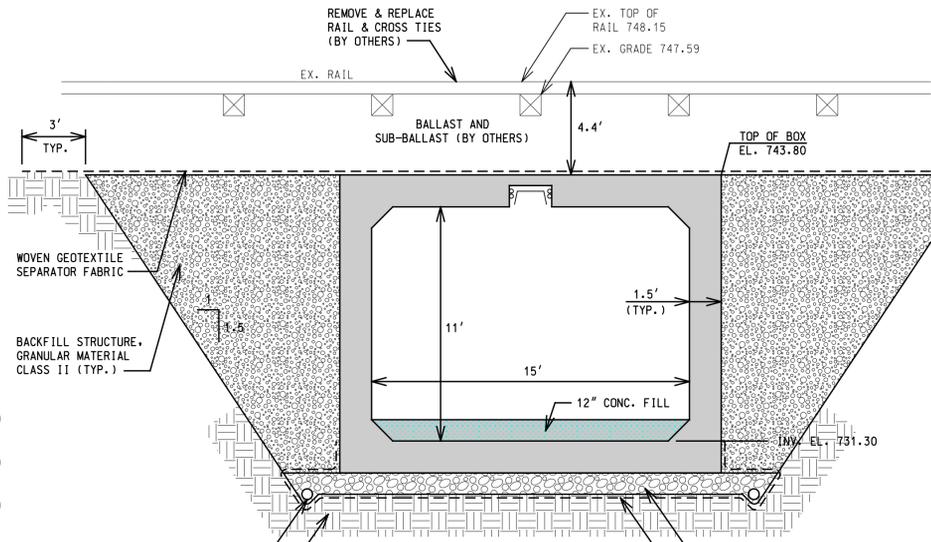
SEQUENCE OF CONSTRUCTION - PEDESTRIAN TUNNEL

1. CONTRACTOR SHALL OBTAIN RIGHT-OF-ENTRY PERMIT FROM CN PRIOR TO WORKING WITHIN THE CN RAILROAD RIGHT-OF-WAY. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE CN RIGHT-OF-ENTRY PERMIT AND ASSOCIATED SAFETY REQUIREMENTS.
2. INSTALL TEMPORARY BRACED EXCAVATION SYSTEM, CONSISTING MAINLY OF STEEL SHEET PILING AND SOLDIER PILES, WHILE MAINTAINING SERVICE ON BOTH SETS OF RAIL TRACKS. CONTRACTOR SHALL SUSPEND CRANE OPERATIONS OVER TRACKS WHEN NOTIFIED BY CN OF APPROACHING TRAINS. CURRENT DAILY TRAIN TRAFFIC CONSISTS OF SIX AMTRAK TRAINS AND TWO FREIGHT TRAINS. TEMPORARY OR PERMANENT OBSTRUCTIONS ARE NOT PERMITTED WITHIN THE SAFE SPACE AREA ALONG THE ACTIVE RAILROAD TRACKS. CONTRACTOR SHALL NOT DRIVE EQUIPMENT OVER TRACKS UNLESS SPECIFICALLY APPROVED AND PERMITTED BY CN.
3. CN WILL DIVERT TRAIN TRAFFIC TO THE WESTERMOST TRACKS AND REMOVE THE EASTERN TRACKS (RAILS, CROSS-TIES, ETC.) AND STONE BALLAST FROM THE WORK AREA. CONTRACTOR SHALL COMPLETE EXCAVATION, PEDESTRIAN TUNNEL INSTALLATION AND BACKFILL WORK UP TO THE TEMPORARY BRACED EXCAVATION SYSTEM. WORK BY CONTRACTOR MUST BE COMPLETED IN TEN DAYS. CN WILL REPLACE STONE BALLAST AND EASTERN TRACKS.
4. CN WILL DIVERT TRAIN TRAFFIC TO THE EASTERMOST TRACKS AND REMOVE THE WESTERN TRACKS (RAILS, CROSS-TIES, ETC.) AND STONE BALLAST FROM THE WORK AREA. CONTRACTOR SHALL EXCAVATE WESTERN FACE OF TEMPORARY BRACED EXCAVATION SYSTEM AND SHALL CUT-OUT SHEET PILING TO ALLOW EXTENSION OF THE PEDESTRIAN TUNNEL. CONTRACTOR SHALL COMPLETE EXCAVATION, PEDESTRIAN TUNNEL INSTALLATION AND BACKFILL WORK. THE UPPER PORTION OF THE TEMPORARY BRACED EXCAVATION SYSTEM MUST BE CUT-OFF AT THREE FEET BELOW FINISHED GRADE AND REMOVED FROM THE WORK AREA. WORK BY CONTRACTOR MUST BE COMPLETED IN TEN DAYS. CN WILL REPLACE STONE BALLAST AND WESTERN TRACKS.
5. CN WILL REMOVE ANY TRACK DIVERSIONS AND OPEN BOTH SETS OF TRACKS TO TRAIN TRAFFIC FOR REMAINDER OF PROJECT.



SECTION A-A
NOT TO SCALE

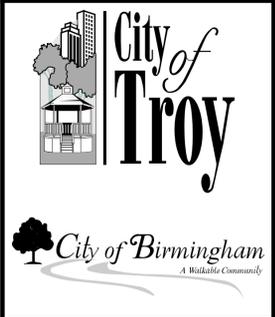
SEE STRUCTURAL DRAWING SHT. S-01 FOR BOX CULVERT AND REINFORCING STEEL DETAILS



SECTION B-B
NOT TO SCALE

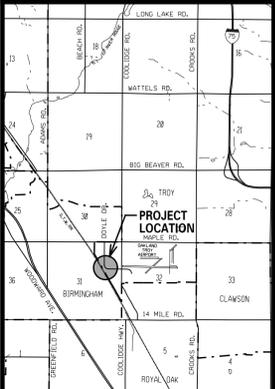
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48303 - 0824
PHONE: (248) 454-6300
FAX (1st Floor): (248) 454-6312
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DATE	ADDITIONS AND/OR REVISIONS
8-27-10	PRELIMINARY SITE PLAN SUBMITTAL
7-8-10	PRELIMINARY SITE PLAN SUBMITTAL
6-30-10	PRELIMINARY SITE PLAN SUBMITTAL
1-11-10	PRELIMINARY SITE PLAN SUBMITTAL
8-21-09	JOINT PLANNING COMM. STUDY SESSION
DATE	DESIGNED
	J.J.S.
DATE	DRAWN
	J.A.R.
DATE	CHECKED
	J.J.S.
DATE	APPROVED
	W.H.A.



CITIES OF TROY & BIRMINGHAM	
INTERMODAL TRANSIT FACILITY	
OAKLAND COUNTY	MICHIGAN
TUNNEL PROFILE AND SECTIONS	
HRC JOB NO. 20090078	SCALE AS NOTED
DATE AUGUST 2009	SHEET NO. C-04

TIME - 27-AUG-2010 14:59

REV. TBL - I:\MSV\B\B\Platting\Bldg2.dwg

CLB. TBL - I:\MSV\B\B\Bldg2\Bldg2.dwg

QUEL - V:\bldg\Bldg2.dwg

DESIGN FILE - I:\200900\20090078\Troy_rfw\wall_elev.dwg

USER NAME - Jrc



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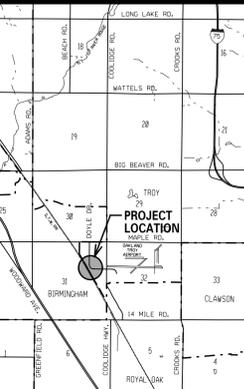
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8-21-09	JOINT PLANNING COMM. STUDY SESSION

DESIGNED **J.J.S.**
DRAWN **J.A.R.**
CHECKED **J.J.S.**
APPROVED **W.H.A.**

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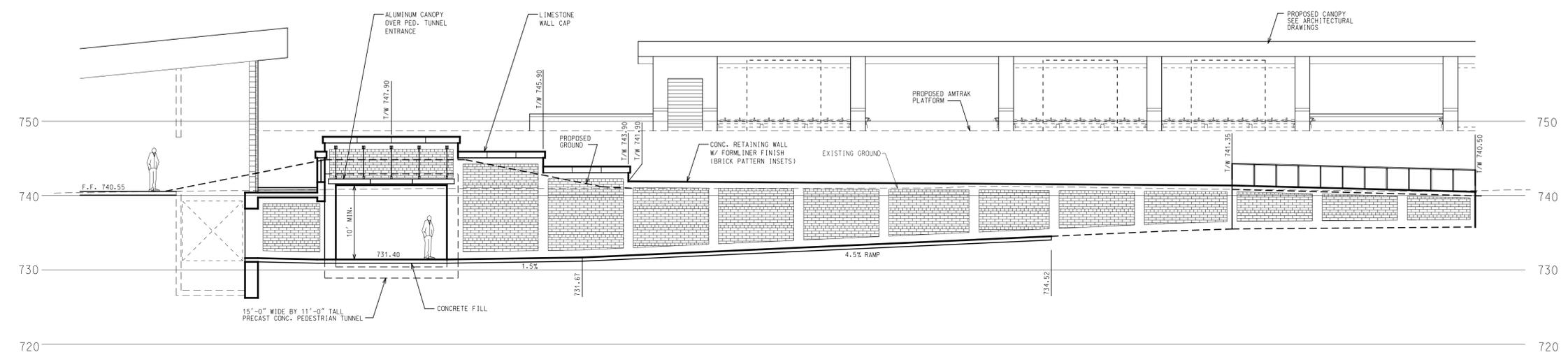
CITIES OF TROY & BIRMINGHAM

