

**PLANNING COMMISSION
MEETING AGENDA
SPECIAL/STUDY MEETING**

500 W. Big Beaver
Troy, MI 48084
(248) 524-3364
www.troymi.gov
planning@troymi.gov

Michael W. Hutson, Chair, and Mark Maxwell, Vice Chair
Donald Edmunds, Tom Krent, Philip Sanzica, Robert M. Schultz
Thomas Strat, John J. Tagle, and Lon M. Ullmann

May 24, 2011

7:30 P.M.

Council Board Room

1. ROLL CALL
2. APPROVAL OF AGENDA
3. MINUTES – May 10, 2011 Regular Meeting
4. PUBLIC COMMENT – For Items Not on the Agenda
5. ZONING BOARD OF ZONING (ZBA) REPORT
6. DOWNTOWN DEVELOPMENT AUTHORITY (DDA) REPORT
7. PLANNING AND ZONING REPORT

PRELIMINARY SITE PLAN REVIEW

8. PRELIMINARY SITE PLAN REVIEW (File Number SP 186 A) – Proposed Sunset Plaza CVS Pharmacy Drive-Through, Northeast Corner of Long Lake and Livernois (125 E. Long Lake), Section 10, Currently Zoned Neighborhood Node M District (Controlled by Consent Judgment)

STUDY ITEM

9. POTENTIAL REVISION - PRELIMINARY SITE PLAN APPROVAL (File Number SP 921) – Briggs Park Condominium, East side of Rochester, North side of Lamb, Section 14, Currently Zoned RT (One Family Attached Residential), EP (Environmental Protection) and R-1C (One Family Residential) Districts

OTHER ITEMS

10. PUBLIC COMMENT – Items on Current Agenda
11. PLANNING COMMISSION COMMENT

ADJOURN

NOTICE: People with disabilities needing accommodations for effective participation in this meeting should contact the City Clerk by e-mail at clerk@troymi.gov or by calling (248) 524-3317 at least two working days in advance of the meeting. An attempt will be made to make reasonable accommodations.

The Regular Meeting of the Troy City Planning Commission was called to order by Chair Hutson at 7:30 p.m. on May 10, 2011, in the Council Chamber of the Troy City Hall.

1. ROLL CALL

Present:

Donald Edmunds
Michael W. Hutson
Tom Krent
Mark Maxwell
Robert Schultz
Thomas Strat
John J. Tagle
Lon M. Ullmann

Absent:

Philip Sanzica

Also Present:

R. Brent Savidant, Acting Planning Director
Allan Motzny, Assistant City Attorney
Zachary Branigan, Carlisle/Wortman Associates, Inc.
Kathy L. Czarnecki, Recording Secretary

2. APPROVAL OF AGENDA

Resolution # PC-2011-05-024

Moved by: Schultz
Seconded by: Strat

RESOLVED, To approve the Agenda as prepared.

Yes: All present (8)
Absent: Sanzica

MOTION CARRIED

3. APPROVAL OF MINUTES

Resolution # PC-2011-05-025

Moved by: Edmunds
Seconded by: Schultz

RESOLVED, To approve the minutes of the April 26, 2011 Special/Study meeting as published.

Yes: All present (8)
Absent: Sanzica

MOTION CARRIED

4. PUBLIC COMMENTS – Items not on the Agenda

There was no one present who wished to speak.

PRELIMINARY SITE PLAN REVIEWS

5. PRELIMINARY SITE PLAN REVIEW (File Number SP 343 D) – Proposed Site Improvements, North Side of Big Beaver, West of Crooks (1500 W. Big Beaver), Section 20, Zoned O-1 (Low Rise Office) District

Resolution # PC-2011-05-026

Moved by: Schultz

Seconded by: Strat

WHEREAS, The applicant submitted the Preliminary Site Plan Approval application prior to the adoption of the comprehensively rewritten Zoning Ordinance, therefore the now-repealed Zoning Ordinance provisions shall apply.

THEREFORE BE IT RESOLVED, That Preliminary Site Plan Approval, pursuant to Section 03.40.03 of the Zoning Ordinance, as requested for the proposed site improvements for 1500 W. Big Beaver, located on the north side of Big Beaver and west of Crooks, in Section 20, within the O-1 zoning district, be granted, subject to the following conditions:

1. The establishment of a cross access easement in the northwest corner of the property to the property to the north and all necessary documentation as required by the City.
2. No existing trees shall be removed from within the Big Beaver right of way.
3. If either of these conditions is not agreed to, this item shall be returned to Planning Commission.

Yes: All present (8)

Absent: Sanzica

MOTION CARRIED

6. PRELIMINARY SITE PLAN REVIEW (File Number SP 968) – Proposed Suburban Cadillac Buick, North of Maple, West of Crooks (1810 Maplelawn), Section 29, Zoned M-1 (Light Industrial) District

Resolution # PC-2011-05-027

Moved by: Maxwell

Seconded by: Schultz

WHEREAS, The applicant submitted the Preliminary Site Plan Approval application prior to adoption of the comprehensively rewritten Zoning Ordinance, therefore the now-repealed Zoning Ordinance provisions shall apply.

THEREFORE BE IT RESOLVED, That Preliminary Site Plan Approval, pursuant to Section 03.40.03 of the Zoning Ordinance, as requested for the proposed site improvements for Suburban Cadillac Buick, located north of Maple, west of Crooks, at 1810 Maplelawn, in Section 29, within the M-1 zoning district, be granted, subject to the following conditions:

- 1. A detailed square footage calculation provided to the Planning Consultant.

Yes: All present (8)
Absent: Sanzica

MOTION CARRIED

SPECIAL USE REQUEST

- 7. PUBLIC HEARING – SPECIAL USE REQUEST AND PRELIMINARY SITE PLAN REVIEW (File Number SU 387) – Proposed Maple Plaza Renovations (McDonald’s Restaurant with Drive-Thru Facility), Northwest Corner of Maple and Livernois (72 W. Maple), Section 28, Zoned B-2 (Community Business) District

PUBLIC HEARING OPENED

Louis Marcucci, 93 Forthton, Troy, spoke in opposition.

For the record, there are three written communications on file.

PUBLIC HEARING CLOSED

Resolution # PC-2011-05-028

Moved by: Tagle
Seconded by: Schultz

WHEREAS, The applicant submitted the Preliminary Site Plan Approval application prior to the adoption of the comprehensively rewritten Zoning Ordinance, therefore the now-repealed Zoning Ordinance provisions shall apply.

RESOLVED, The Planning Commission hereby approves a reduction in the number of required parking spaces for the proposed Maple Plaza Renovations, including a McDonald’s Restaurant with Drive-Thru Facility, to 110 when a total of 154 spaces are required on the site based on off-street parking space requirements, as per Article XL. This 44-space reduction is justified through a parking analysis in a report prepared by Carlisle/Wortman Associates, Inc.

BE IT FURTHER RESOLVED, That Special Use Approval and Preliminary Site Plan Approval for the proposed Maple Plaza Renovations, including a McDonald’s Restaurant with Drive-Thru Facility, located at the northwest corner of Maple and Livernois, Section 28, within the B-2 zoning district, be granted, subject to the following conditions:

1. All parking lot lighting shall be shielded to prevent spillage onto adjacent property owners.
2. A note shall be applied to the drawing indicating “eating in vehicles shall be prohibited”.
3. All parking stalls perpendicular to sidewalks shall be reduced to a depth of 17 feet to increase the sidewalk width to 7 feet, and the landscape area adjacent to the south right of way shall be increased by 2 feet and the parking stalls reduced by 17 feet.

Yes: All present (8)
 Absent: Sanzica

MOTION CARRIED

STUDY ITEMS

8. POTENTIAL REZONING AND DEVELOPMENT PROJECT – East Side of Stephenson, North of Fourteen Mile (455 Stephenson), Section 35, Currently Zoned RC (Research Center) District
9. POTENTIAL DEVELOPMENT PROJECT – Potential Tim Horton’s Restaurant, Northwest Corner of Square Lake and Dequindre (43003 Dequindre), Section 1, Currently Zoned NN (Neighborhood Node) District

OTHER BUSINESS

10. PUBLIC COMMENTS – Items on Current Agenda

There was no one present who wished to speak.

11. PLANNING COMMISSION COMMENTS

The Regular Meeting of the Planning Commission adjourned at 9:28 p.m.

Respectfully submitted,

Michael W. Hutson, Chair

Kathy L. Czarnecki, Recording Secretary

DATE: May 18, 2011

TO: Planning Commission

FROM: R. Brent Savidant, Planning Director

SUBJECT: PRELIMINARY SITE PLAN REVIEW (File Number SP 186 A) – Proposed Sunset Plaza CVS Pharmacy Drive-Through, Northeast Corner of Long Lake and Livernois (125 E. Long Lake), Section 10, Currently Zoned Neighborhood Node M District (Controlled by Consent Judgment)

The applicant, NORR, LLC, submitted the above referenced Preliminary Site Plan Review application. The applicant is proposing adding a drive-through window for the existing CVS Pharmacy at the south side of the existing building.

The property is zoned Neighborhood Node “M”; however, it is controlled by a Consent Judgment which allows the property to be used and occupied for those uses permitted in the B-2 zoning district, under the repealed Zoning Ordinance. The B-2 designation allows for drive-through windows for non-restaurant uses provided that the special conditions set forth in Section 21.25.01 are met. Another provision of the Consent Judgment is that the document does not need to be amended for “minor modifications to the site plan...so long as Troy and plaintiffs consent in writing”. The judgment does not define what constitutes a “minor modification”. Given that the abutting property to the east is residential, the addition of a drive-through window is more than just a “minor modification”. Therefore, CVS needs to go through the Preliminary Site Plan Approval process with Planning Commission (for a recommendation), then the Site Plan and Amended Consent Judgment must be approved by City Council.

The attached report prepared by Carlisle/Wortman Associates, Inc. summarizes the application.

Attachments:

1. Maps
2. Report prepared by CWA

cc: Applicant
File/ SP 186 A

G:\SITE PLANS\SP 186 A Sunset Plaza Shopping Plaza Sec 10\SP-186A Sunset Plaza 05 24 11.docx

PRELIMINARY SITE PLAN REVIEW

8. PRELIMINARY SITE PLAN REVIEW (File Number SP 186 A) – Proposed Sunset Plaza CVS Pharmacy Drive-Through, Northeast Corner of Long Lake and Livernois (125 E. Long Lake), Section 10, Currently Zoned Neighborhood Node M District (Controlled by Consent Judgment)

Proposed Resolution # PC-2011-05-

Moved by:

Seconded by:

RESOLVED, That the Planning Commission hereby recommends that Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Sunset Plaza CVS Pharmacy Drive-Through, located on the Northeast Corner of Long Lake and Livernois (125 E. Long Lake), in Section 10, within the Neighborhood Node Form-Based Zoning District, controlled by Consent Judgment be (granted, subject to the following conditions):

_____) or

(denied, for the following reasons: _____) or

(postponed, for the following reasons: _____)

Yes:

No:

Absent:

MOTION CARRIED / DENIED

SUNSET PLAZA, 125 E LONG LAKE

City of Troy Planning Department



Legend

-  I-75
-  Road Centerline
 -  Major Road
 -  Industrial Road
 -  Local Road
-  Ponds and Basins
-  Streams and Creeks
-  Parcels
- Aerial Photos - 2010**
 -  Red:Band_1
 -  Green:Band_2
 -  Blue:Band_3

355 0 178 355Feet

Scale 1: 2,131



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Printed: 5/11/2011

SUNSET PLAZA, 125 E LONG LAKE

City of Troy Planning Department



Legend

- I-75
- Road Centerline
 - Major Road
 - Industrial Road
 - Local Road
- Form Based Zoning (Current)
 - (PUD) Planned Unit Development
 - (CF) Community Facilities District
 - (EP) Environmental Protection District
 - (BB) Big Beaver Road
 - (MRR) Maple Road
 - (NN) Neighborhood Nodes (A-U)
 - (CB) Community Business
 - (GB) General Business
 - (IB) Integrated Industrial Business District
 - (O-1) Office Building District
 - (OM) Office Mixed Use
 - (P) Vehicular Parking District
 - (R-1A) One Family Residential District
 - (R-1B) One Family Residential District
 - (R-1C) One Family Residential District
 - (R-1D) One Family Residential District
 - (R-1E) One Family Residential District
 - (RT) One Family Attached Residential District
 - (MR) Multi-Family Residential
 - (MHP) Manufactured Housing
 - (UR) Urban Residential
 - (R-C) Research Center District
 - (PV) Planned Vehicle Sales
- Ponds and Basins
- Streams and Creeks
- Parcels
- Aerial Photos - 2010
 - Red:Band_1
 - Green:Band_2
 - Blue:Band_3

355 0 178 355Feet

Scale 1: 2,131



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Printed: 5/11/2011



CARLISLE/WORTMAN ASSOCIATES, INC.