INTERMODAL TRANSIT FACILITY

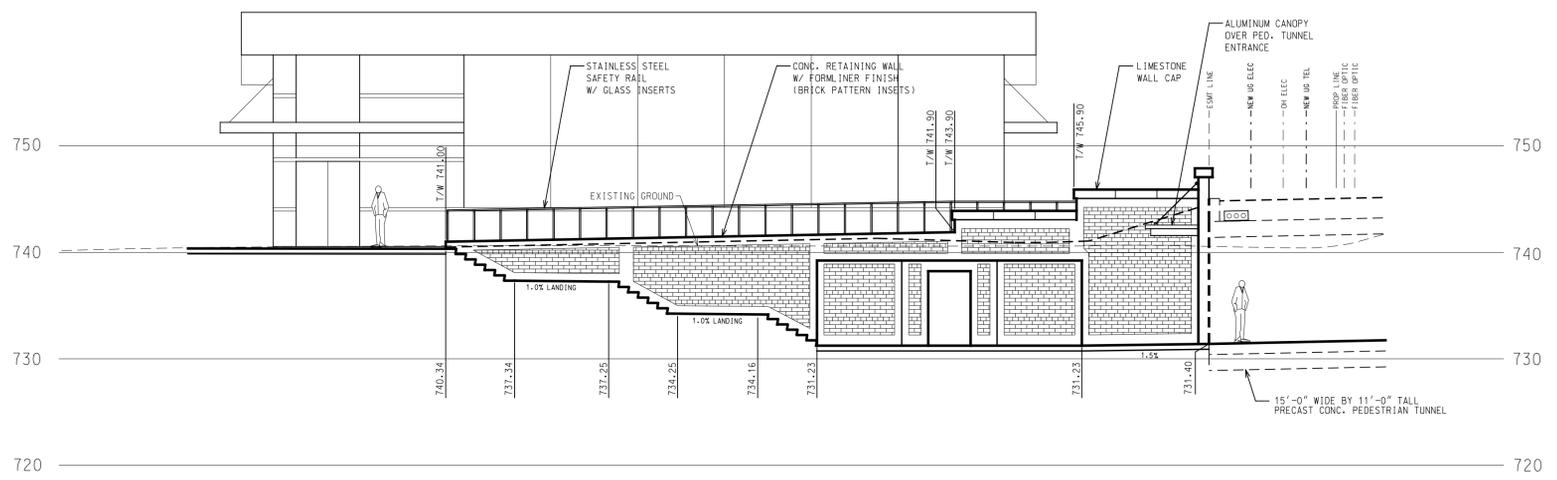
OAKLAND COUNTY MICHIGAN

PRELIMINARY TROY WALL ELEVATIONS

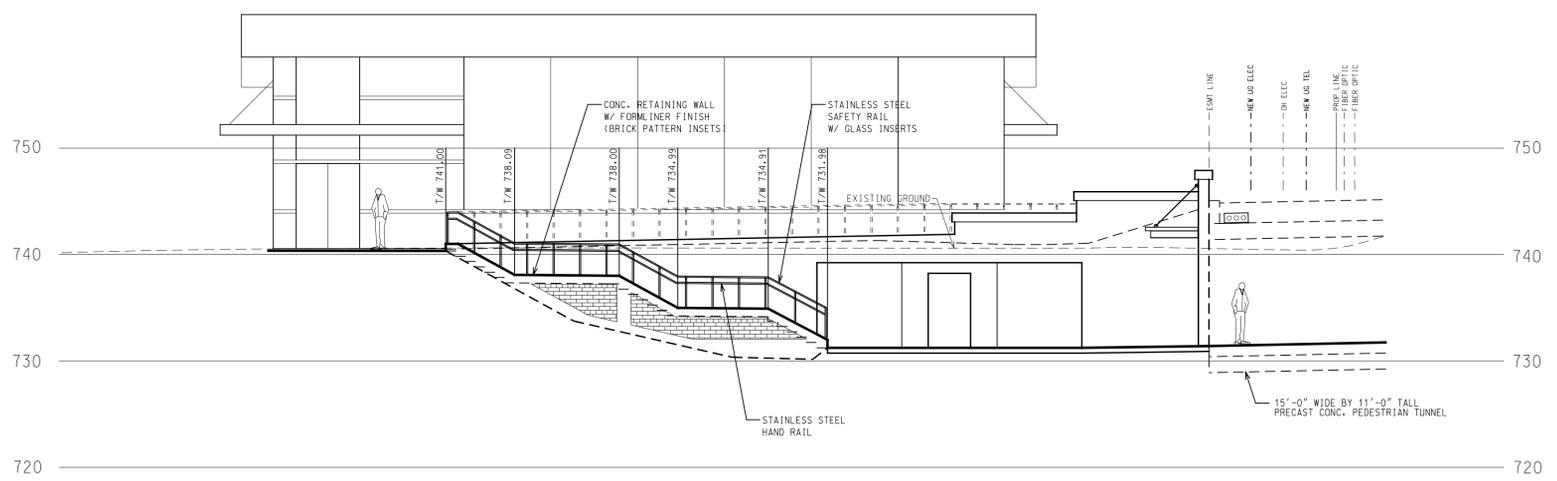
HRC JOB NO. 20090078 SCALE 1" = 8'
DATE AUGUST 2009 SHEET NO. C-05 OF



WALL ELEVATION LOOKING WEST ON TROY SIDE



WALL ELEVATION LOOKING SOUTH ON TROY SIDE



WALL ELEVATION LOOKING SOUTH ON TROY SIDE

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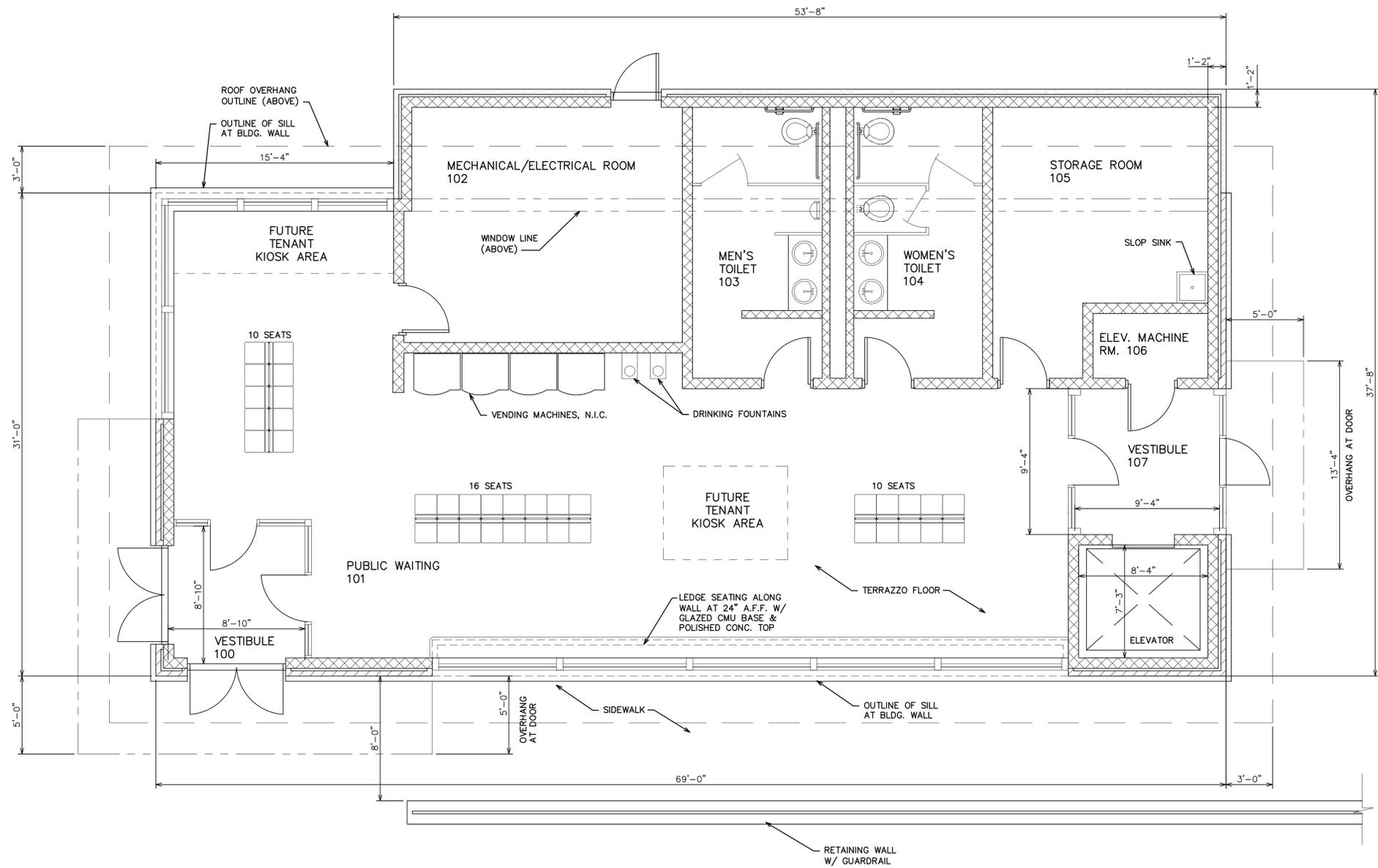
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Cochran, Jane



FLOOR PLAN
SCALE: 1/4" = 1'-0"



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555 HULET DRIVE
BLOOMFIELD HILLS, MICH. P.O. BOX 824
48303 - 0824
PHONE: (248) 454-6300
FAX (1st. Floor): (248) 454-6312
FAX (2nd. Floor): (248) 338-2592
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08-23-10	PRELIMINARY SITE PLAN SUBMITTAL
DATE	ADDITIONS AND/OR REVISIONS
DESIGNED	JMG
DRAWN	JMG
CHECKED	LRA
APPROVED	

CITIES OF
TROY & BIRMINGHAM
**INTERMODAL
TRANSIT FACILITY**

PRELIMINARY
FLOOR PLAN

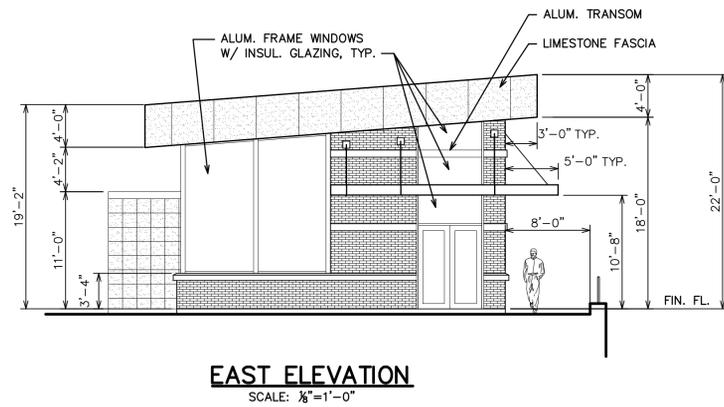
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DATE AUGUST 2009	SHEET NO. A-2 OF

8/27/2010 11:39 AM

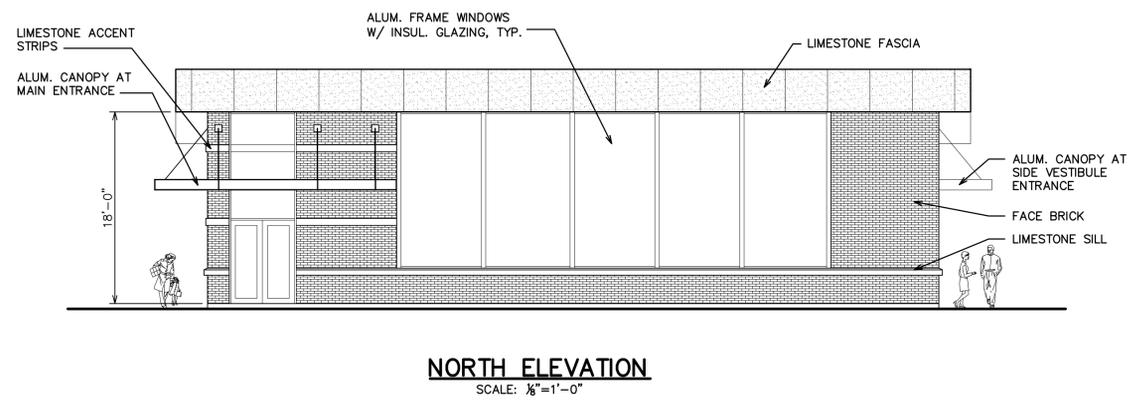
HRC_OIW_CH.CTB

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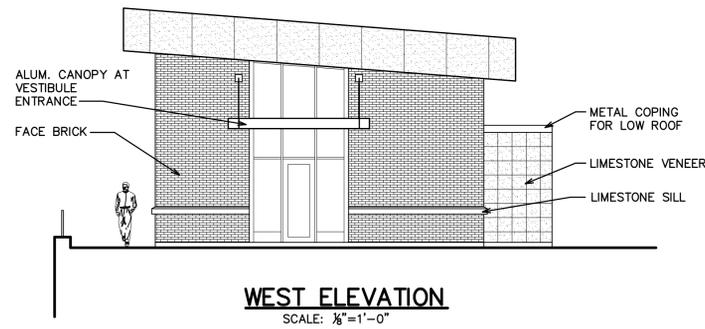
Cochran Jane



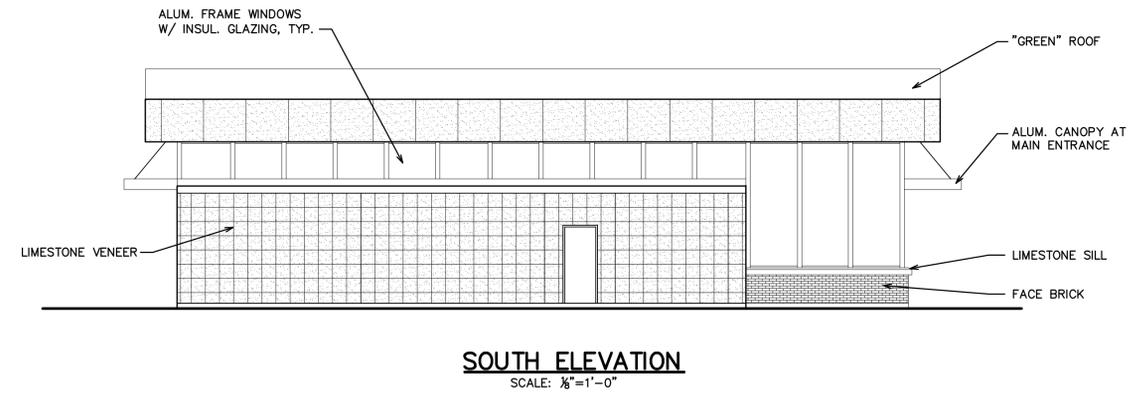
EAST ELEVATION
SCALE: 1/8"=1'-0"



NORTH ELEVATION
SCALE: 1/8"=1'-0"



WEST ELEVATION
SCALE: 1/8"=1'-0"



SOUTH ELEVATION
SCALE: 1/8"=1'-0"



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555 HULET DRIVE
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PHONE: (248) 454-6300
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8-23-10	PRELIMINARY SITE PLAN SUBMITTAL
DATE	ADDITIONS AND/OR REVISIONS
DESIGNED	JMG
DRAWN	JMG
CHECKED	LRA
APPROVED	

CITIES OF
TROY & BIRMINGHAM
**INTERMODAL
TRANSIT FACILITY**

PRELIMINARY
BUILDING ELEVATIONS

HRC JOB NO. 20090078	SCALE AS NOTED
DATE AUGUST 2009	SHEET NO. A-6 OF



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Urban Retail Design
Landscape Architecture
Traditional Town Planning
39 South Main Street Suite 22
Clarkston, Michigan 48346
248.922.0415 fax
248.922.0799

Carlisle/Wortman Associates, Inc.

605 South Main St
Suite 1
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tel: 734-962-2200
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Project:
Intermodal Transit Facility

Cities of Troy and Birmingham, MI

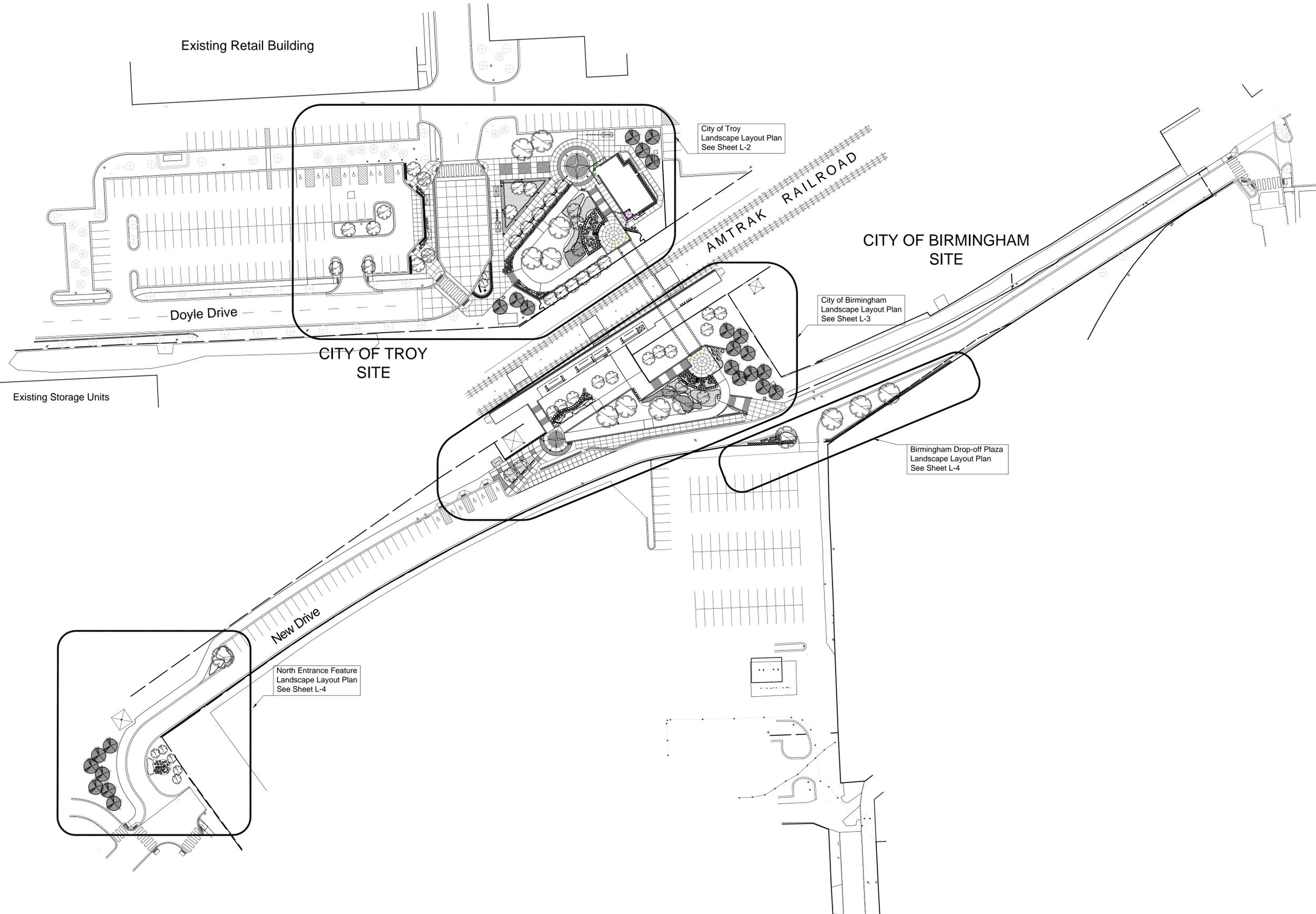


Title:
OVERALL LANDSCAPE LAYOUT

Issues / Revisions	
Review	01-05-10
Site Plan Submission	01-11-10
Site Plan Submission	06-30-10
Preliminary Site Plan Submittal	07-09-10
Preliminary Site Plan Submittal	08-23-10

Drawn by:
ADS
Checked By:
JTE
Date:
January 5, 2010
Scale:
As Noted
Sheet

L-01





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Clarkston, Michigan 48346
248.922.0415 fax
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605 South Main St
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Project: Intermodal Transit Facility

Cities of Troy and Birmingham, MI



Title: CITY OF TROY LANDSCAPE LAYOUT

Issues / Revisions

Review	01-05-10
Site Plan Submission	01-11-10
Site Plan Submission	06-30-10
Preliminary Site Plan Submittal	07-09-10
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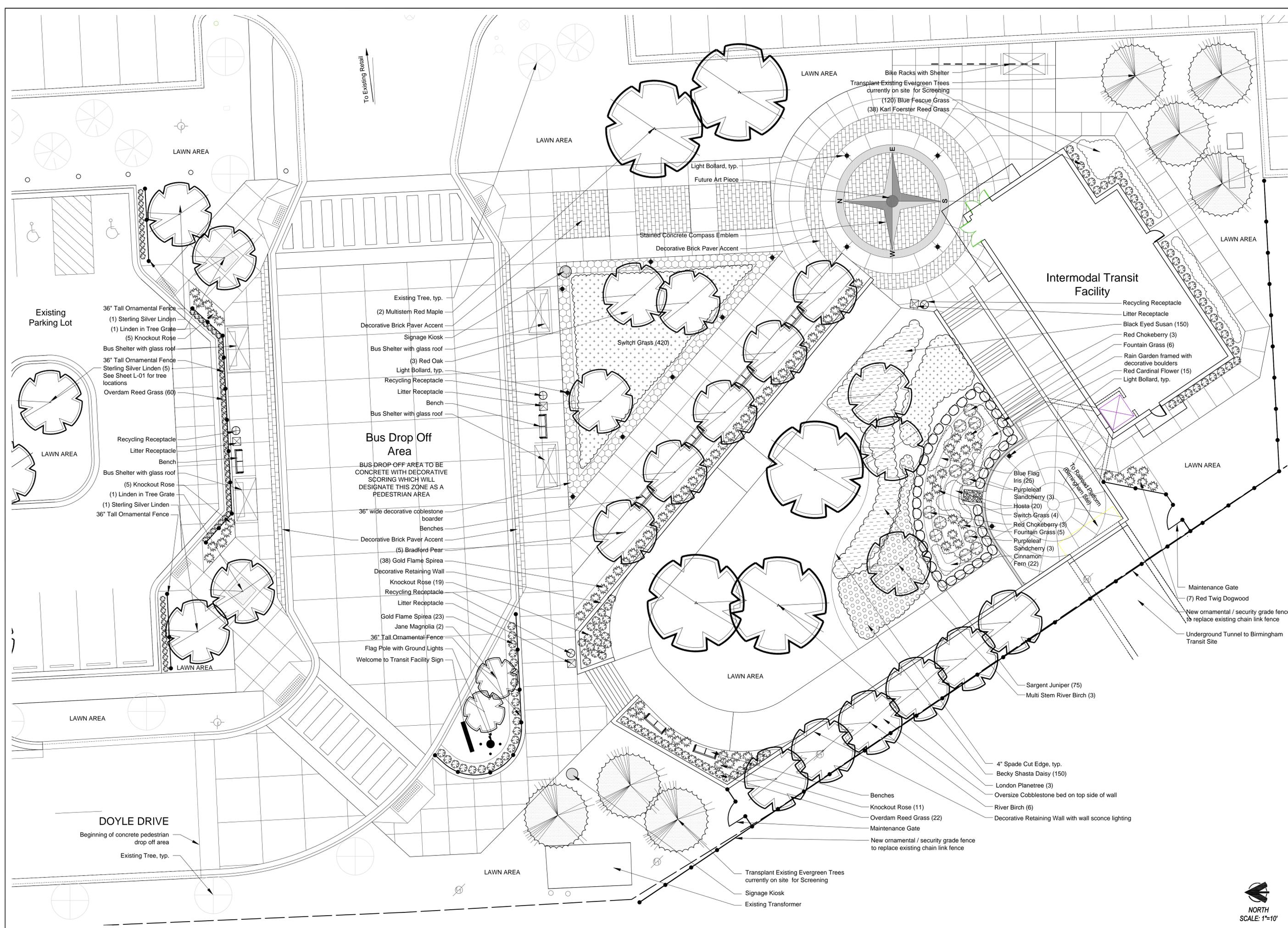
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Scale:
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Sheet:
L-02