Community Planners /Landscape Architects

605 S. Main, Suite 1
Ann Arbor, MI 48104
734-662-2200
fax 734-662-1935

6401 Citation Drive, Suite E
Clarkston, MI 48346
248-625-8480
fax 248-625-8455

MEMORANDUM

TO: Brent Savidant
FROM: Zachary Branigan
DATE: May 20, 2011
RE: CVS Drive-Through, 125 East Long Lake Road

We are in receipt of a submittal for an amendment to a consent judgment to allow the addition of a drive-through window facility at an existing CVS store on Long Lake Road. Given the limited nature of the project, we are providing a more focused review in a memorandum format.

The existing CVS store is part of the Sunset Plaza development, and is located at 125 East Long Lake Road, on the northeast corner of Long Lake Road and Livernois Road. The project was permitted as part of a consent judgment. The current zoning, however, is Neighborhood Node District, Site Type A, Street Type A (Node M). In the Neighborhood Node Form-Based District, drive-through facilities are permitted in Use Group 6 under special use permit when the site is classified as Site Type A, Street Type A. However, given the status of this project as a part of a consent judgment, the Planning Commission is only obligated to make a recommendation to City Council with regard to the project.

We have reviewed the project with regard to circulation, site design, parking, landscaping, screening, and the general provisions of the Ordinance. Given the limited nature of this proposal as an amendment to the existing consent judgment and an addition to an established shopping center, we have not provided comments on the overall existing development, only those issues surrounding the proposed changes. We have the following comments:

Circulation and Stacking

The proposed configuration of the drive-through is awkward. As designed, drivers would be forced to cross the oncoming traffic in the maneuvering lane heading west from the east entrance towards the main parking area. Further, there are two proposed windows (which we understand are for separated drop-off and pick-up functions), both of which exit next to one another and would presumably produce south-turning traffic movements. This double-loaded right turn area may cause conflicts as adjacent cars pull into the southbound traffic lane towards the site exit to Long Lake Road. Further, we are concerned that the only separation from the oncoming traffic is a striped area immediately south of the drive-through lanes. We suggest, at a minimum, that

additional directional striping be added to the maneuvering lanes to further establish the turning movements into the drive-through and that the striped island be replaced with a curbed landscape island to formally separate the maneuvering lanes from the drive-through lanes.

We also suggest that the separated drive-through windows be consolidated into a single window. While we recognize that the CVS preference is for two separate windows for drop-off and pick-up, we also understand that the traffic volume for these windows is exceptionally small and therefore a single window should suffice. We feel that the consolidation of these drive-through lanes into a single lane will alleviate some of our concern with regard to existing vehicles, and with vehicle conflicts entering the lanes.

We are also concerned that the proposed location for the barrier-free spaces may be in conflict with the proposed approach to the drive-through lanes. Vehicles wishing to use these spaces could become trapped by stacked cars using the drive-through or, at a minimum, be a hazard to vehicles attempting to cross the maneuvering lane and enter the drive-through lanes. We suggest the barrier-free spaces be relocated to the west side of the building by redesigning existing conventional parking spaces.

Drive-Through Facility Standards

Section 6.10 establishes a series of requirements for all drive-through facilities. Section 6.10.A states that: "Drive-through facilities shall be designed in a manner which promotes pedestrian and vehicular safety." As noted above, we are concerned that the proposed design does not meet this criterion.

Further, Section 6.10.B states that: "Single-lane drive-throughs may be located at the side of a building. Multiple-lane drive-throughs shall be located in a manner that will be the least visible from a public thoroughfare." As noted above, the proposed facility includes two lanes. These lanes are proposed in a location where they would be most visible to Long Lake Road. For circulation purposes, our recommendation is that the lanes be reduced to a single lane. Compliance with this recommendation would also result in compliance with Section 6.10.B.

Finally, and also as noted above, Section 6.10.C.3, in Table 6.10, requires four stacking spaces per lane, which this proposal as designed does not meet. We are comfortable with fewer spaces, given the confined location, provided that the applicant reduces the number of lanes to a single lane and justifies the proposed number of stacking spaces. That said, if the lanes are consolidated and the barrier-free spaces are relocated as recommended, then sufficient area may exist to meet the minimum four-space standard.

Landscaping and Screening

The new drive-through facility would point at an existing single-family residential area. This area is already adjacent the shopping center, but would now also be subjected to the cars queuing in the drive-through lanes. There is an existing wall at this location; however, we are concerned that the wall does not provide a desirable or sufficient screening alternative given the increased intensity. Table 13.02-B, Landscape Screening Schedule, in Section 13.02.B, requires screening

alternative 3 (established in able 13.02-A and Figure 13.02-C) and/or a wall between any Use Group 6 use and a residential area. Alternative 3 requires one large evergreen tree for every 10 linear feet and one narrow evergreen for every 5 linear feet.

While a wall does exist at this location, walls are intended to be used as a last resort for screening under the new Ordinance landscaping provisions. Section 13.02.B.3 states that: “Where a land use activity creates noise, light, dust or other similar nuisance that cannot be effectively screened by a landscape buffer, a solid opaque wall or fence may be required. Such wall or fence shall be a minimum of six (6) and a maximum of eight (8) feet in height as measured on the side of the proposed wall having the higher grade.” Section 13.02.B.4 goes on to add: “A combination of landscaping and a solid opaque wall or fence may be approved where such a combination provides more effective screening.” We feel the removal of parking spaces along the east property boundary adjacent the drive-through exit to allow for an enhanced screen incorporating the existing wall and the elements of screening alternative 3 would be a significant improvement in this critical area.

Other requirements

An existing water main is located directly beneath the proposed drive-through canopy. The City Engineering Department has stated that the water main must be relocated and provided a new easement, and that the existing easement must be abandoned.

Recommendation

We do not object to the provision of a drive-through window for the existing CVS store. Drive-through pharmacy windows are a new typical element to pharmacy establishments, and have been included on new pharmacy facilities throughout Michigan. We are confident that, given a series of amendments to the site plan, a drive-through could be a compatible addition to this existing site and could help the existing store continue to thrive in Troy. However, the project cannot be approved as currently designed. At a minimum, the following recommendations should be incorporated into the site plan:

1. The drive-through lanes must be consolidated into a single lane to permit a drive-through facility clearly visible from a major thoroughfare (Section 6.10.B)
2. Additional directional striping must be added to the maneuvering lanes to further establish the turning movements into the drive-through.
3. The striped island should be replaced with a curbed landscape island to formally separate the maneuvering lanes from the drive-through.
4. Four stacking spaces must be provided for each drive-through lane proposed (although only one lane is permitted in a location visible from a major thoroughfare, as noted above). Alternatively, we are open to the applicant providing documentation justifying a reduced number of stacking spaces.
5. The proposed barrier-free spaces must be relocated, preferably to the west side of the building, to eliminate potential conflicts with the drive-through.
6. An enhanced screen incorporating the existing wall and the elements of screening alternative 3 should be added to the east property boundary, potentially be removing spaces along the east boundary wall near the drive-through exit.

7. The existing water main must be relocated and provided a new easement, and that the existing easement must be abandoned.

Until these elements are addressed we cannot support the proposed drive-through. We recommend the Planning Commission discuss these issues with the applicant. If the applicant is willing to make changes to address the issues noted herein, the Planning Commission may elect to postpone action on the recommendation to City Council until such time as a revised plan is provided. Alternatively, the Planning Commission may wish to make an affirmative recommendation, conditioned on the applicant revising the plans to address these issues. Or, if the applicant is unwilling to redesign the proposed addition, the Planning Commission may elect to pass a recommendation of denial to City Council.

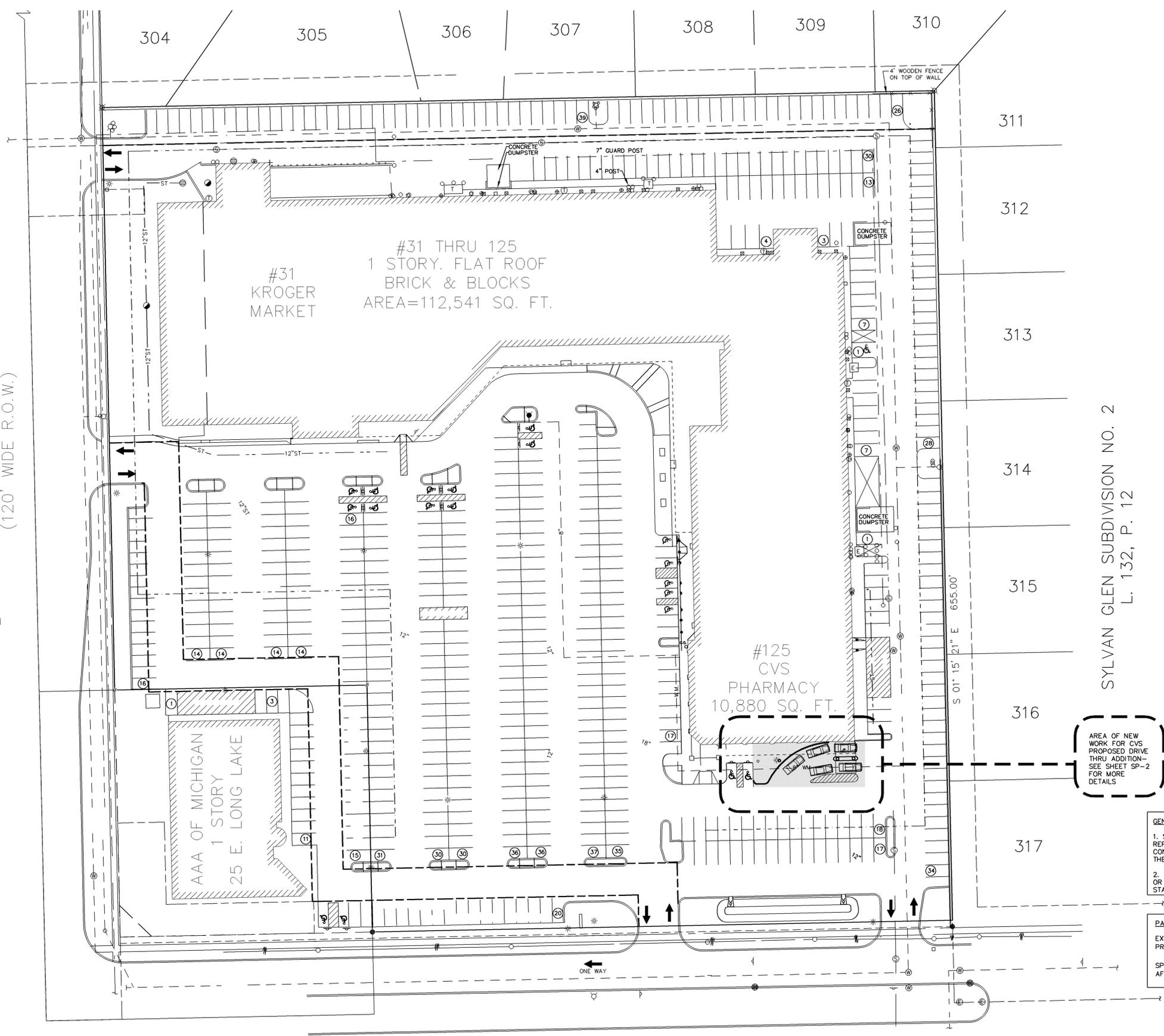
Please do not hesitate to contact us with any questions, comments, or concerns.

Sincerely,



CARLISLE/WORTMAN ASSOCIATES, INC.
Zachary G. Branigan, LEED AP, AICP
Associate

☐ LIVERNOIS ROAD (5 LANES CONC. - PUBLIC)
(120' WIDE R.O.W.)



AREA OF NEW WORK FOR CVS PROPOSED DRIVE THRU ADDITION - SEE SHEET SP-2 FOR MORE DETAILS

GENERAL NOTES:

- SOLID GRAY HATCH ON SITE PLAN REPRESENTS AREA OF CVS EXCLUSIVE CONTROL. THE AREA NOT HATCHED ON THE SITE PLAN IS NOT TO BE ALTERED.
- THIS SITE PLAN IN NO WAY CHANGES OR NEGATES ANY EXCLUSIVE PARKING AS STATED IN THE BASE LEASE

PARKING CALCULATIONS

EXISTING SPACES - PRIOR TO DT ADDITION	633 SPACES
SPACES AFTER DT ADDITION	621 SPACES

1 SITE PLAN
SP-1 SCALE: 1" = 40'-0"

☐ EAST LONG LAKE ROAD
(CONC. PVM'T, 130' R.O.W. - PUBLIC)



STORE NUMBER: 08026
125 EAST LONG LAKE ROAD
TROY, MI 48098

PROJECT TYPE: DT

CS NUMBER: CS54199
CAP. CODE: 102

DEVELOPER:

ARCHITECT OF RECORD:

719 Griswold Street
Suite 1000
Detroit, MI 48226
www.norr.com



CONSULTANT:

SEAL:

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSES UNTIL THE SEAL AND SIGNATURE OF THE RESPONSIBLE REGISTRANT APPEARS ON THE DRAWING, AND PROPER PERMIT FORMS AND RELATED FEES ARE TRANSMITTED BY THE OWNER, OWNER'S AGENT OR CONTRACTOR TO THE AUTHORITY HAVING JURISDICTION.

ISSUES:
03-02-2011 PERMIT SET

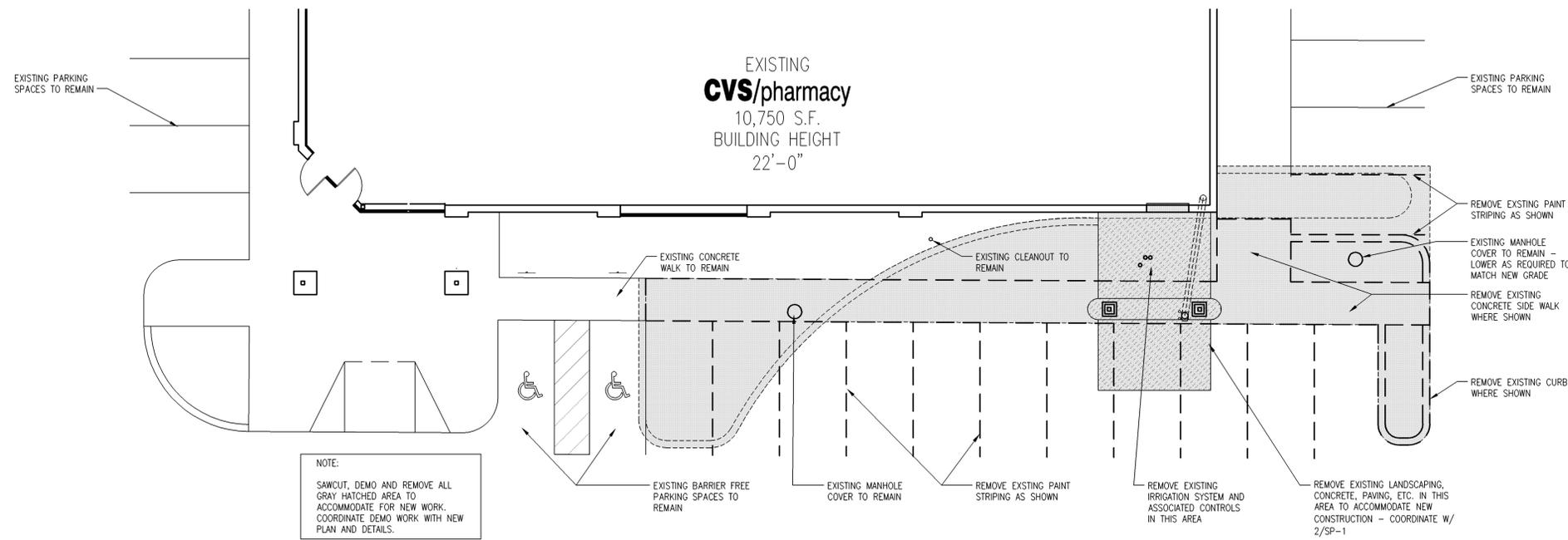
NORR JOB #: JC DT11.0055.00
CVS PROJECT MANAGER: B. BESUDEN
DRAWING BY: I. PETROVIC
DATE: 03-02-2011
JOB NUMBER: CS54199
TITLE:

SITE PLAN

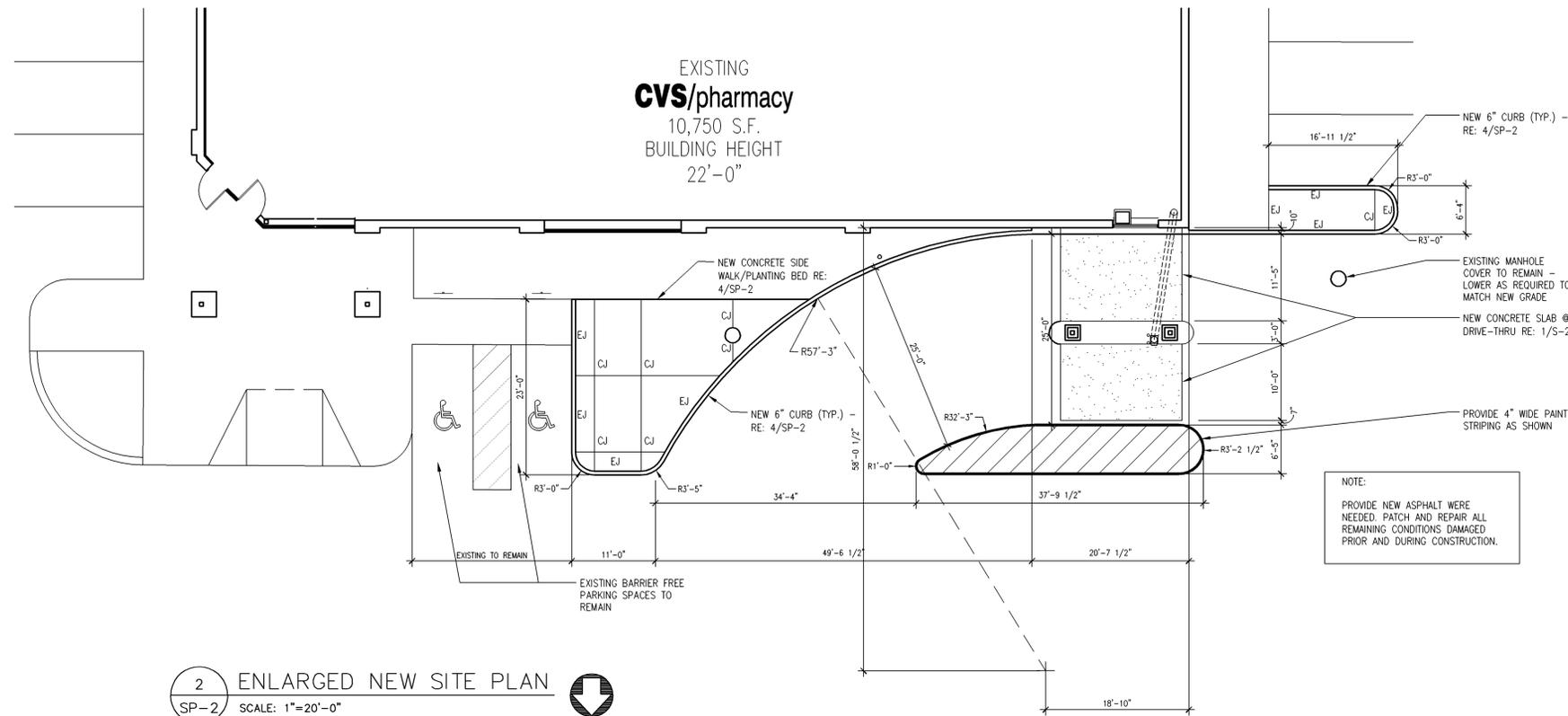
SHEET NUMBER:

SP-1

COMMENTS:



1 ENLARGED DEMO SITE PLAN
SP-2 SCALE: 1"=20'-0"



2 ENLARGED NEW SITE PLAN
SP-2 SCALE: 1"=20'-0"

GENERAL NOTES:

- EXISTING INFORMATION IS BASED ON RECORD DRAWINGS AND FIELD OBSERVATIONS.
- MINIMIZE INTERFERENCE WITH ADJACENT STRUCTURES. MAINTAIN AND PROTECT EGRESS, ADJACENT SURFACES, FINISHES AND ACCESS AT ALL TIMES.
- PROVIDE, ERECT AND MAINTAIN TEMPORARY BARRIERS AND SECURITY DEVICES.
- RESTORE PAVEMENTS AND OTHER SURFACES DISTURBED BY CONTRACT OPERATIONS TO THEIR ORIGINAL CONDITION OR BETTER. CONTRACTOR TO PROTECT ALL ADJACENT FINISHES AND SURFACES.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION'S CURRENT STANDARDS AND SPECIFICATIONS.
- STAGING OF THE WORK SHALL BE DONE IN A MANNER TO ALLOW SAFE AND CONVENIENT ACCESS INTO THE STORE FOR THE CUSTOMERS.

DEMOLITION NOTES:

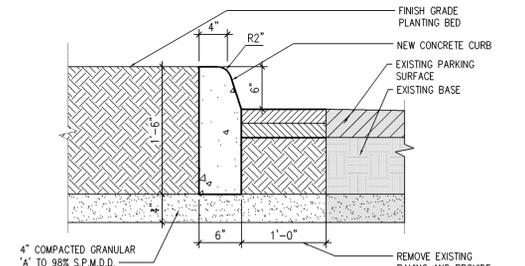
- EXTENT OF DEMOLITION AS INDICATED ON DRAWING.
- ALL DEMOLISHED MATERIALS SHALL BE REMOVED BY THE CONTRACTOR FROM THE SITE AND DISPOSED OF IN A PROPER AND LEGAL MANNER. THE SELECTION OF DUMP SITE AND DISPOSITION OF MATERIAL IS THE RESPONSIBILITY OF THE CONTRACTOR. CVS SHALL RETAIN ALL SALVAGE RIGHTS.

STRIPING NOTES

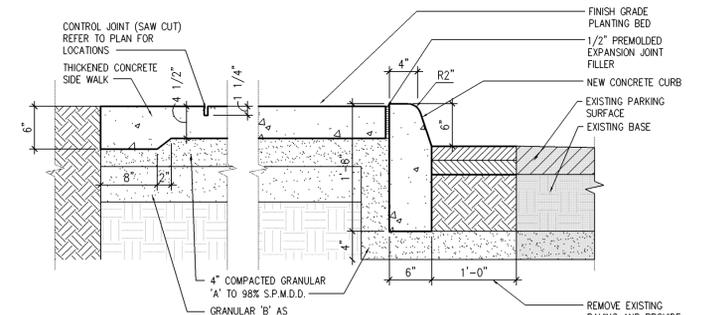
- PAVEMENT-MARKING PAINT: ALKYD-RESIN TYPE, LEAD AND CHROMATE FREE, READY MIXED, COMPLYING WITH FS TT-P-115, TYPE I OR AASHTO M 248, TYPE N.
- COLOR OF STRIPING SHALL MATCH EXISTING SCHEME.
- DO NOT APPLY PAVEMENT-MARKING PAINT UNTIL LAYOUT, COLORS, AND PLACEMENT HAVE BEEN VERIFIED WITH REGISTERED DESIGN PROFESSIONAL. A COLOR SAMPLE AND SKETCH IS TO BE PROVIDED TO DESIGN PROFESSIONAL.
- SWEEP AND CLEAN SURFACE TO ELIMINATE LOOSE MATERIAL AND DUST DAILY.
- APPLY PAINT WITH MECHANICAL EQUIPMENT TO PRODUCE PAVEMENT MARKINGS WITH UNIFORM STRAIGHT EDGES. APPLY AT MANUFACTURER'S RECOMMENDED RATES TO PROVIDE MINIMUM NET FILM THICKNESS OF 15 MILS (0.4 MM)
- 4" WIDE STRIPES IN THE LOADING ZONE SHALL BE 3" ON CENTER.

CIVIL GENERAL NOTES:

- TOPOGRAPHIC INFORMATION: EXISTING INFORMATION IS BASED ON RECORD DRAWINGS AND FIELD OBSERVATIONS.
- EXISTING UTILITIES: INFORMATION HAS BEEN OBTAINED FROM EXISTING AVAILABLE DRAWINGS. VERIFY THE INFORMATION BEFORE CONSTRUCTION. NOTIFY THE OWNER'S REPRESENTATIVE OF DISCREPANCIES OR INTERFERENCES.
- CONSTRUCTION DRAINAGE: USE PUMPS, TEMPORARY DITCHES, SLOPES TO MAINTAIN A WELL DRAINED SITE, FREE OF STANDING WATER AND WATER SOFTENED SOILS.
- LAYOUT: LOCATE DRIVE-THROUGH BY MEASUREMENTS FROM CONNECTING AREAS OF EXISTING BUILDINGS. CONFIRM HORIZONTAL AND VERTICAL CONTROL POINTS PRIOR TO CONSTRUCTION.
- SURFACE RESTORATION: RESTORE PAVEMENTS AND OTHER SURFACES DISTURBED BY CONTRACT OPERATIONS TO THEIR ORIGINAL CONDITION OR BETTER.
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF TROY CURRENT STANDARDS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF TROY ENGINEER AND/OR THE AUTHORITY HAVING JURISDICTION 48 HOURS PRIOR TO THE BEGINNING OF CONSTRUCTION.



3 CURB DETAIL
SP-2 SCALE: 1" = 1'-0"



4 CURB DETAIL
SP-2 SCALE: 1" = 1'-0"



STORE NUMBER: 08026
125 EAST LONG LAKE ROAD
TROY, MI 48098

PROJECT TYPE: DT
CS NUMBER: CS54199
CASE CODE: 102

DEVELOPER:

ARCHITECT OF RECORD:

719 Griswold Street
Suite 1000
Detroit, MI 48226
www.norr.com



CONSULTANT:

SEAL:

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSES UNTIL THE SEAL AND SIGNATURE OF THE RESPONSIBLE REGISTRANT APPEARS ON THE DRAWING, AND PROPER PERMIT FORMS AND RELATED FEES ARE TRANSMITTED BY THE OWNER, OWNER'S AGENT OR CONTRACTOR TO THE AUTHORITY HAVING JURISDICTION.