- Existing Parking Lot**
- 36" Tall Ornamental Fence
 - (1) Sterling Silver Linden
 - (1) Linden in Tree Grate
 - (5) Knockout Rose
 - Bus Shelter with glass roof
 - 36" Tall Ornamental Fence
 - Sterling Silver Linden (5)
 - See Sheet L-01 for tree locations
 - Overdam Reed Grass (60)
 - Recycling Receptacle
 - Litter Receptacle
 - Bench
 - Bus Shelter with glass roof
 - (5) Knockout Rose
 - (1) Linden in Tree Grate
 - (1) Sterling Silver Linden
 - 36" Tall Ornamental Fence

- Bus Drop Off Area**
- Existing Tree, typ.
 - (2) Multistem Red Maple
 - Decorative Brick Paver Accent
 - Signage Kiosk
 - Bus Shelter with glass roof
 - (3) Red Oak
 - Light Bollard, typ.
 - Recycling Receptacle
 - Litter Receptacle
 - Bench
 - Bus Shelter with glass roof

- BUS DROP OFF AREA TO BE CONCRETE WITH DECORATIVE SCORING WHICH WILL DESIGNATE THIS ZONE AS A PEDESTRIAN AREA**
- 36" wide decorative cobblestone border
 - Benches
 - Decorative Brick Paver Accent
 - (5) Bradford Pear
 - (38) Gold Flame Spirea
 - Decorative Retaining Wall
 - Knockout Rose (19)
 - Recycling Receptacle
 - Litter Receptacle
 - Gold Flame Spirea (23)
 - Jane Magnolia (2)
 - 36" Tall Ornamental Fence
 - Flag Pole with Ground Lights
 - Welcome to Transit Facility Sign

- LAWN AREA**
- Bike Racks with Shelter
 - Transplant Existing Evergreen Trees currently on site for Screening
 - (120) Blue Fescue Grass
 - (38) Karl Foerster Reed Grass

- LAWN AREA**
- Light Bollard, typ.
 - Future Art Piece
 - Stained Concrete Compass Emblem
 - Decorative Brick Paver Accent
 - Switch Grass (420)

- Intermodal Transit Facility**
- Recycling Receptacle
 - Litter Receptacle
 - Black Eyed Susan (150)
 - Red Chokeberry (3)
 - Fountain Grass (6)
 - Rain Garden framed with decorative boulders
 - Red Cardinal Flower (15)
 - Light Bollard, typ.

- To Railroad Platform (Birmingham Station)**
- Blue Flag Iris (25)
 - Purpleleaf Sandcherry (3)
 - Hosta (20)
 - Switch Grass (4)
 - Red Chokeberry (3)
 - Fountain Grass (5)
 - Purpleleaf Sandcherry (3)
 - Cinnamon Fern (22)

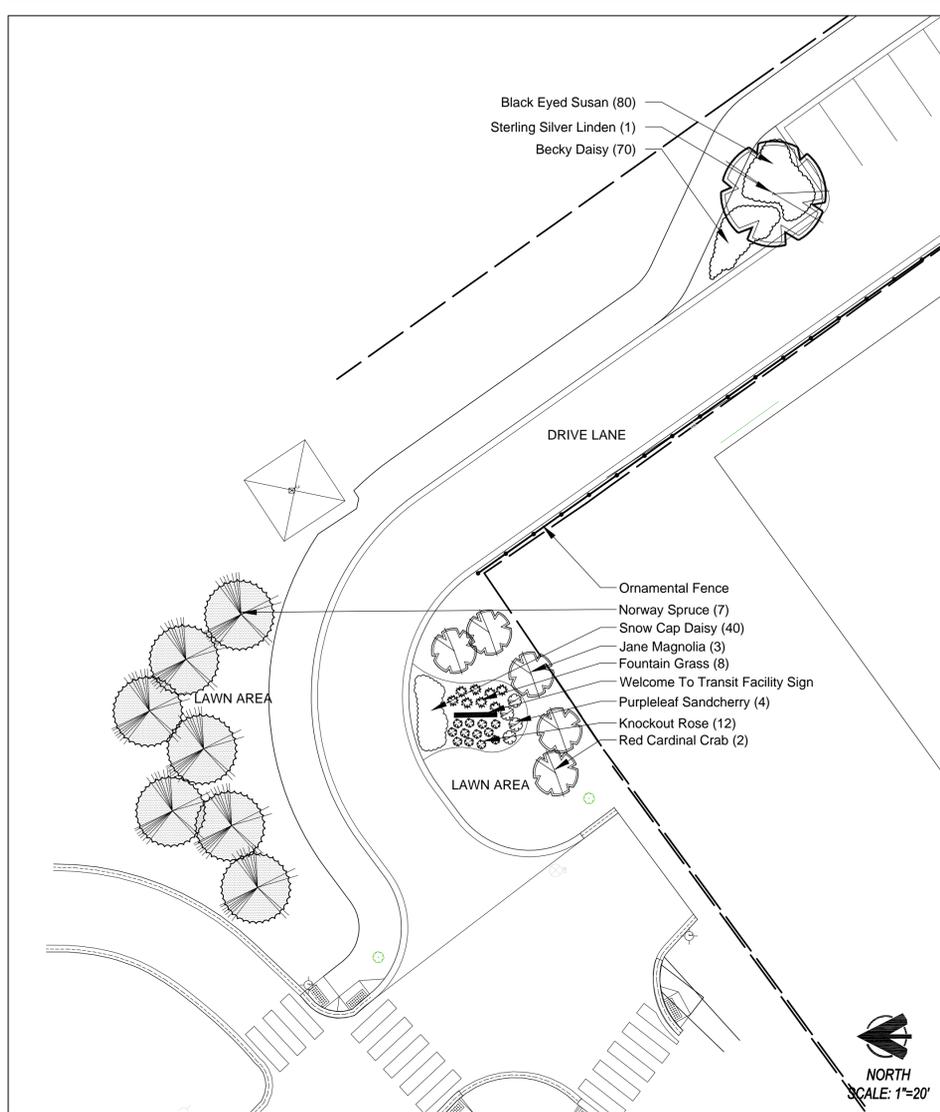
- Sargent Juniper (75)
- Multi Stem River Birch (3)

- 4" Spade Cut Edge, typ.
- Becky Shasta Daisy (150)
- London Planetree (3)
- Oversize Cobblestone bed on top side of wall
- River Birch (6)
- Decorative Retaining Wall with wall sconce lighting
- Benches
- Knockout Rose (11)
- Overdam Reed Grass (22)
- Maintenance Gate
- New ornamental / security grade fence to replace existing chain link fence

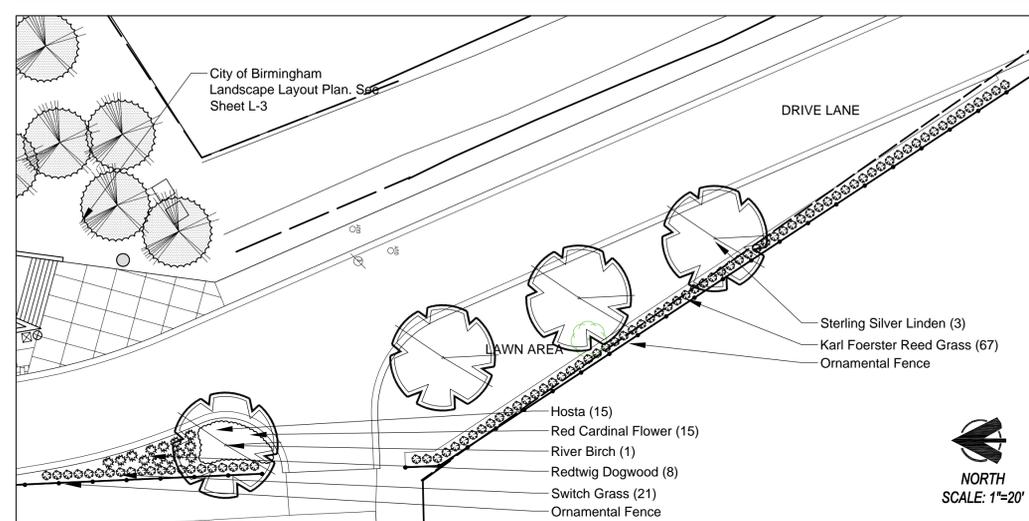
- Transplant Existing Evergreen Trees currently on site for Screening
- Signage Kiosk
- Existing Transformer

- DOYLE DRIVE**
- Beginning of concrete pedestrian drop off area
 - Existing Tree, typ.

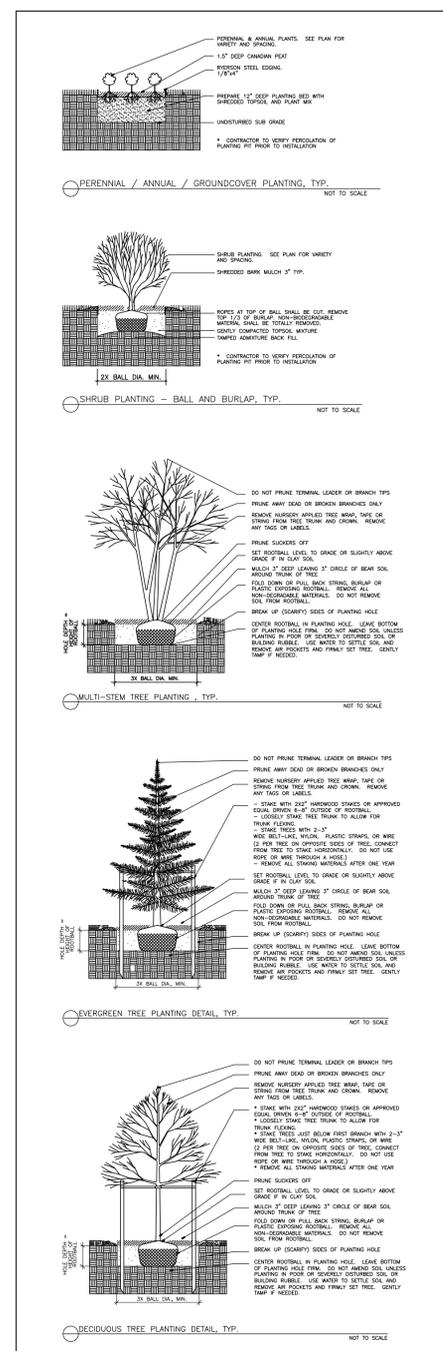
North Entrance Feature (Birmingham)