ISSUES:
03-02-2011 PERMIT SET

NORR JOB #: JCOT11.0055.00
CVS PROJECT MANAGER: B. BESUDEN
DRAWING BY: I. PETROVIC
DATE: 03-02-2011
JOB NUMBER: CS54199
TITLE:

ENLARGED NEW AND DEMO SITE PLAN

SHEET NUMBER:

SP-2

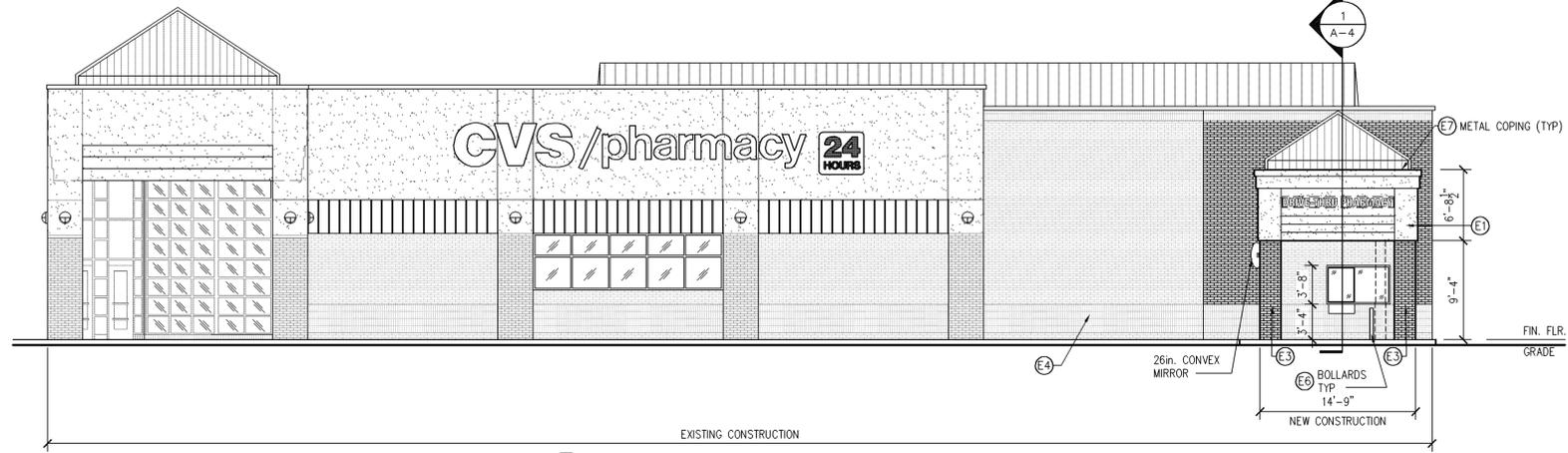
COMMENTS:

EXTERIOR FINISH SCHEDULE					
TAG	MATERIAL / DESCRIPTION	MFR	STYLE / CAT. NO.	COLOR	NOTES
E1	EIFS	STO LOTUSAN SYSTEM	191 STOLUT LOTUSAN 1.5	MATCH EXISTING	INSTALLED PER MANUF. SPECIFICATIONS
E2	NOT USED				
E3	BRICK VENEER			MATCH EXISTING	MORTAR COLOR - MATCH EXISTING
E4	BRICK VENEER (EXISTING)				
E5	CMU BLOCK (EXISTING)		SMOOTH-FACE, SCORED		
E6	PAINT	BENJAMIN MOORE		MATCH OSHA YELLOW	TYP FOR ALL PIPE BOLLARDS
E7	PREFINISHED METAL COPING			MATCH EXISTING	
GLAZING					
G1	BULLET RESISTIVE GLASS			CLEAR	

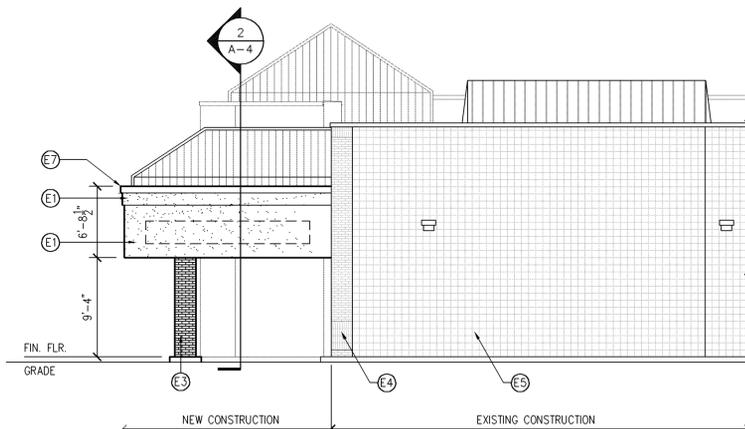
NOTE:
EXTERIOR SIGNAGE UNDER
SEPARATE PERMIT

LEGEND

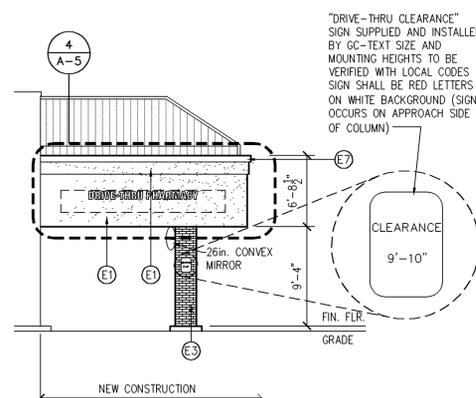
(X) - RE: EXTERIOR FINISH SCHEDULE



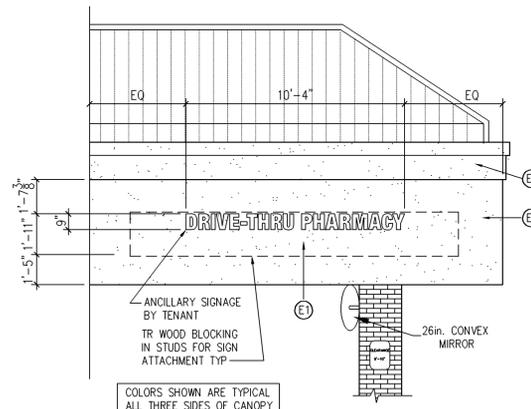
1 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



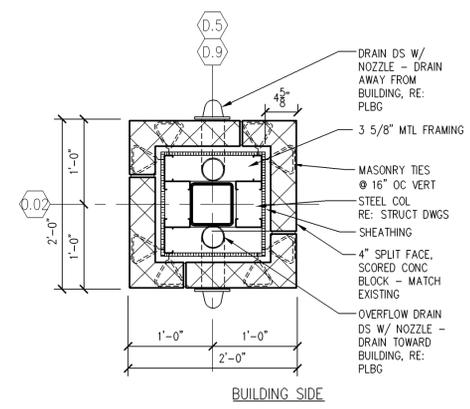
2 PARTIAL WEST ELEVATION
SCALE: 1/8" = 1'-0"



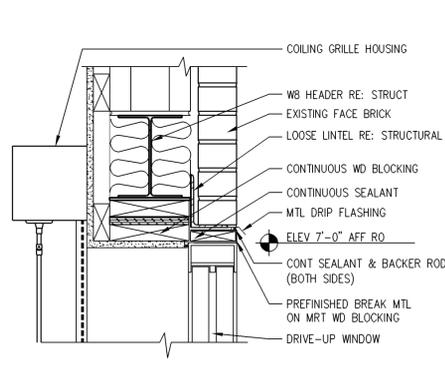
3 PARTIAL EAST ELEVATION
SCALE: 1/8" = 1'-0"



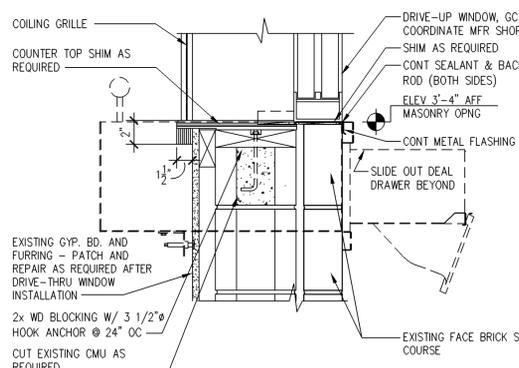
4 ENLARGED SIGN ELEVATION
SCALE: 1/4" = 1'-0"



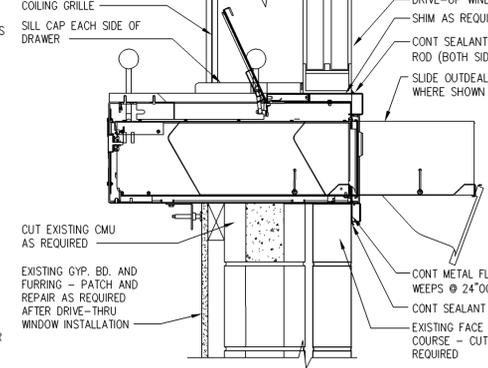
5 COLUMN SHAFT @ DRIVE-THRU
SCALE: 1" = 1'-0"



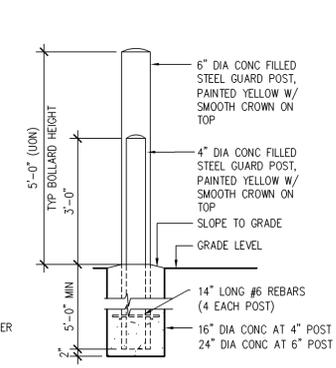
6 HEAD DETAIL
SCALE: 1 1/2" = 1'-0"



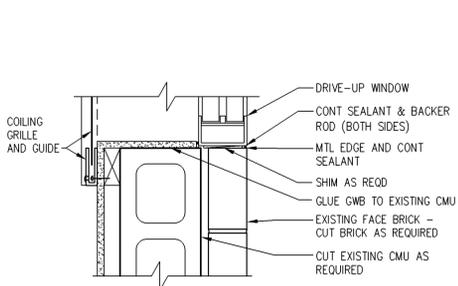
8 SILL DETAIL
SCALE: 1 1/2" = 1'-0"



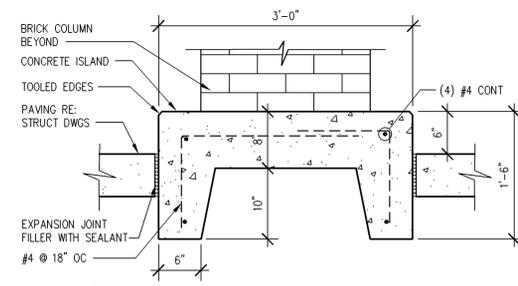
9 SILL DETAIL
SCALE: 1 1/2" = 1'-0"



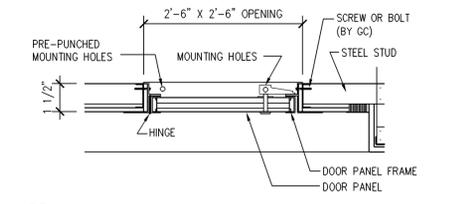
12 BOLLARD DETAIL
SCALE: 1/2" = 1'-0"



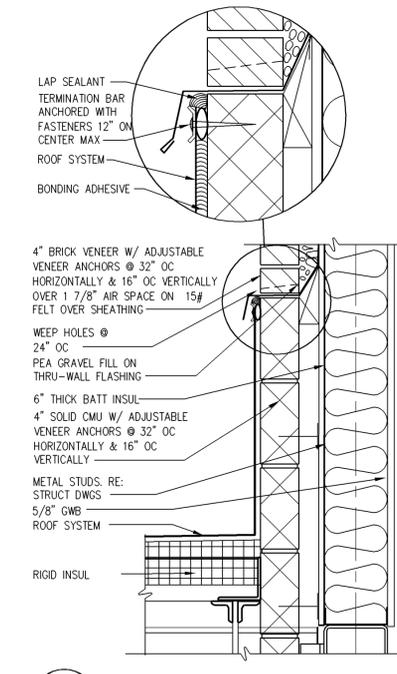
7 JAMB DETAIL
SCALE: 1 1/2" = 1'-0"



10 CONC. ISLAND DETAIL
SCALE: 1" = 1'-0"



11 SECTION THRU ACCESS PANEL
SCALE: 3/4" = 1'-0"



13 ROOF DETAIL
SCALE: 1 1/2" = 1'-0"

CVS
pharmacy

STORE NUMBER: 08026
125 EAST LONG LAKE ROAD
TROY, MI 48098

PROJECT TYPE: DT
CS NUMBER: CS54199
CASE CODE: 102

DEVELOPER:

ARCHITECT OF RECORD:

719 Griswold Street
Suite 1000
Detroit, MI 48226
www.norr.com

NORR
ARCHITECTS ENGINEERS PLANNERS

CONSULTANT:

SEAL:

THIS DRAWING SHALL NOT BE USED FOR CONSTRUCTION PURPOSES UNTIL THE SEAL AND SIGNATURE OF THE RESPONSIBLE REGISTRANT APPEARS ON THE DRAWING, AND PROPER PERMIT FORMS AND RELATED FEES ARE TRANSMITTED BY THE OWNER, OWNER'S AGENT OR CONTRACTOR TO THE AUTHORITY HAVING JURISDICTION.

ISSUES:
03-02-2011 PERMIT SET

NORR JOB #: JCDT11.0055.00
CVS PROJECT MANAGER: B. BESUDEN
DRAWING BY: C. MENARD
DATE: 03-02-2011
JOB NUMBER: CS54199
TITLE:

ELEVATIONS
AND DETAILS

SHEET NUMBER: A-5

COMMENTS:

DATE: May 17, 2011

TO: Planning Commission

FROM: R. Brent Savidant, Planning Director

SUBJECT: POTENTIAL REVISION - PRELIMINARY SITE PLAN APPROVAL (File Number SP 921) – Briggs Park Condominium, East side of Rochester, North side of Lamb, Section 14, Currently Zoned RT (One Family Attached Residential), EP (Environmental Protection) and R-1C (One Family Residential) Districts

Briggs Park Condominium received Preliminary Site Plan Approval from the Planning Commission on December 13, 2005. Final Site Plan Approval was granted administratively on September 28, 2006. The approved layout included 54 units within 16 buildings, comprised of a combination of attached 3-unit and 4-unit buildings. To date, only 12 units have been constructed.

The applicant intends to redesign the site to replace 42 proposed attached units (in 12 buildings) with 35 unattached units. This would result in a decrease of 7 units on the site. The proposed setback of the units from the street and perimeter property lines would not change. The area of the site upon which the residential units are located is zoned RT, which permits detached residential dwellings. The sketch provided appears to comply with RT provisions, although a detailed review has not been conducted at this time.

The Master Plan classifies this area as Rochester Road: Green Corridor.

The applicant believes the detached units will be more marketable than the attached units, and seeks feedback from the Planning Commission on this matter.

Please be prepared to discuss this item at the May 24, 2011 Special/Study meeting.

Attachments:

1. Maps
2. Site information, provided by applicant
3. City of Troy Master Plan (excerpt)

cc: Applicant
File

G:\SITE PLANS\SP 921 Briggs Park Condominiums Sec 14\Potential Revision\Briggs Park PC Memo 05 24 11.docx

Briggs Park Condominium

City of Troy Planning Department



Legend

- I-75
- Road Centerline
 - Major Road
 - Industrial Road
 - Local Road
- Form Based Zoning (Current)
 - (PUD) Planned Unit Development
 - (CF) Community Facilities District
 - (EP) Environmental Protection District
 - (BB) Big Beaver Road
 - (MRR) Maple Road
 - (NN) Neighborhood Nodes (A-U)
 - (CB) Community Business
 - (GB) General Business
 - (IB) Integrated Industrial Business District
 - (O-1) Office Building District
 - (OM) Office Mixed Use
 - (P) Vehicular Parking District
 - (R-1A) One Family Residential District
 - (R-1B) One Family Residential District
 - (R-1C) One Family Residential District
 - (R-1D) One Family Residential District
 - (R-1E) One Family Residential District
 - (RT) One Family Attached Residential District
 - (MR) Multi-Family Residential
 - (MHP) Manufactured Housing
 - (UR) Urban Residential
 - (R-C) Research Center District
 - (PV) Planned Vehicle Sales
- Ponds and Basins
- Streams and Creeks
- Parcels
- Aerial Photos - 2010
 - Red:Band_1
 - Green:Band_2
 - Blue:Band_3

336 0 168 336Feet

Scale 1: 2,019



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.

Printed: 5/17/2011



NOWAK & FRAUS

Consulting Engineers
Land Surveyors
Land Planners

46777 Woodward Avenue
Pontiac, Michigan 48342
Tel. (248) 332-7931
Fax. (248) 332-8257

PROJECT
**Briggs Park
Condominium**

CLIENT
Michigan Home Builders
2617 Beacon Hill Drive
Auburn Hills, MI 48328

PROJECT LOCATION
Part of the SW 1/4 of
Section 14
T. 2 North, R. 11 East
City of Troy,
Oakland County, Michigan

SEAL

DATE/ REVISION

06-20-06 Per City of Troy
07-21-06 Per City of Troy

SHEET
**Paving and Grading
Plan**

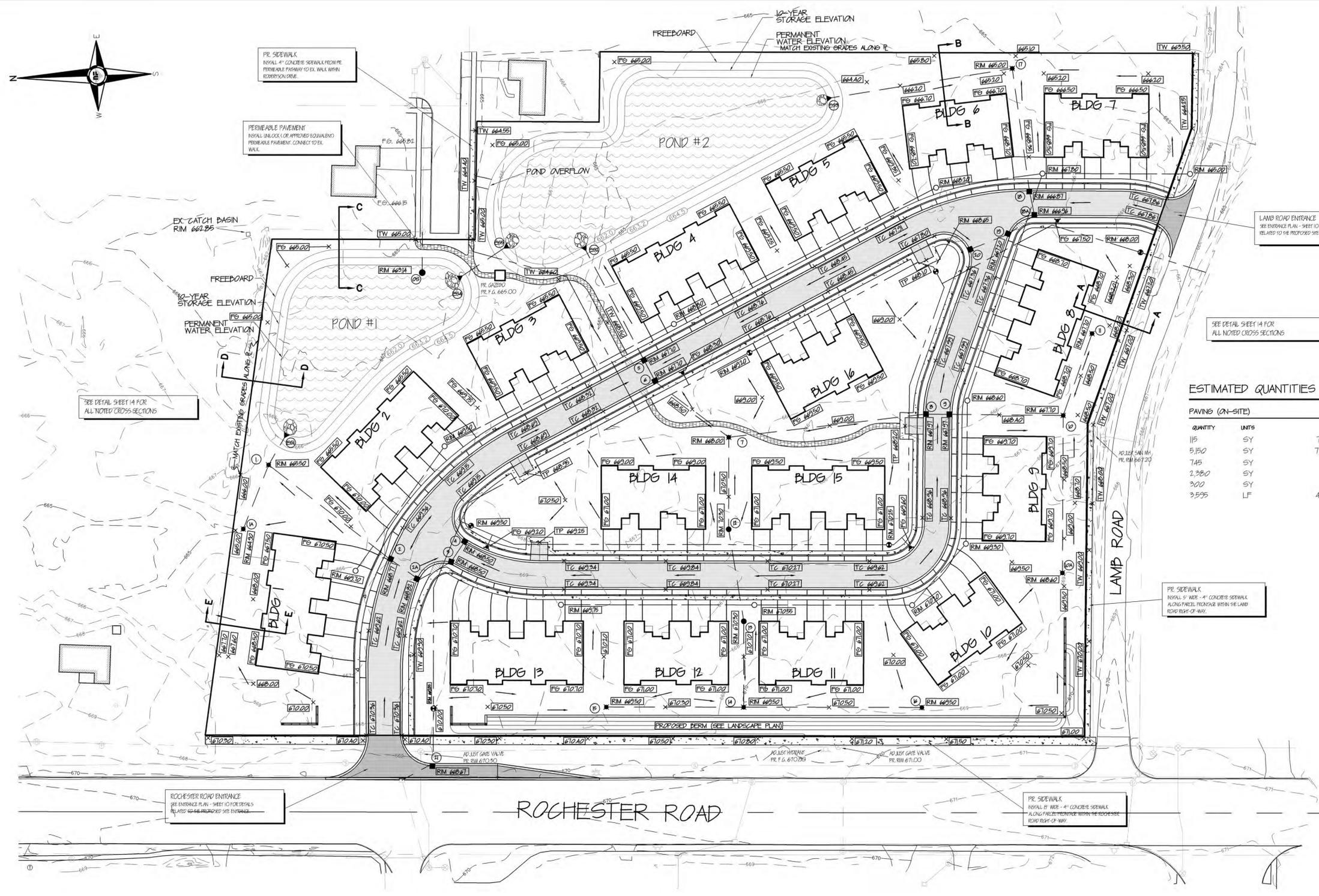
DRAWN BY:
SWS
DESIGNED BY:
R.M. / SWS
APPROVED BY:
R.M.

DATE:
02-16-06

SCALE:
1" = 50'
N/F JOB NO.

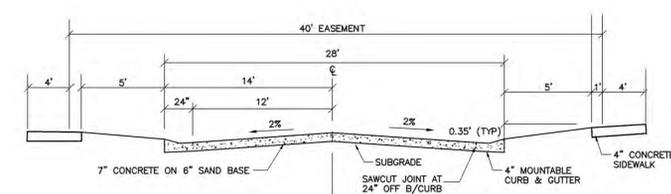
SHEET NO. **D622**

5



ESTIMATED QUANTITIES

PAVING (ON-SITE)		DESCRIPTION
QUANTITY	UNITS	
115	SY	T' CONCRETE SECTION (PARKING)
5,150	SY	T' CONCRETE SECTION (ROADWAY)
745	SY	8" CONCRETE BIKE PATH
2,380	SY	4" CONCRETE SIDEWALK
300	SY	PERMEABLE PATHWAY
3,555	LF	4" MOUNTABLE CURB AND GUTTER



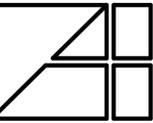
TYPICAL 40' EASEMENT SECTION
CONCRETE PAVEMENT W/4" MONOLITHIC CURB AND GUTTER
N.T.S.

HATCH LEGEND

	PROPOSED CONCRETE SIDEWALK
	PROPOSED T' CONCRETE PAVEMENT
	PROPOSED PERMEABLE PAVEMENT
	PROPOSED 4" CONCRETE PAVEMENT

LEGEND

	PR. TOP OF CURB ELEVATION
	PR. TOP OF WALK ELEVATION
	PR. TOP OF PWMT. ELEVATION
	FINISH GRADE ELEVATION
	DRAINAGE DIRECTION



Interior Design
 2445 Franklin Road
 Bloomfield Hills, MI 48302
 248-334-5000

BUILDING PLANS
 BUILDING #300 (3 UNITS)
 UNITS A/B/D

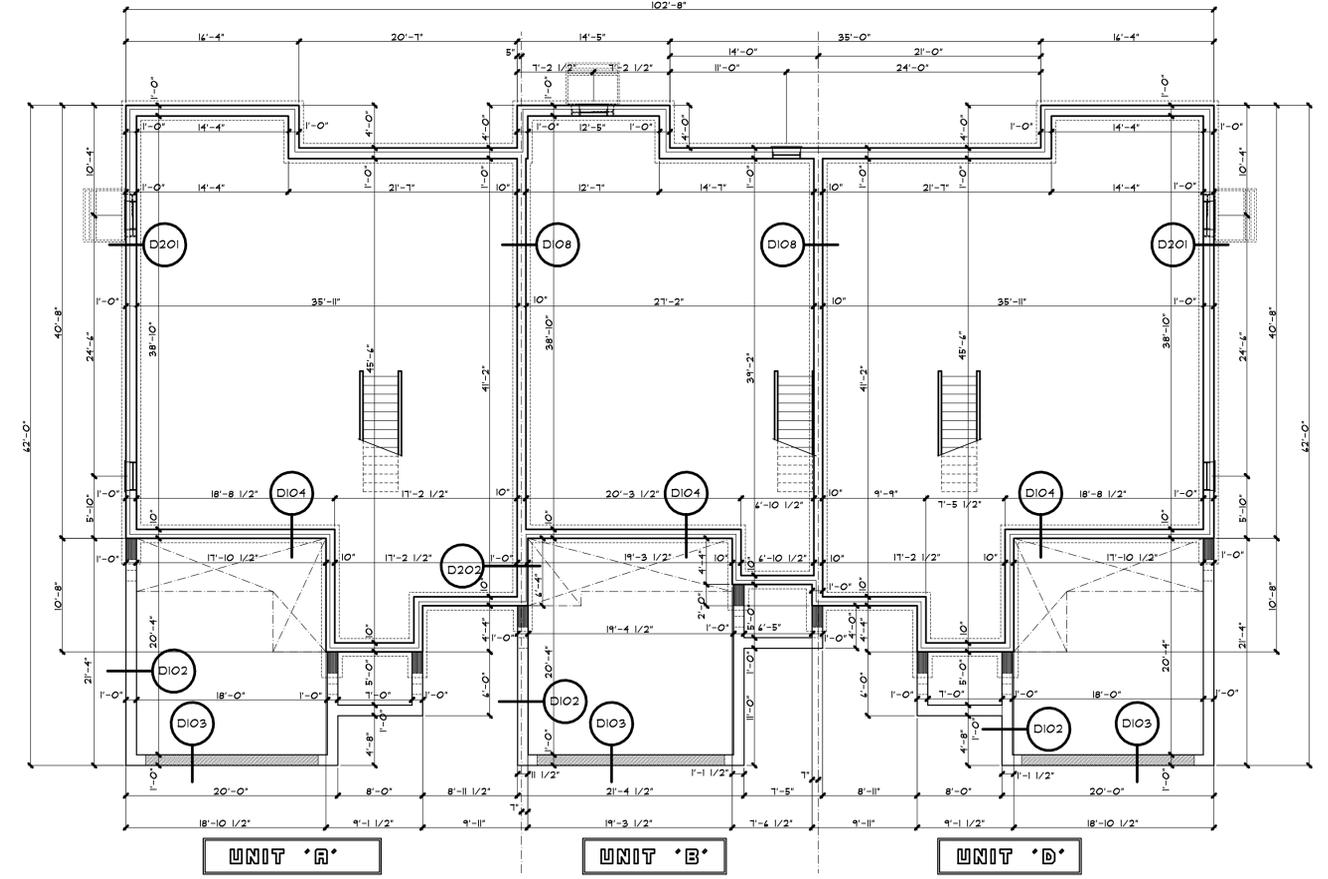
MICHIGAN HOME BUILDERS
 BRIGGS PARK CONDOMINIUM
 TROY, MICHIGAN

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PRELIMINARY 10-03-05
 BIDS 02-06-08
 PERMITS 02-06-08
 CONSTRUCTION 02-06-08
 REVISIONS