Birmingham Drop - Off Plaza (Birmingham)



Planting Details: Not to Scale



PLANTING NOTES:

- Contractor shall be responsible for contacting and coordinating with all pertinent utility companies 72 hours in advance of any digging to make themselves familiar with all underground utilities, pipes and structures. Contractor shall take sole responsibility for any cost incurred due to damage of said utilities or structures.
- Contractor shall not willfully proceed with construction as designed when it is obvious that unknown obstructions and/or grade differences exist. Such conditions shall immediately be brought to the attention of the Owner's Representative. The contractor shall assume full responsibility for all necessary revisions due to failure to give such notification.
- Any discrepancies between dimensioned layout and actual field conditions shall be reported to the Owner's Representative. Failure to make such discrepancies known will result in contractor's responsibility and liability for any changes and associated costs.
- Contractor shall be responsible for any coordination with subcontractors as required to accomplish construction installation operations.
- Contractor shall provide and maintain positive surface drainage.
- Contractor shall be responsible for any existing materials that are damaged during construction.
- See Plant & Material List and Planting Details for planting requirements, materials and execution.
- All trees shall have a clay loam or clay root ball. Trees with sand root balls will not be accepted.
- All tree varieties and substitutions to be approved by the Owner prior to being delivered to site. Any plant material delivered to site not previously approved may be rejected and are the sole responsibility of the contractor.
- The location of all plant material shall be scaled from drawings or interpreted from plant list. Final location of all plant material shall be subject to approval from the Owner's Representative.
- The contractor shall "water in" and fertilize all plants immediately after planting.
- HydroSeed all disturbed and new lawn areas as noted on drawings.
- Contractor shall install 3" depth Shredded Hardwood Mulch in all shrub and tree planting beds unless otherwise indicated. Peat Moss is to be installed in all annual flower, perennial flower and ground cover planting beds. Such beds shall have no shredded mulch, typ.
- Contractor shall coordinate lawn repair and planting bed construction in conjunction with the timing of other building construction and improvements.
- The contractor shall guarantee all trees, shrubs, ground cover and other plant materials for one year from the date of installation, including labor and removal and disposal of dead material. Project owner shall be responsible maintenance program which includes replacing all dead or diseased material by the end of the following planting season.
- All plant material shall be nursery grown. All trees and plant material shall meet the current standards of the American Society of Nurseryman.
- All diseased, damaged, or dead material shown on the site plan shall be replaced by the end of the following growing season. Property owner shall be responsible to maintain plant material in healthy condition and/or replace dead plant material for the life of the project.

PLANT LIST:

QTY.	DESCRIPTION	COMMENTS
12	Aronia arbutifolia Red Chokeberry, 5 gal.	Container
13	Betula nigra River Birch, 14'	B&B
127	Calamagrostis acutifolia 'Karl Foerster' Karl Foerster Feather Reed Grass, 1 gal.	Container
82	Calamagrostis acutifolia 'Overdam' Overdam Reed Grass, 1 gal.	Container
25	Geranium x 'Johnson Blue' Johnson Blue Geranium, 1 gal.	Container
2	Ginkgo biloba Ginkgo Tree, 4" cal.	B&B
57	Hosta spp Hosta spp, 1 gal.	Container
10	Hydrangea 'Endless Summer' Endless Summer Hydrangea, 5 gal.	Container
51	Iris 'Blue Flag' Blue Flag Iris, 1 gal.	Container
160	Juniperus chinensis 'Sargent' Sargent Juniper, 3 gal.	Container
320	Leucanthemum superbum 'Becky' Becky Shasta Daisy, 1 gal.	Container
60	Leucanthemum superbum 'Snow Cap' Snow Cap Shasta Daisy, 1 gal.	Container
56	Lobelia cardinalis Red Cardinal Flower, 1 gal.	Container
8	Magnolia x 'Jane' Jane Magnolia, 7-8'	B&B
6	Malus 'Prairiefire' Prairiefire Crab, 2" cal.	B&B
2	Malus 'Red Cardinal' Red Cardinal Crab, 2" cal.	B&B
3	Malus 'Red Jewel' Red Jewel Crab, 2" cal.	B&B
22	Osmunda cinnamomea Cinnamon Fern, 1 gal.	Container
460	Panicum virgatum Switchgrass, 3 gal.	Container
24	Pennisetum alopecuroides Fountain Grass, 1 gal.	Container
7	Picea abies Norway Spruce, 16'	B&B / Spade Truck
7	Platanus x acerifolia London Plane Tree, 3" cal.	B&B
19	Prunus x cistena Purpleleaf Sandcherry, 4-5'	B&B
3	Quercus rubra Red Oak, 3" cal.	B&B
65	Rosa 'Knockout' Knockout Rose, 3 gal.	Container
465	Rudbeckia Black Eyed Susan, 1 gal.	Container
61	Spirea japonica 'Gold Flame' Gold Flame Spirea, 3 gal.	Container
13	Tilia cordata 'Sterling Silver' Sterling Silver Linden, 3" cal.	B&B
120	Festuca ovina glauca Elijah Blue Fescue, 1 gal	Container
2	Acer rubrum Red Maple, 3" cal.	B&B
5	Pyrus calleryana Bradford Pear	B&B
15	Cornus sericea Red Twig Dogwood	Container



J EPPINK PARTNERS INC

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Traditional Town Planning
39 South Main Street Suite 22
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248.922.0415 fax
248.922.0789

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Ann Arbor, MI 48104
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Project:

**Intermodal
Transit Facility**

Cities of Troy and Birmingham, MI

Owner:



Title:
**CITY OF BIRMINGHAM
FEATURE AREAS
LANDSCAPE
LAYOUT / NOTES
& DETAILS**

Issues / Revisions

Review	01-05-10
Site Plan Submission	01-11-10
Site Plan Submission	06-30-10
Preliminary Site Plan Submittal	07-09-10
Preliminary Site Plan Submittal	08-23-10

Drawn by
ADS

Checked by
JTE

Date
January 5, 2010

Scale
As Noted

Sheet

L-04

Intermodal Transit Center

Joint Planning Commission Workshop Meeting July 27, 2010

Consensus Building Exercise – Preliminary Results

(No-italics = written comments ; *italics* = spoken comments; (G) = Troy; (Y) = Birmingham)

Building:

- 1) Energy Efficient building (**CONSENSUS**):
 - A) I would like to see the building to be energy efficient. I'm concerned about the ongoing energy costs. (G)
 - B) A low cost, truly energy efficient building with low maintenance and operating costs. (G)
 - C) Now that the building has been re-situated, do energy costs become greater because of N/S exposure? (Y)
- 2) Building design
 - A) A more traditional design that is expandable, if possible. More walls, less glass if we keep the existing design. (G)
 - B) Prefer a more traditional design, something that will last for 50+ years. (G)
 - C) Building should be driven by information – hence should be highly accessible, lots of glass, LCD screens (in and out) providing the user with real-time data on busses, trains and more; well lit; exciting and inviting. (Y)
 - D) Needs more thought; looks unattractive; change materials; no clock; Troy/B'Ham structures must relate. (Y)
 - E) Building expression: What does it say about our communities? (Y)
 - F) Building design does not achieve: (Y)
 - i) Sensitive to history of area/integrate modern materials and technology
 - ii) Celebrate rail's place in community
 - iii) Integrate historic materials with modern details (bldg. & canopy)
 - G) *Want to see how building relates to surroundings. Show context. What it looks like from Doyle, what it looks like from the train tracks, and at night.*
 - H) *Want to have the design architect justify the design of the building in terms of the program, and the design elements (glass, swooped roof). "Unveil" why the building is the way it is. An "expert" needs to present this information to the PC.*
- 3) Future expansion (**VOTED, BUT NO CONSENSUS – 5 VOTED IN SUPPORT**)
 - A) A more traditional design that is expandable, if possible. (G)
 - B) Building location and the potential for future expansion. (G)
- 4) Green roof
 - A) I like the green roof as designed. (G)
 - B) Are we confident that a green roof will thrive/survive a Michigan winter? Does weight (with rain & snow) become a factor for liability and safety? How does a green roof drain? Is there a reason for no green room on Birmingham side? (Y)
 - C) Green roof? Maybe not. (Y)
 - D) *Want to see LEED checklist.*
 - E) *How were the "sustainable" elements of the building chosen? Requested ROI study.*
- 5) Vertical element (**CONSENSUS**)
 - A) I would like to see a much stronger/bolder vertical component on the bldg. Could be elevator shaft. Integration of the elev. form and vestibule into bldg. form needs to be studied. (G)

- B) Building form & massing: visibility and relationship (height) relative to Kroger. (Supplements idea with graphic, identifying vertical element as “tower, beacon, landmark, wayfinder,” and that it can be seen from the Birmingham side.) (Y)
- 6) Are we making adequate provision to accommodate travelers with luggage i.e. carts & perhaps lockers for those wanting to shop or keep an appointment and leaving their luggage? (Y)

Train Platform/Canopy:

- 1) Canopy to reflect Troy building (**CONSENSUS**):
 - A) Platform must reflect Troy structure; needs to have information too. (Y)
 - B) Is structure closed to elements (within canopy) from north, south & west directions? (Y)
 - C) Platform and building should be cohesive & complimentary. (G)
 - D) Mirror building roof. (G)
- 2) What is basis (rationale) of separate canopies, as opposed to one larger, heated structure on Birmingham platform? (Y)
- 3) How much room is there on Birmingham platform (distance east to west) on which canopies will be constructed? (Y)
- 4) More substantial canopy design vs. bus stop design that looks like it can withstand the elements (**CONSENSUS - 9 VOTED IN SUPPORT**).