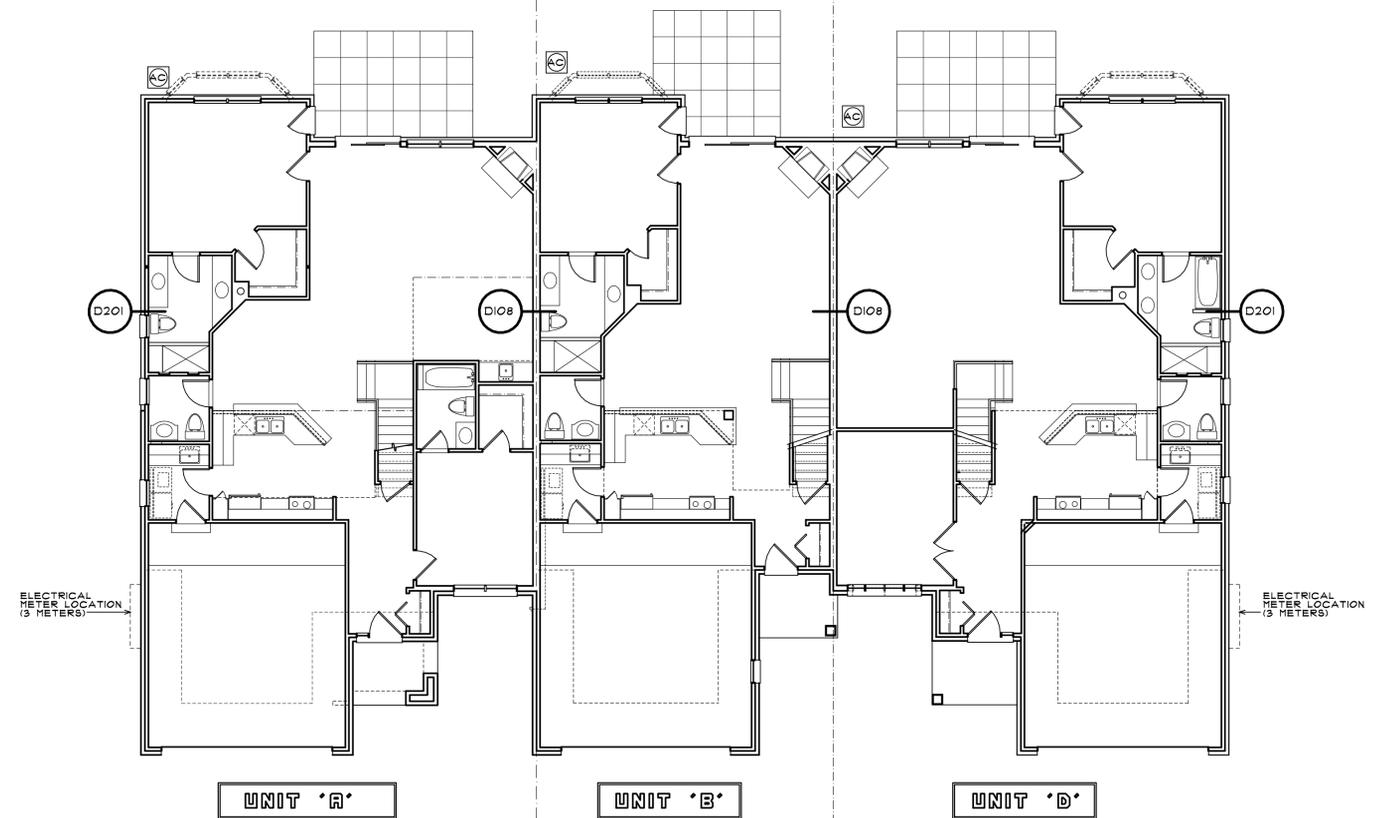
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 CHECKED BY: AM
 JOB NUMBER: 05378
 DATE: 02-06-08
 SHEET NUMBER: 16

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FOUNDATION PLAN

NOTE: SEE 1/4" SCALE UNIT PLANS FOR ADDITIONAL NOTES, DIMENSIONS & STRUCTURAL INFORMATION.
 NOTE: PATIO, DECK, AND BAY ARE SUBJECT TO CIVIL ENGINEER VERIFICATION PERIMETER YARD AND DETENTION AREA.
 NOTE: WATER METER LOCATIONS TO BE DETERMINED BY BUILDER.
 SCALE: 1/8" = 1'-0"

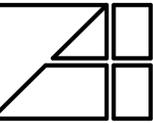


FIRST FLOOR PLAN

NOTE: SEE 1/4" SCALE UNIT PLANS FOR ADDITIONAL NOTES, DIMENSIONS & STRUCTURAL INFORMATION.
 NOTE: PATIO, DECK, AND BAY ARE SUBJECT TO CIVIL ENGINEER VERIFICATION PERIMETER YARD AND DETENTION AREA.
 NOTE: WATER METER LOCATIONS TO BE DETERMINED BY BUILDER.
 SCALE: 1/8" = 1'-0"

ALL INTERIOR PARTITION DIMENSIONS ARE 3/16" UNLESS NOTED OTHERWISE.
 BEFORE CONSTRUCTION OBTAIN WINDOW & DOOR ROUGH OPENING SIZES FROM WINDOW/DOOR SUPPLIER.
 TRUSS DESIGNER/FABRICATOR SEE SHEET T-1 FOR ADDITIONAL NOTES & INFORMATION.
 SEE SHEET N-1 FOR ADDITIONAL NOTES REGARDING DRAWINGS & CONSTRUCTION.
 SEE SHEET N-1 FOR TREATED WOOD & ENGINEERED WOOD PRODUCTS AND ANCHOR ATTACHMENTS.
 DO NOT SCALE THESE DRAWINGS. USE CALCULATED DIMENSIONS. IF VARIATIONS OCCUR CONTACT ARCHITECT FOR CLARIFICATION.

**BUILDING #300
 BRIGGS PARK**



Interior Design
 2445 Franklin Road
 Bloomfield Hills, MI 48302
 248-334-5000

BUILDING PLANS
 BUILDING #300 (3 UNITS)
 UNITS A/B/D

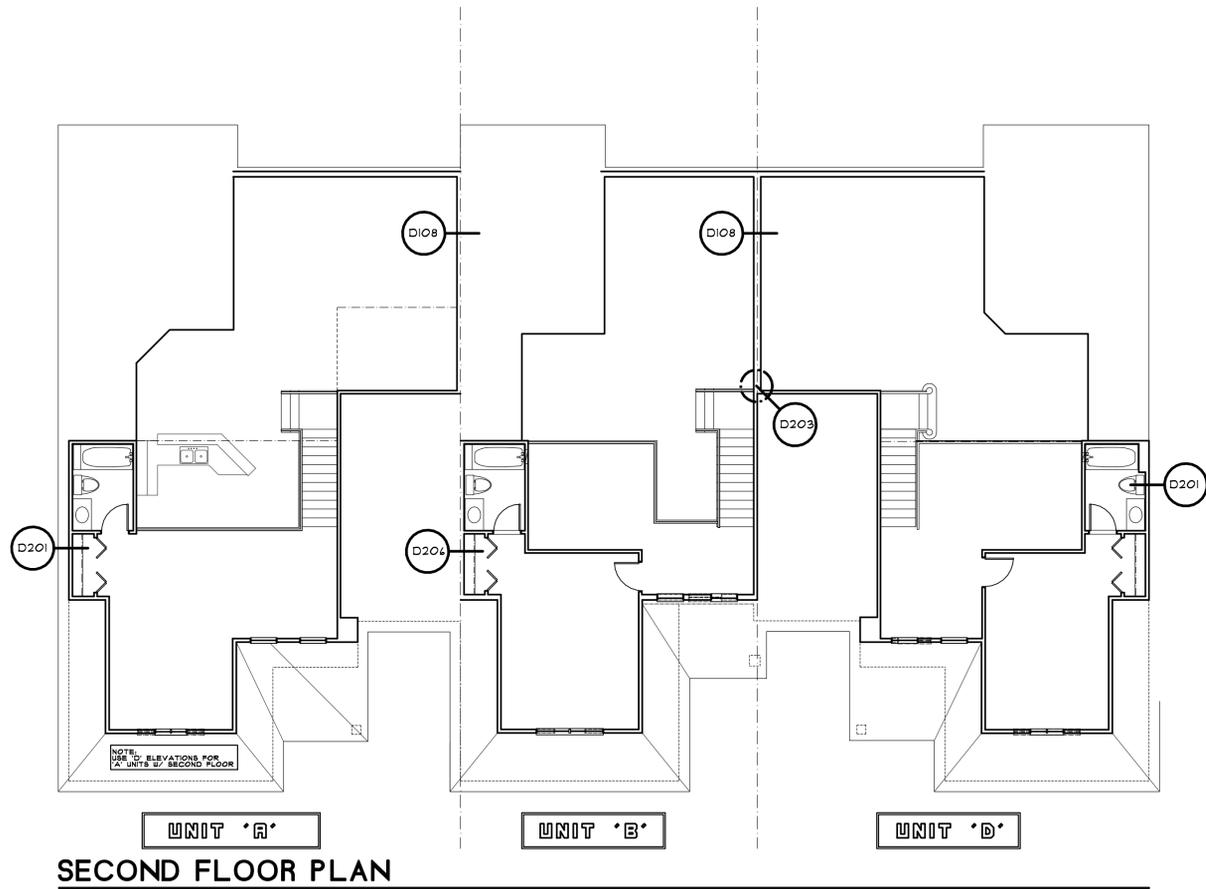
MICHIGAN HOME BUILDERS
 BRIGGS PARK CONDOMINIUM
 TROY, MICHIGAN

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■ PRELIMINARY
 10-03-05
 ■ BIDS
 02-06-08
 ■ PERMITS
 02-06-08
 ■ CONSTRUCTION
 02-06-08

REVISIONS

DRAWN BY
 DZ
 CADD FILE
 BLDG#300IABD.Dwg
 CHECKED BY
 AM
 JOB NUMBER
 05378
 DATE
 02-06-08
 SHEET NUMBER



SECOND FLOOR PLAN

NOTE:
 SEE 1/4" SCALE UNIT
 PLANS FOR ADDITIONAL
 NOTES, DIMENSIONS &
 STRUCTURAL INFORMATION

SCALE: 1/8" = 1'-0"

ALL INTERIOR PARTITION DIMENSIONS ARE 3/16" UNLESS NOTED OTHERWISE.

BEFORE CONSTRUCTION OBTAIN WINDOW & DOOR ROUGH OPENING SIZES FROM WINDOW/DOOR SUPPLIER.

TRUSS DESIGNER/FABRICATOR SEE SHEET T-1 FOR ADDITIONAL NOTES & INFORMATION

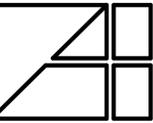
SEE SHEET N-1 FOR ADDITIONAL NOTES REGARDING DRAWINGS & CONSTRUCTION.

SEE SHEET N-1 FOR TREATED WOOD & ENGINEERED WOOD PRODUCTS AND ANCHOR ATTACHMENTS

DO NOT SCALE THESE DRAWINGS. USE CALCULATED DIMENSIONS. IF VARIATIONS OCCUR CONTACT ARCHITECT FOR CLARIFICATION.

BUILDING #300
BRIGGS PARK

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SHEET TITLE
 BUILDING ELEVATIONS
 BUILDING #300 (3 UNITS)
 UNITS A/B/D

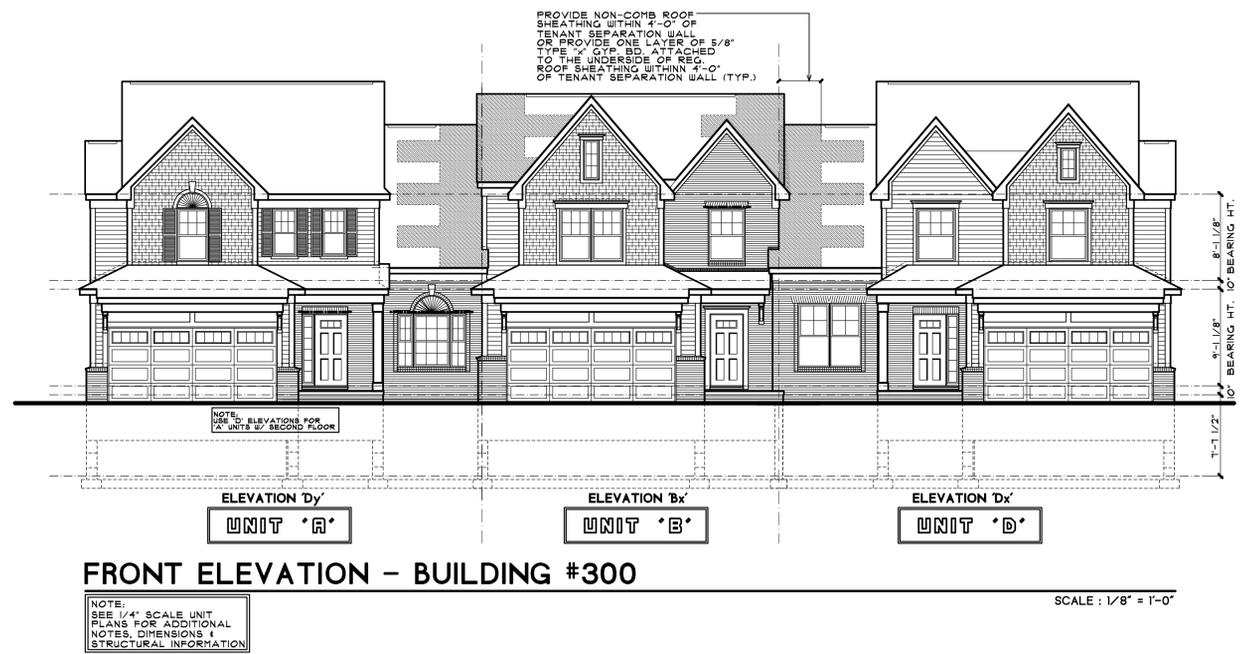
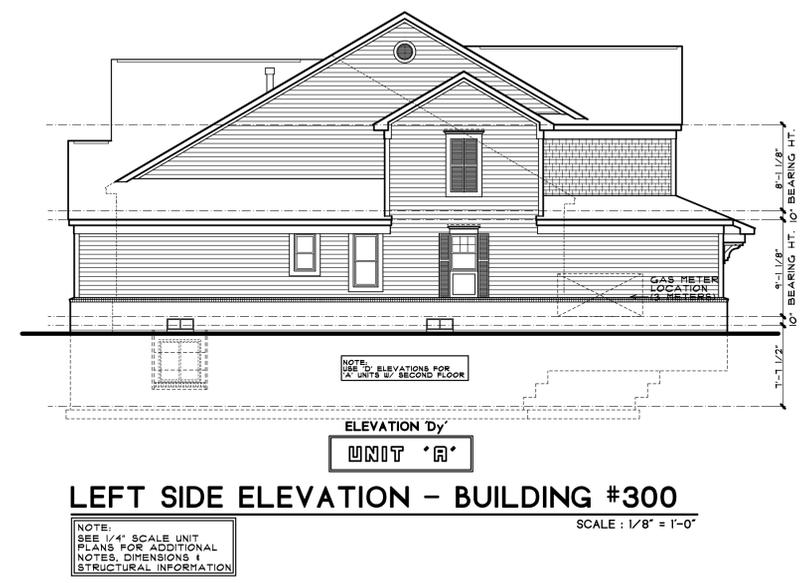
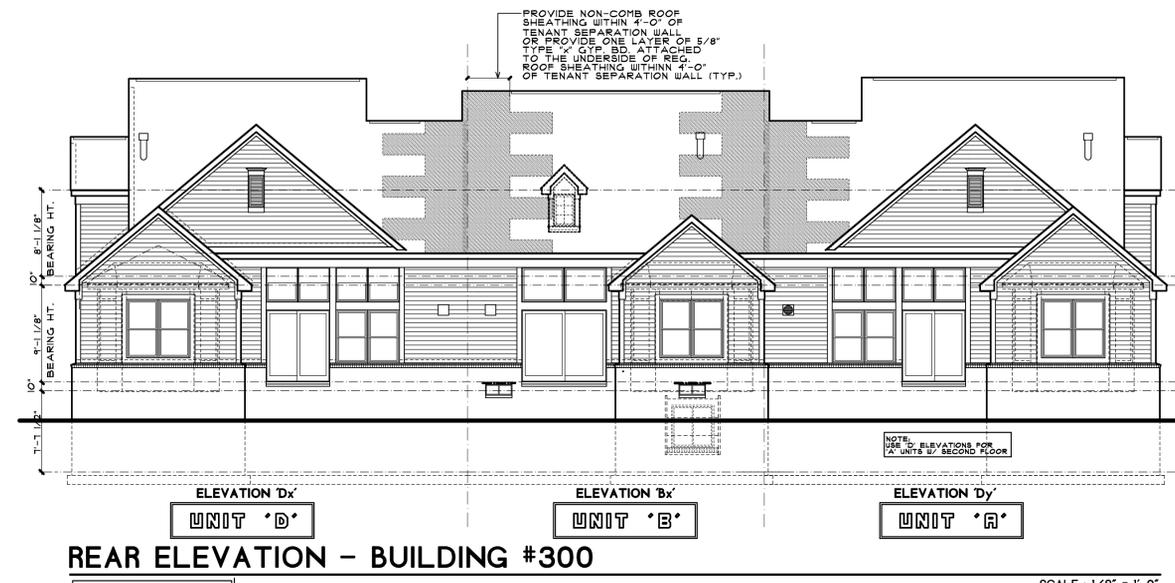
CLIENT/PROJECT
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- PERMITS 02-06-08
- CONSTRUCTION 02-06-08

REVISIONS

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 AM
 JOB NUMBER
 05378
 DATE
 02-06-08
 SHEET NUMBER



ALL INTERIOR PARTITION DIMENSIONS ARE 3/16" UNLESS NOTED OTHERWISE.

BEFORE CONSTRUCTION OBTAIN WINDOW & DOOR ROUGH OPENING SIZES FROM WINDOW/DOOR SUPPLIER.

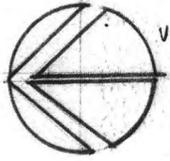
TRUSS DESIGNER/FABRICATOR SEE SHEET T-1 FOR ADDITIONAL NOTES & INFORMATION

SEE SHEET N-1 FOR ADDITIONAL NOTES REGARDING DRAWINGS & CONSTRUCTION.

SEE SHEET N-1 FOR TREATED WOOD & ENGINEERED WOOD PRODUCTS AND ANCHOR ATTACHMENTS

DO NOT SCALE THESE DRAWINGS. USE CALCULATED DIMENSIONS. IF VARIATIONS OCCUR CONTACT ARCHITECT FOR CLARIFICATION.

BUILDING #300
 BRIGGS PARK



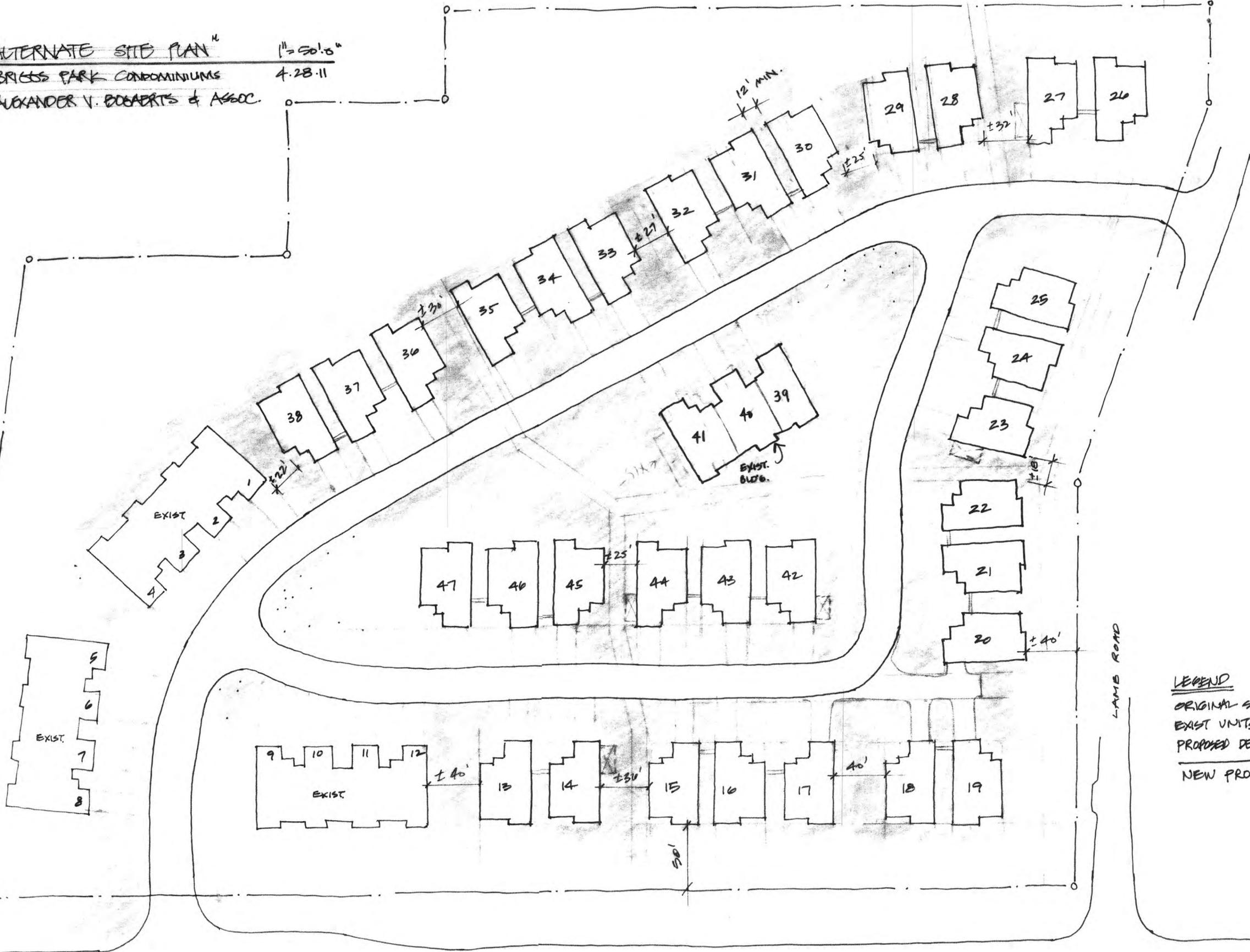
ALTERNATE SITE PLAN^N

BRISSEY PARK CONDOMINIUMS

ALEXANDER V. BOGAERTS & ASSOC.