Tunnel:

No Comments

Elevator Design:

- 1) B’ham elevator upper level should have some kind of vestibule along west and south sides. (G)
- 2) Elevator design more integrated into building. (G)
- 3) An elevator integrated into the Transit Center building. The chosen route is convenient and allows a sheltered route. The elevator is a must and costs should be reduced on the building to allow for the elevator. (G)

Pedestrian Circulation:

- 1) We seem afraid of ADA compliant ramps and therefore the site design (bldg., landscape, ???) isn’t cohesive. (Y)
- 2) *Where sloped sidewalk meets ½ circle pavement design doesn’t coordinate well.*
- 3) *Ramp design could be re-worked now that elevators are provided. Can reduce the length of the ramps and still be ADA compliant.*
- 4) I feel that the design is appropriate. However, I’m concerned with the cost of the heated sidewalks!! (G)
- 5) Reduce curbs to absolute minimum – use texture and finish to identify use and pedestrian conflict (Lots of curb-less access at airports and stations). (Y)

Plazas:

- 1) The plaza needs to be re-examined to incorporate and create visual connection to building. (G)
- 2) *Plaza to have its own “program” to define the space and arrangement.*

Bicycle Amenities:

No Comments

Bus Amenities:

No Comments

Wayfinding:

- 1) Excellent wayfinding needed. (Y)
- 2) *Wayfinding should be integral part of the design of all elements (vs. sole dependence on signage.)
Example provided: canopy over tunnel with “architectural element” sign on canopy.*

Landscaping:

- 1) Cohesive; big idea; sustainable (low maintenance). (Y)
- 2) Beautiful, but excessive (cost to maintain on both sides?). (Y)
- 3) Keep soft landscaping to a minimum – this is an urban-style transit center and should be designed for “hard” use. (Y)

Lighting:

No Comments

Other:

- 1) Cost
 - A) Cost of project within grants provided. (G)
 - B) This budget is very tight, even allowing for contingencies. \$1.2 million for the station should be reduced to avoid going over budget. (G)
 - C) Let’s move on. The capital costs are being funded by the grants – the costs to the city are O&M costs. (G)
 - D) There should be a list of sustainable components and a corresponding cost to value or ROI study performed. (G)
 - E) *Dumpster location?*
 - F) *Approach to abandoned shopping carts at the Transit Center.*
 - G) *Breakdown of costs so the PC can see how they were arrived at.*



LEED 2009 for New Construction and Major Renovation Project Scorecard

Project Name:
Project Address:

Yes ? No

20 5 1 SUSTAINABLE SITES 26 Points

Y	Prereq	Points	Requirement	Points	Notes
	Prereq 1		Construction Activity Pollution Prevention	Required	
1	Credit 1		Site Selection	1	
5	Credit 2		Development Density and Community Connectivity	5	Connectivity
1	Credit 3		Brownfield Redevelopment	1	Verify
6	Credit 4.1		Alternative Transportation - Public Transportation Access	6	How could it be better
1	Credit 4.2		Alternative Transportation - Bicycle Storage and Changing Rooms	1	Storage only
3	Credit 4.3		Alternative Transportation - Low-Emitting and Fuel-Efficient Vehicles	3	Electric Vehicle Charging
2	Credit 4.4		Alternative Transportation - Parking Capacity	2	
1	Credit 5.1		Site Development - Protect or Restore Habitat	1	Possible on Rain Gardens
1	Credit 5.2		Site Development - Maximize Open Space	1	
1	Credit 6.1		Stormwater Design - Quantity Control	1	Harvesting & Green Roof
1	Credit 6.2		Stormwater Design - Quality Control	1	Rain Garden
1	Credit 7.1		Heat Island Effect - Nonroof	1	
1	Credit 7.2		Heat Island Effect - Roof	1	Green Roof
1	Credit 8		Light Pollution Reduction	1	

Yes ? No

10 WATER EFFICIENCY 10 Points

Y	Prereq	Points	Requirement	Points	Notes
	Prereq 1		Water Use Reduction	Required	
4	Credit 1		Water Efficient Landscaping	2 to 4	
			Reduce by 50%	2	
			No Potable Water Use or Irrigation	4	No irrigation
2	Credit 2		Innovative Wastewater Technologies	2	
4	Credit 3		Water Use Reduction	2 to 4	
			Reduce by 30%	2	
			Reduce by 35%	3	Low flow fixtures & flush with rainwater
			Reduce by 40%	4	

10 2 3

ENERGY & ATMOSPHERE 35 Points

Y	Prereq	Points	Requirement	Points	Notes
	Prereq 1		Fundamental Commissioning of Building Energy Systems	Required	
	Prereq 2		Minimum Energy Performance	Required	
	Prereq 3		Fundamental Refrigerant Management	Required	
5	Credit 1		Optimize Energy Performance	1 to 19	
			Improve by 12% for New Buildings or 8% for Existing Building Renovations	1	
			Improve by 14% for New Buildings or 10% for Existing Building Renovations	2	
			Improve by 16% for New Buildings or 12% for Existing Building Renovations	3	
			Improve by 18% for New Buildings or 14% for Existing Building Renovations	4	
		5	Improve by 20% for New Buildings or 16% for Existing Building Renovations	5	Can we make this or possibly more
			Improve by 22% for New Buildings or 18% for Existing Building Renovations	6	
			Improve by 24% for New Buildings or 20% for Existing Building Renovations	7	
			Improve by 26% for New Buildings or 22% for Existing Building Renovations	8	
			Improve by 28% for New Buildings or 24% for Existing Building Renovations	9	
			Improve by 30% for New Buildings or 26% for Existing Building Renovations	10	
			Improve by 32% for New Buildings or 28% for Existing Building Renovations	11	
			Improve by 34% for New Buildings or 30% for Existing Building Renovations	12	
			Improve by 36% for New Buildings or 32% for Existing Building Renovations	13	
			Improve by 38% for New Buildings or 34% for Existing Building Renovations	14	
			Improve by 40% for New Buildings or 36% for Existing Building Renovations	15	
			Improve by 42% for New Buildings or 38% for Existing Building Renovations	16	
			Improve by 44% for New Buildings or 40% for Existing Building Renovations	17	
			Improve by 46% for New Buildings or 42% for Existing Building Renovations	18	
			Improve by 48%+ for New Buildings or 44%+ for Existing Building Renovations	19	
	Credit 2		On-Site Renewable Energy	1 to 7	
			1% Renewable Energy	1	
			3% Renewable Energy	2	
			5% Renewable Energy	3	
			7% Renewable Energy	4	
			9% Renewable Energy	5	
			11% Renewable Energy	6	
			13% Renewable Energy	7	
	Credit 3		Enhanced Commissioning	2	
2	Credit 4		Enhanced Refrigerant Management	2	Assuming we can do this with the heat pumps
3	Credit 5		Measurement and Verification	3	
2	Credit 6		Green Power	2	



**LEED 2009 for New Construction and Major Renovation
Project Scorecard**

Project Name:
Project Address:

Yes ? No
Yes ? No

4 4 3 MATERIALS & RESOURCES 14 Points

Y	?	No	Prereq	Requirement	Points	Notes
			Prereq 1	Storage and Collection of Recyclables	Required	
			Credit 1.1	Building Reuse - Maintain Existing Walls, Floors and Roof	1 to 3	
				Reuse 55%	1	
				Reuse 75%	2	
				Reuse 95%	3	
			Credit 1.2	Building Reuse - Maintain Interior Nonstructural Elements	1	
			Credit 2	Construction Waste Management	1 to 2	
				50% Recycled or Salvaged	1	
				75% Recycled or Salvaged	2	
			Credit 3	Materials Reuse	1 to 2	
				Reuse 5%	1	
				Reuse 10%	2	
			Credit 4	Recycled Content	1 to 2	
				10% of Content	1	
				20% of Content	2	Maybe 20% with pavement
			Credit 5	Regional Materials	1 to 2	
				10% of Materials	1	
				20% of Materials	2	
			Credit 6	Rapidly Renewable Materials	1	
			Credit 7	Certified Wood	1	

15 INDOOR ENVIRONMENTAL QUALITY 15 Points

Y	?	No	Prereq	Requirement	Points	Notes
			Prereq 1	Minimum Indoor Air Quality Performance	Required	
			Prereq 2	Environmental Tobacco Smoke (ETS) Control	Required	
			Credit 1	Outdoor Air Delivery Monitoring	1	
			Credit 2	Increased Ventilation	1	
			Credit 3.1	Construction Indoor Air Quality Management Plan - During Construction	1	
			Credit 3.2	Construction Indoor Air Quality Management Plan - Before Occupancy	1	
			Credit 4.1	Low-Emitting Materials - Adhesives and Sealants	1	
			Credit 4.2	Low-Emitting Materials - Paints and Coatings	1	
			Credit 4.3	Low-Emitting Materials - Flooring Systems	1	
			Credit 4.4	Low-Emitting Materials - Composite Wood and Agrifiber Products	1	
			Credit 5	Indoor Chemical and Pollutant Source Control	1	
			Credit 6.1	Controllability of Systems - Lighting	1	
			Credit 6.2	Controllability of Systems - Thermal Comfort	1	
			Credit 7.1	Thermal Comfort - Design	1	
			Credit 7.2	Thermal Comfort - Verification	1	
			Credit 8.1	Daylight and Views - Daylight	1	
			Credit 8.2	Daylight and Views - Views	1	

4 INNOVATION IN DESIGN 6 Points

Y	?	No	Prereq	Requirement	Points	Notes
			Credit 1	Innovation in Design	1 to 5	
				Innovation or Exemplary Performance	1	Tunnel Connectivity
				Innovation or Exemplary Performance	1	Transit Center Functionality
				Innovation or Exemplary Performance	1	Negotiated SMART for regional hub
				Innovation	1	
				Innovation	1	
			Credit 2	LEED [®] Accredited Professional	1	

4 REGIONAL PRIORITY 4 Points

Y	?	No	Prereq	Requirement	Points	Notes
			Credit 1	Regional Priority	1 to 4	
				Regionally Defined Credit Achieved	1	SSc2
				Regionally Defined Credit Achieved	1	SSc3
				Regionally Defined Credit Achieved	1	SSc6.1
				Regionally Defined Credit Achieved	1	SSc6.2

67 11 7 PROJECT TOTALS (Certification Estimates) 110 Points

Certified: 40-49 points Silver: 50-59 points Gold: 60-79 points Platinum: 80+ points

Troy/Birmingham Intermodal Transit Facility Summary of Preliminary Construction Cost Estimate

Site Utilities	\$	939,454
Site Paving	\$	826,314
Pedestrian Tunnel and Retaining Walls	\$	1,257,620
Site Landscaping and Ammenities	\$	481,180
Birmingham Access Road and Storm	\$	791,994
Birmingham Water Main	\$	173,960
General Items	\$	264,964
Subtotal Civil & Site Work	\$	4,735,486
Train Platform Structure	\$	196,040
Train Platform Canopy	\$	296,000
Elevator Structures & Mech.Elec/HVAC	\$	605,000
Subtotal Platform/Canopy & Elevator Work	\$	1,097,040
Transit Center Building	\$	1,183,000
Estimated Construction Cost	\$	7,015,526
10% Contingency	\$	701,553
22% Eng. Design and Construction Mgt.	\$	1,543,416
DTE O.H. Relocation - TroySide	\$	76,000
DET O.H. Relocation - Birmingham Side	\$	25,000
ATT O.H. Relocation	\$	50,000
Level 3 F.O. Relocation	\$	25,000
Rogers F.O. Relocation	\$	238,619
Sprint F.O. Relocation	\$	75,000
Subtotal Utility Relocation Costs	\$	489,619
CN Railroad Permitting Costs	\$	10,000
CN Railroad Track Relocation Work	\$	200,000
Subtotal CN Railroad Costs	\$	210,000
Total Construction Cost	\$	8,416,698
Total Eng, Design and Construction Mgmt.	\$	1,543,416
Total Project Cost	\$	9,960,113
Birmingham Property Acquisition	\$	740,000
Troy Property Acquisition	\$	-
Total Project Cost + Property Acquisition	\$	10,700,113
High Speed Rail Award	\$	8,485,212
Earmark	\$	1,300,000
LED Lighting	\$	250,000
Total Grant Funding	\$	10,035,212
Troy Contribution	\$	1,300,000
Birmingham Contribution	\$	300,000
Total City Contributions	\$	1,600,000

Annual Transit Center Operation & Maintenance

	Unit Cost	1000 sq.ft. Building	2500 sq.ft. Building
<u>Operations Costs:</u>			
Utilities (based on proposed green systems)*	\$1.72 to 2.15/sq.ft.	\$1720 to \$2150	\$4300 to \$5375
Personnel (Ambassador or Porter on site)**	\$20.66/ hour	\$60,161.92	\$60,161.92
Total Operational Costs:		\$61,881.92 to \$62,311.92	\$64,461.92 to \$65,536.92
<u>Maintenance Costs:</u>			
External Building	\$0.10 to \$0.15/sq.ft.	\$100 to \$150	\$250 to \$375
Interior Systems	\$1.20 to \$1.50/sq.ft.	\$1200 to \$1500	\$3000 to \$3750
Janitorial & Grounds Maintenance Services***	\$20.00/hour	\$29,120	\$29,120
Unexpected Expenses:		\$5000	\$5000
Total Maintenance Costs:		\$35,420 to \$37,070	\$37,370 to \$38,245
Total Costs:		\$97,301.92 to \$99,381.92	\$101,831.92 to \$103,781.92
Total Costs (Without Ambassador or Porter):		\$37,140 to \$39,220	\$41,670 to \$43,620
Depreciation****	\$15,000/year		

* Building will be constructed to LEED Silver Standard, green systems proposed at this time will result in estimated 14% reduction in utility costs (\$2.00 to \$2.50/sq.ft. standard utility cost)

** Assume Transit Center staffed 8 hours/day, 7 days/week (56 hours/week)

*** Assume staff on site 4 hours/day, 7 days/week (28 hours/week)

****Assumes 50 Year Building Life for Government Facilities (\$750,000 building cost)

Source: Plante Moran Cresa (2009)

Comps – MDOT \$33,060 for Janitorial and Grounds for Rest Area

Carlos - \$1.50 to \$2.00 for utilities, \$20/hour for janitorial and grounds, including supplies

Nat'l Parking - \$20.66 Ambassador rate, including benefits



Troy / Birmingham

Multi-Modal Transit Center

Business Plan

Contributors:

City of Troy

City of Birmingham

Draft
December 9, 2008

EXECUTIVE SUMMARY

Mission Statement

The Birmingham/Troy Multi-Modal Transit Center is intended to improve the attractiveness, reliability, safety and economic efficiency of rail passenger service in the Metro Detroit region. Design of the Transit Center will encourage increased rail travel, expanded multi-modal transit coordination and integration with other transit/para-transit modes. A pedestrian tunnel will connect the loading platform in Birmingham with the Transit Center in Troy. Construction of the Transit Center will provide the impetus for Transit Oriented Development (TOD) in both Birmingham and Troy.

Business Opportunities

Objective 1 – Provide a Transit Center to serve the existing and future Amtrak rail customers. <http://www.amtrak.com/servlet/ContentServer?pagename=Amtrak/HomePage>.

Objective 2 – Provide a transfer point and coordination of regional SMART bus services. <http://www.smartbus.org/smart/home>.

Objective 3 – Provide opportunities for public-private partnerships with traditional taxi service, black sedan service, and auto rental companies.

Objective 4 – Provide opportunities for public/private partnerships with non-traditional car sharing services (e.g. <http://www.zipcar.com/>).

Objective 5 – Provide opportunities for public/private partnerships for shared or rental bicycle services.

Objective 6 – Provide opportunities for mass transit connections for customers of the Oakland Troy “Executive” Airport. <http://www.oakgov.com/aviation/ota/>.

Objective 7 – Provide for a Transit Center that is integrated into the proposed Detroit Regional Mass Transit Initiative.

Objective 8 – Provide the impetus for Transit Oriented Development (TOD) in the Cities of Birmingham and Troy.

Objective 9 – Provide opportunities to integrate with the Detroit Region Aerotropolis (<http://www.detroitregionaerotropolis.com/>).

Objective 10 – Provide economic development opportunities for Birmingham, Troy and the Detroit region.

Financial Projections

On April 16, 2007, Troy City Council approved a contract with Wendel Duhscherer of Amherst, New York, for concept and preliminary engineering design services. On June 30, 2007, the contract was executed and representatives of the Cities of Troy and Birmingham worked with the consultant to develop a series of design plans for the proposed Transit Center. Consultant deliverables to date include the following: Traffic impact statement; Environmental site assessment report; Major considerations and design criteria; and, alternate conceptual plans and cost estimates. The most recent option addresses site improvement and safety issues, while providing a plan that minimizes future operating costs. This conceptual plan identifies three distinct components including a Transit Center building, train loading/unloading platform and pedestrian tunnel. Initial building envelope will encompass 3,000 to 3,500 square feet. Estimated cost of initial phase of the proposed building, platform and tunnel is \$6 million.

Both Cities (Troy \$1.3 million and Birmingham \$300,000) have allocated funding for the Transit Center. It is necessary to secure \$5 million in grants or appropriations to fully fund the design, bid and construction of the Transit Center. During the design and development phase of the project, the floor plan will be determined. This would be designed with consideration for potential public/private partnerships. There is the potential for lease arrangements with public and private transportation providers. This potential will be explored in greater detail as the building design and floor plan are developed. It is the goal of the City of Troy and City of Birmingham to create a Transit Center that does not require operational financial support. Therefore, during the design and development phase, the building's floor plan can be designed to provide for public/private partnerships opportunities. Financial projections will be developed as the scope of the Transit Center is defined.

INDUSTRY ENVIRONMENT

Overview of the Industry

According to *Critical Link 2007*, Amtrak, the Pontiac-Detroit-Chicago Corridor is ranked the ninth busiest Amtrak Corridor in the USA. In fiscal year 2006, this corridor had 439,000 riders over its 281 mile length. The existing Birmingham Amtrak Station is located on this corridor and serves 19,404 riders per year, according to the *Troy Impact Study, Troy Multimodal Transit Center*. This study also projects ridership to increase to 24,765 in 2012; 31,607 in 2017; and 40,340 in 2022. While metro regions throughout the USA are implementing mass transit systems including heavy rail and light rail, the Detroit Metro Regional Mass Transit System is still in its infancy. On December 8, 2008 the "Big 4" – Oakland County, Macomb County, Wayne County and the City of Detroit – voted to support John Hertel's Detroit Regional Mass Transit Initiative. It appears the support is based upon the "Big 4" adopting a resolution that would require the Michigan Legislature to create a regional mass transit authority. Additionally, the resolution requires a public vote on any local subsidy. The vote also allows for continued study of governance possibilities for a regional mass transit system. Finally, the Transit Center is identified in SEMCOG's RTP and MDOT's TIP.

Projected Position for the Future

On a local level, passenger rail is an engine for economic growth. There are numerous studies that indicate that rail stations inject economic growth for retail establishments. Further, it is also shown that both commercial and residential property values increase around rail stations. A study completed by the University of Michigan for the Urban Land Institute entitled the *Troy/Birmingham Transit Center Strategic & Implementation Plan* identifies the market potential for future development, transportation options and complementary land uses near the proposed Transit Center. The study indicates that there is 715,353 square feet (both walkable and drivable) of local retail demand over the next five years. The study goes on to state that, "While the for-sale housing market is struggling, there is sufficient demand for as many as 300 attached residential rental units within the Transit Center District in the next five years." There are over 4,400 properties within a one-mile radius of the proposed Transit Center.

The study indicates a proposed development area located along Maple Road immediately east of the rail line. By locating the proposed Transit Center into this area, there are possibilities of producing a dynamic mix of uses and enhancing the area's potential walkable character. The proposed Transit Center could also act as a catalyst for upgrading the Maple Road Corridor.

At the national level, passenger trains develop transportation options, mobility for underserved populations, congestion mitigation and jobs. Jobs provided include both the railroad industry and the ancillary industries that support rail.

Potential Customers

Amtrak and their riders.
SMART and their riders.
Future Detroit Regional Mass Transit Initiative riders.
Taxi and Black Sedan users and providers.
Car Rental users and providers.
Car Sharing users and providers.
Bicycle Rental or Sharing users and providers.
Air Commuters and providers.
Transit Oriented Development (TOD), residents, business owners and developers.

Direct Competitors

There is currently a modest Amtrak platform and structure located in the existing Transit Center area. The Transit Center would not have any direct competitors because it would be the only multi-modal Transit Oriented Development of its kind in Metro Detroit.

MARKETING

Business Opportunity

As the process of design and development of the Transit Center building progresses, business opportunities will be developed in more detail. It is clear that the Transit Center offers public/private partnerships and provides multi-modal transportation options. These could include lease arrangements or other options that need to be explored in greater detail as the project progresses.

Community Impact

The City of Troy Master Plan, adopted in October 2008, calls for the creation of the Transit Center District. The district envisions “The combination of air, rail, bus and non-motorized transportation in one compact area, supported by a high-density residential development and regional commercial uses. It will work to create a vibrant gateway to the southwest corner of Troy.” It also calls for cooperation with the City of Birmingham to implement the Transit Center District.

Within the City of Birmingham, there is the MX Zoning District which requires the Rail District to be built out in a mixed-use compact urban form.

Community Strategy

The City of Birmingham Planning Board and the City of Troy Planning Commission have met jointly on three occasions. On December 2, 2008, the public bodies jointly adopted the following:

CITY OF BIRMINGHAM / CITY OF TROY JOINT PLANNING STATEMENT OF SUPPORT

The City of Birmingham Planning Board and the City of Troy Planning Commission hereby support the following.

- Designation of the boundaries for the Birmingham/Troy Multi-Modal Transit Center Study Area.
- Joint development of appropriate Transit Oriented Design standards to apply to all or a part of the Birmingham/Troy Multi-Modal Transit Center Study Area.
- Cooperation between the City of Birmingham and the City of Troy on planning issues within all or a part of the Birmingham/Troy Multi-Modal Transit Center Study Area.
- Establishment of a Joint Birmingham/Troy Planning Commission for all or a part of the Birmingham/Troy Multi-Modal Transit Center Study Area, including defining

composition, powers and duties, membership requirements, terms of office, operating procedures, and other related matters.

- Joint planning for the appropriate redevelopment of all or a part of the Birmingham/Troy Multi-Modal Transit Center Study Area.

OPERATIONS

Organizational Structure

The City of Birmingham and the City of Troy will continue to formulate this portion of the Business Plan.

Capital Requirements

Estimated cost of design, bid and construction	\$6,000,000
Contingencies (10%)	\$ 600,000
City of Troy financial commitment	\$1,300,000
City of Birmingham financial commitment	\$ 300,000
Federal/State grant or appropriation required	\$5,000,000

Core Operations

The City of Birmingham and the City of Troy will continue to formulate this portion of the business plan.

FINANCIAL PROJECTIONS

As the Transit Center project progresses, the business opportunities can be explored in greater detail with the multi-modal transportation providers. The Business Plan will be updated to provide the basic financial information required to analyze the growth potential for the Transit Center, capital requirements, revenue projections, etc. In addition, a traditional business plan would include a pro forma 3- to 5-year financial statement, as well as a break-even analysis. This section will need to be expanded upon.

G:\Transit Center\Business Plan\Transit Center Business Plan.docx

July FY10

NEC Spine	Ridership					Ticket Revenue				
	FY10	FY09	Budget	% change vs.		FY10	FY09	Budget	% change vs.	
				FY09	Budget				FY09	Budget
1 - Acela	258,789	239,627	249,205	+8.0	+3.8	\$34,953,246	\$31,104,055	\$33,144,607	+12.4	+5.5
5 - Northeast Regional	629,243	606,907	610,097	+3.7	+3.1	\$40,473,905	\$35,982,770	\$38,092,916	+12.5	+6.3
99 - Special Trains	50	50	400	0.0	-87.5	\$17,700	\$15,900	\$60,000	+11.3	-70.5
Subtotal	888,082	846,584	859,702	+4.9	+3.3	\$75,444,851	\$67,102,725	\$71,297,523	+12.4	+5.8

State Supported and Other Short Distance Corridors

3 - Ethan Allen	5,720	4,891	5,157	+16.9	+10.9	\$289,714	\$239,369	\$247,643	+21.0	+17.0
4 - Vermonter	9,928	7,014	7,836	+41.5	+26.7	\$573,474	\$370,521	\$416,510	+54.8	+37.7
7 - Albany-Niagara Falls-Toronto	45,339	33,033	37,269	+37.3	+21.7	\$2,701,903	\$1,864,677	\$2,156,478	+44.9	+25.3
9 - Downeaster	47,173	47,441	44,803	-0.6	+5.3	\$713,854	\$715,317	\$717,682	-0.2	-0.5
12 - New Haven-Springfield	34,368	29,711	27,944	+15.7	+23.0	\$1,009,119	\$826,774	\$833,946	+22.1	+21.0
14 - Keystone	116,513	105,629	108,697	+10.3	+7.2	\$2,561,918	\$2,102,958	\$2,275,295	+21.8	+12.6
15 - Empire (NYP-ALB)	90,771	82,990	84,798	+9.4	+7.0	\$3,457,468	\$3,231,012	\$3,310,885	+7.0	+4.4
20 - Chicago-St. Louis (Lincoln Service)	62,336	53,827	53,756	+15.8	+16.0	\$1,592,334	\$1,322,442	\$1,406,197	+20.4	+13.2
21 - Hiawatha	77,797	72,721	74,974	+7.0	+3.8	\$1,438,099	\$1,310,680	\$1,394,644	+9.7	+3.1
22 - Wolverine	54,133	44,137	43,035	+22.6	+25.8	\$1,951,460	\$1,590,019	\$1,592,088	+22.7	+22.6
23 - Chicago-Carbondale (Illini/Saluki)	26,119	22,250	22,485	+17.4	+16.2	\$766,968	\$631,191	\$634,141	+21.5	+20.9
24 - Chicago-Quincy (IL Zephyr/Carl Sandburg)	22,921	19,351	19,588	+18.4	+17.0	\$557,870	\$458,591	\$477,578	+21.6	+16.8
29 - Heartland Flyer	12,354	8,662	9,917	+42.6	+24.6	\$283,926	\$186,902	\$230,371	+51.9	+23.2
35 - Pacific Surfliner	263,417	256,410	263,086	+2.7	+0.1	\$5,767,150	\$5,025,728	\$5,686,549	+14.8	+1.4
36 - Cascades	85,973	72,607	86,308	+18.4	-0.4	\$3,101,407	\$2,127,047	\$2,525,826	+45.8	+22.8
37 - Capitol Corridor	141,479	134,746	140,084	+5.0	+1.0	\$2,116,266	\$1,862,058	\$2,131,464	+13.7	-0.7
39 - San Joaquin	98,377	88,505	93,753	+11.2	+4.9	\$3,330,050	\$2,763,515	\$2,968,437	+20.5	+12.2
40 - Adirondack	15,303	12,229	13,806	+25.1	+10.8	\$757,544	\$610,113	\$692,522	+24.2	+9.4
41 - Blue Water	19,475	12,989	12,596	+49.9	+54.6	\$573,678	\$444,336	\$428,605	+29.1	+33.8
46 - Washington-Lynchburg	14,075	-	4,892	-	+187.7	\$844,857	-	\$320,605	-	+163.5
47 - Washington-Newport News	50,337	47,156	53,304	+6.7	-5.6	\$2,847,787	\$2,511,868	\$3,002,463	+13.4	-5.2
54 - Hoosier State	3,523	3,390	3,988	+3.9	-11.7	\$83,830	\$76,684	\$86,783	+9.3	-3.4
56 - Kansas City-St. Louis (MO River Runner)	22,039	17,537	18,290	+25.7	+20.5	\$533,031	\$393,050	\$452,294	+35.6	+17.9
57 - Pennsylvanian	18,978	18,906	19,706	+0.4	-3.7	\$865,232	\$759,065	\$826,811	+14.0	+4.6
65 - Pere Marquette	11,625	10,551	10,142	+10.2	+14.6	\$340,363	\$300,777	\$286,128	+13.2	+19.0
66 - Carolinian	31,138	28,120	26,314	+10.7	+18.3	\$2,058,111	\$1,689,007	\$1,639,000	+21.9	+25.6
67 - Piedmont	12,654	5,509	10,392	+129.7	+21.8	\$208,057	\$89,012	\$182,011	+133.7	+14.3
74-81 - Buses	-	-	-	-	-	\$707,449	\$530,143	\$599,686	+33.4	+18.0
96 - Special Trains	0	1,403	400	-100.0	-100.0	\$0	\$379,261	\$47,000	-100.0	-100.0
Subtotal	1,393,865	1,241,715	1,297,320	+12.3	+7.4	\$42,032,919	\$34,412,119	\$37,569,642	+22.1	+11.9

Long Distance

16 - Silver Star	38,390	36,095	35,222	+6.4	+9.0	\$3,193,806	\$2,741,723	\$2,778,420	+16.5	+15.0
18 - Cardinal	12,117	11,327	10,829	+7.0	+11.9	\$768,690	\$717,777	\$691,301	+7.1	+11.2
19 - Silver Meteor	34,933	31,179	31,059	+12.0	+12.5	\$3,769,107	\$3,059,599	\$3,308,473	+23.2	+13.9
25 - Empire Builder	56,643	56,167	47,496	+0.8	+19.3	\$8,179,256	\$6,978,160	\$7,329,637	+17.2	+11.6
26 - Capitol Ltd.	23,602	21,766	20,835	+8.4	+13.3	\$2,203,713	\$1,899,607	\$1,896,403	+16.0	+16.2
27 - California Zephyr	45,636	33,447	32,598	+36.4	+40.0	\$5,859,546	\$4,313,531	\$4,461,819	+35.8	+31.3
28 - Southwest Chief	42,763	33,236	31,387	+28.7	+36.2	\$5,742,089	\$4,275,387	\$4,559,790	+34.3	+25.9
30 - City of New Orleans	24,885	21,205	20,950	+17.4	+18.8	\$2,108,712	\$1,751,950	\$1,787,402	+20.4	+18.0
32 - Texas Eagle	32,352	28,506	28,125	+13.5	+15.0	\$2,769,551	\$2,272,092	\$2,471,389	+21.9	+12.1
33 - Sunset Ltd.	9,366	8,749	8,652	+7.1	+8.3	\$1,186,191	\$1,032,255	\$1,056,444	+14.9	+12.3
34 - Coast Starlight	47,802	46,884	45,433	+2.0	+5.2	\$4,508,189	\$3,702,520	\$3,813,252	+21.8	+18.2
45 - Lake Shore Ltd.	38,923	33,962	33,741	+14.6	+15.4	\$3,323,978	\$2,696,012	\$2,637,109	+23.3	+26.0
48 - Palmetto	20,685	18,189	15,240	+13.7	+35.7	\$1,906,192	\$1,509,555	\$1,356,353	+26.3	+40.5
52 - Crescent	33,124	28,439	29,159	+16.5	+13.6	\$3,305,940	\$2,655,898	\$2,641,342	+24.5	+25.2
63 - Auto Train	23,644	22,419	23,599	+5.5	+0.2	\$5,282,052	\$4,430,414	\$4,921,640	+19.2	+7.3
Subtotal	484,865	431,570	414,325	+12.3	+17.0	\$54,107,013	\$44,036,480	\$45,710,774	+22.9	+18.4

Amtrak Total	2,766,812	2,519,869	2,571,347	+9.8	+7.6	\$171,584,782	\$145,551,324	\$154,577,939	+17.9	+11.0
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NATIONAL RAILROAD PASSENGER CORPORATION (AMTRAK)
SUMMARIZED FY 2010 MICHIGAN RIDERSHIP AND REVENUE RESULTS*

COMPARATIVE SUMMARY - MONTH OF JULY 2010 VERSUS 2009

MONTH ENDED JULY 31, 2010 VERSUS 2009 *

<u>Corridor/Service</u>	Ridership *				Ticket Revenue (In Dollars) *			
	<u>July</u>	<u>July</u>	<u>Increase/(Decrease)</u>		<u>July</u>	<u>July</u>	<u>Increase/(Decrease)</u>	
	<u>2010</u>	<u>2009</u>	<u>Amount</u>	<u>Percentage</u>	<u>2010</u>	<u>2009</u>	<u>Amount</u>	<u>Percentage</u>
<i>Blue Water</i>	19,475	12,989	6,486	49.9%	\$573,678	\$444,336	\$ 129,342	29.1%
<i>Pere Marquette</i>	11,625	10,551	1,074	10.2%	\$340,363	\$300,777	39,586	13.2%
<i>Wolverine</i>	54,133	44,137	9,996	22.6%	\$1,951,460	\$1,590,019	361,441	22.7%
Totals	85,233	67,677	17,556	25.9%	\$2,865,501	\$2,335,132	530,369	22.7%

TENTH MONTH OF STATE FISCAL YEAR - PERIOD ENDED JULY 31, 2010

<u>Corridor/Service</u>	Ridership *				Ticket Revenue (In Dollars) *			
	<u>Year-To-Date JULY 31,</u>		<u>Increase/(Decrease)</u>		<u>Year-To-Date JULY 31,</u>		<u>Increase/(Decrease)</u>	
	<u>2010</u>	<u>2009</u>	<u>Amount</u>	<u>Percentage</u>	<u>2010</u>	<u>2009</u>	<u>Amount</u>	<u>Percentage</u>
<i>Blue Water</i>	127,942	111,090	16,852	15.2%	\$3,864,611	\$3,383,872	\$ 480,739	14.2%
<i>Pere Marquette</i>	84,288	85,932	(1,644)	-1.9%	\$2,408,056	\$2,333,939	74,117	3.2%
<i>Wolverine</i>	390,572	369,876	20,696	5.6%	\$13,768,054	\$12,398,154	1,369,900	11.0%
Totals	602,802	566,898	35,904	6.3%	\$20,040,721	\$18,115,965	1,924,756	10.6%

* Per July 2010 Ridership and Revenue Report (FY10)