1" = 50'-0"

4.28.11



LEGEND

ORIGINAL SITE : 54 UNITS

EXIST UNITS : 12

PROPOSED DETACHED : 35

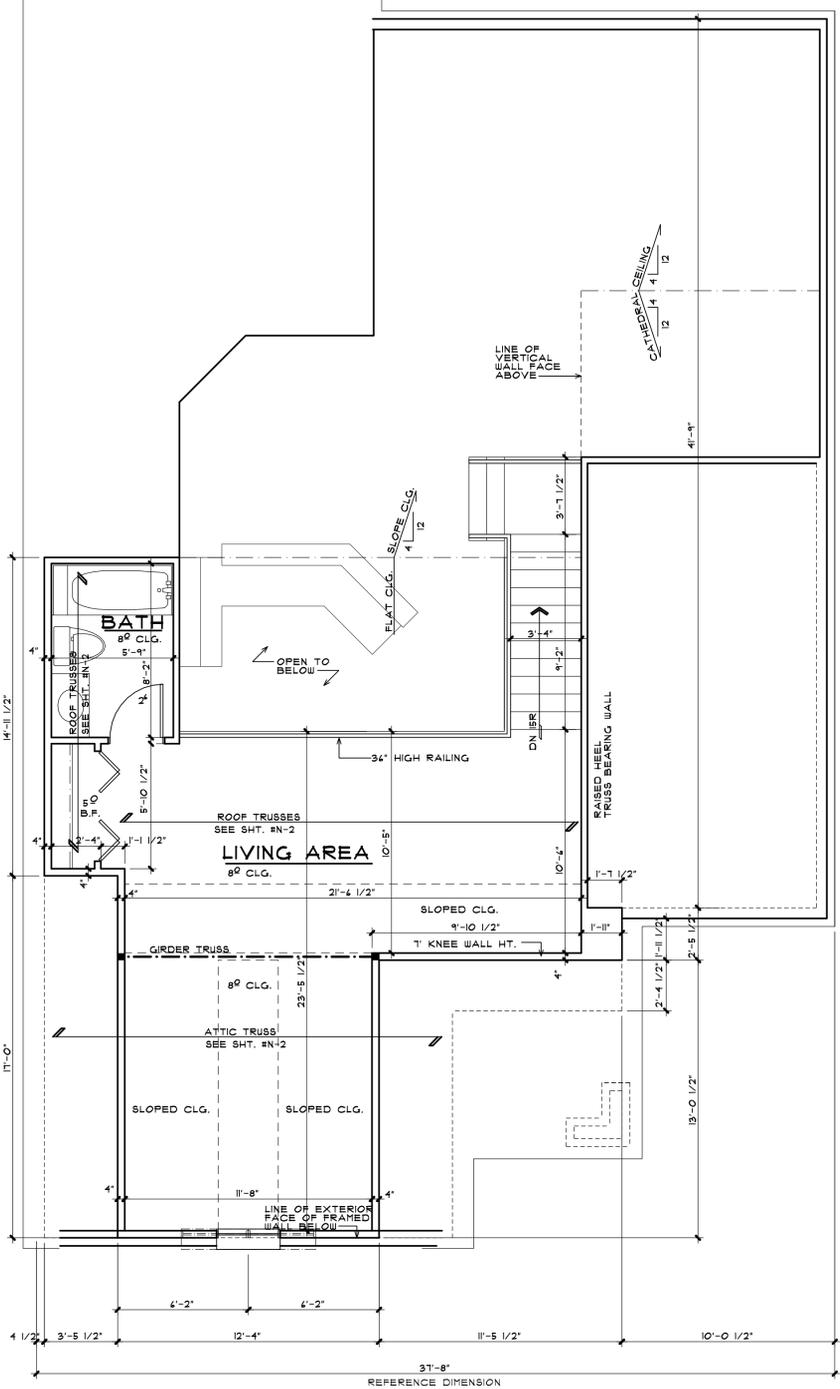
NEW PROPERTY TOTAL 47

ROCHESTER ROAD

LAMB ROAD

STRUCTURAL NOTES:

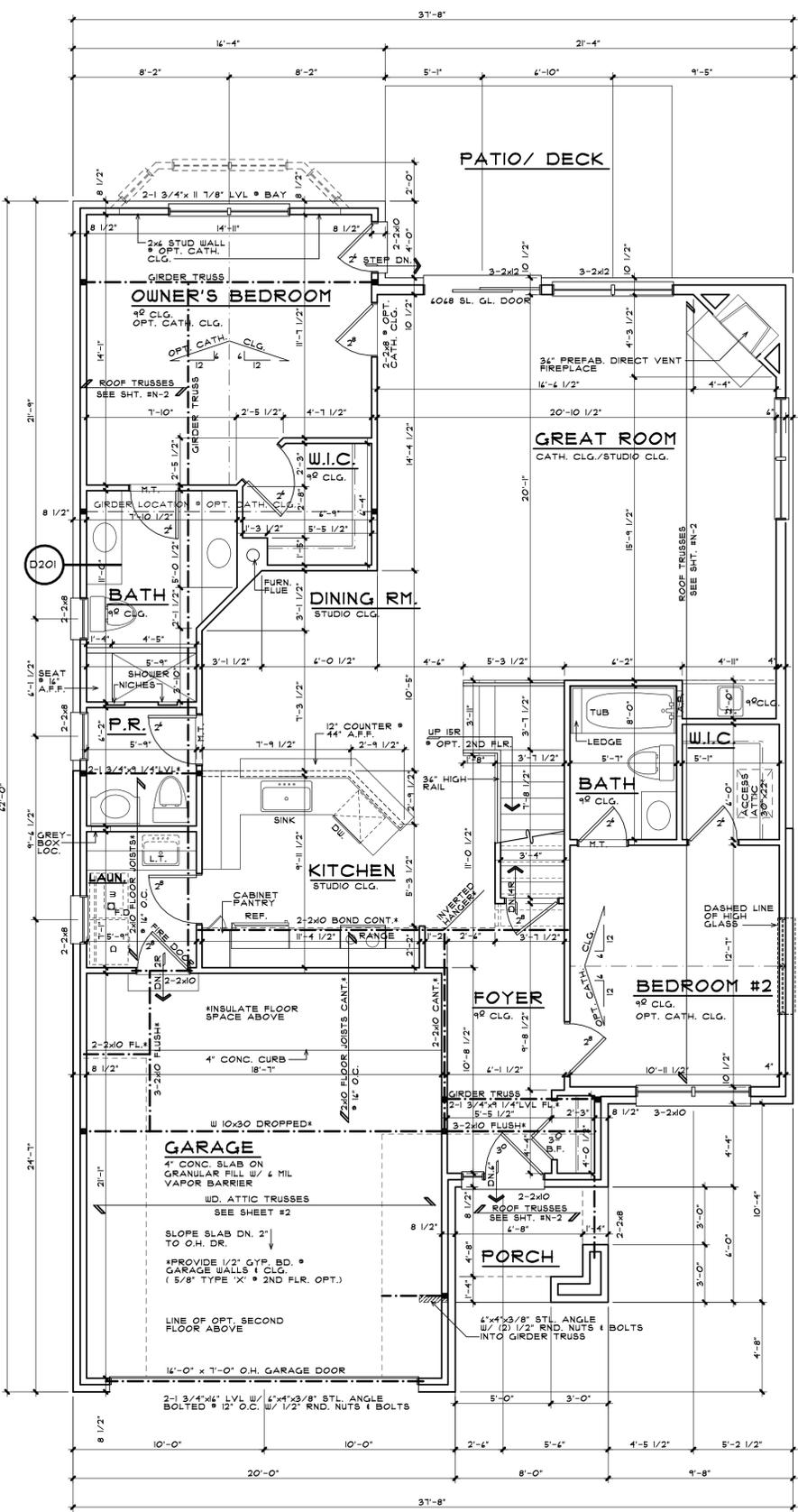
(2) 2x8 HEADERS TO BEAR ON (1) ONE JACK STUD UNLESS NOTED OTHERWISE.
 (2) 2x10 LARGER HEADERS TO BEAR ON (2) TWO JACK STUDS UNLESS NOTED OTHERWISE.
 ALL PRE-ENGINEERED HEADERS TO BEAR ON REQUIRED NUMBER OF STUDS TO MATCH WIDTH OF HEADER MATERIAL OR ON (2) TWO JACK STUDS (USE THE GREATER OF THE TWO CHOICES) AT PARALLEL WALL CONDITION UNLESS NOTED OTHERWISE.
 ALL PRE-ENGINEERED LUMBER HEADERS SHALL BE BUILT-UP FROM THE NUMBER OF HEADERS INDICATED ON DRAWINGS. ALL MEMBERS SHALL BE SECURED WITH NAILS OR BOLTS AS SPECIFIED BY THE MANUFACTURER FOR SIZES INDICATED.
 ALL GIRDER TRUSSES TO BEAR ON (2) TWO STUDS MINIMUM OR AS REQUIRED TO MATCH NUMBER OF TRUSS RAYS UNLESS NOTED OTHERWISE.
 TRUSS FABRICATOR/CONTRACTOR TO PROVIDE ALL HANGERS W/ MODEL NO. CLEARLY STAMPED I LAYOUT DRAWINGS CLEARLY INDICATING LOCATION OF VARIOUS HANGERS REQUIRED.
 CARPENTER CONTRACTOR TO INSTALL NAIL SIZES I NUMBER REQD AS SPECIFIED FOR EACH TYPE OF HANGER.



NOTE: USE 'D' ELEVATIONS FOR 'A' UNITS W/ SECOND FLOOR

SECOND FLOOR PLAN - UNIT 'A'

SCALE: 1/4" = 1'-0"

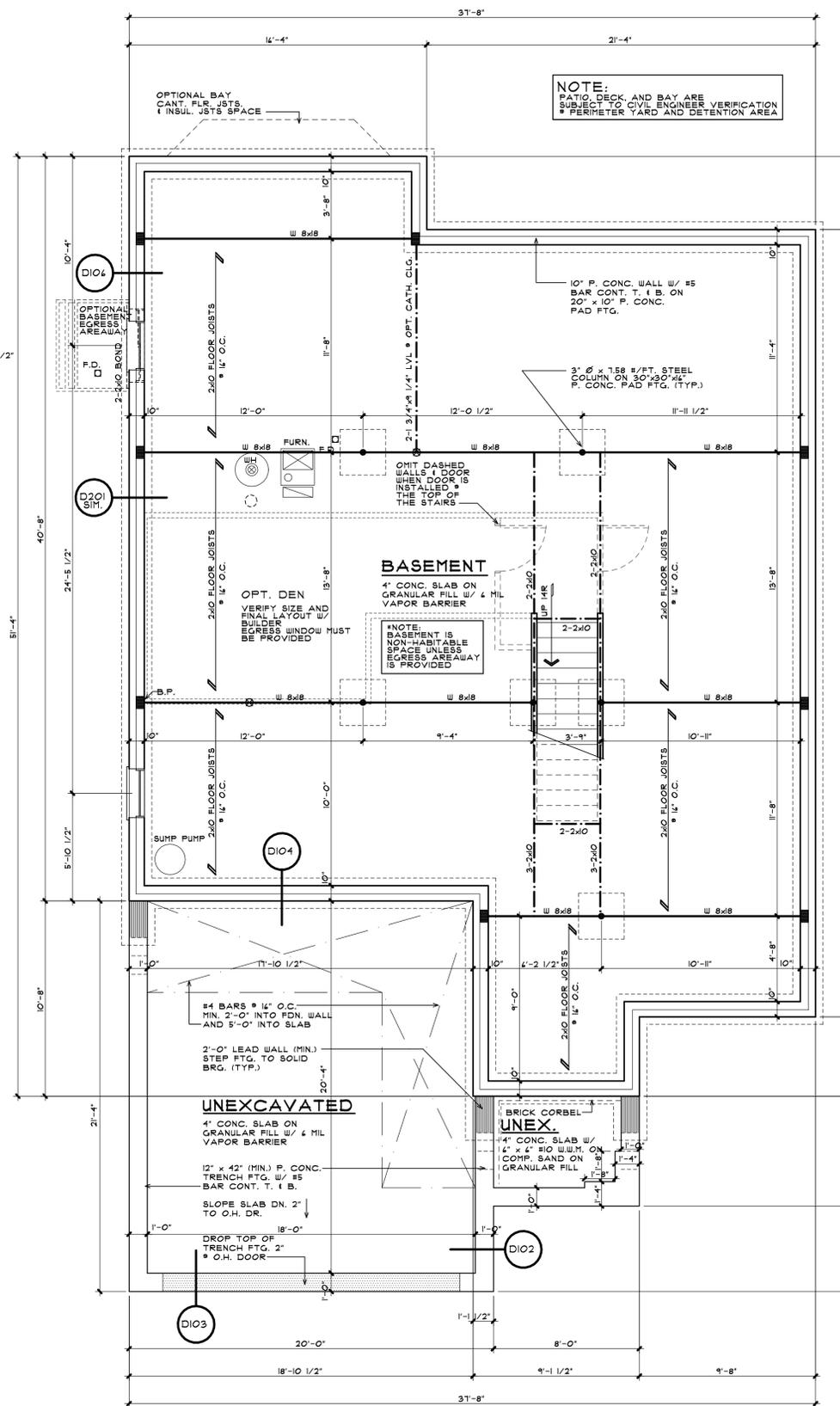


SQUARE FOOTAGE	
1ST FLOOR	1578 SQ FT
2ND FLOOR	420 SQ FT
TOTAL	1998 SQ FT

OPTIONAL BAY IS 18 ADDITIONAL SQUARE FEET

FIRST FLOOR PLAN - UNIT 'A'

SCALE: 1/4" = 1'-0"



FOUNDATION PLAN - UNIT 'A'

SCALE: 1/4" = 1'-0"

ALL INTERIOR PARTITION DIMENSIONS ARE 3/16" UNLESS NOTED OTHERWISE. BEFORE CONSTRUCTION OBTAIN WINDOW & DOOR ROUGH OPENING SIZES FROM WINDOW/DOOR SUPPLIER. TRUSS DESIGNER/FABRICATOR SEE SHEET #2 FOR ADDITIONAL NOTES & INFORMATION. SEE SHEET #3 & 4 FOR ADDITIONAL DETAILS REGARDING DRAWINGS & CONSTRUCTION. SEE SHEET #1 FOR ADDITIONAL NOTES REGARDING DRAWINGS & CONSTRUCTION. DO NOT SCALE THESE DRAWINGS. USE CALCULATED DIMENSIONS. IF VARIATIONS OCCUR CONTACT ARCHITECT FOR CLARIFICATION.

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 2445 Franklin Road
 Bloomfield Hills, MI 48302
 248-334-5000

SHEET TITLE
FOUNDATION PLAN
FIRST FLOOR PLAN
SECOND FLR. PLAN

CLIENT/PROJECT
BRIGGS PARK LLC
 BRIGGS PARK SINGLE FAMILY
 TROY, MI

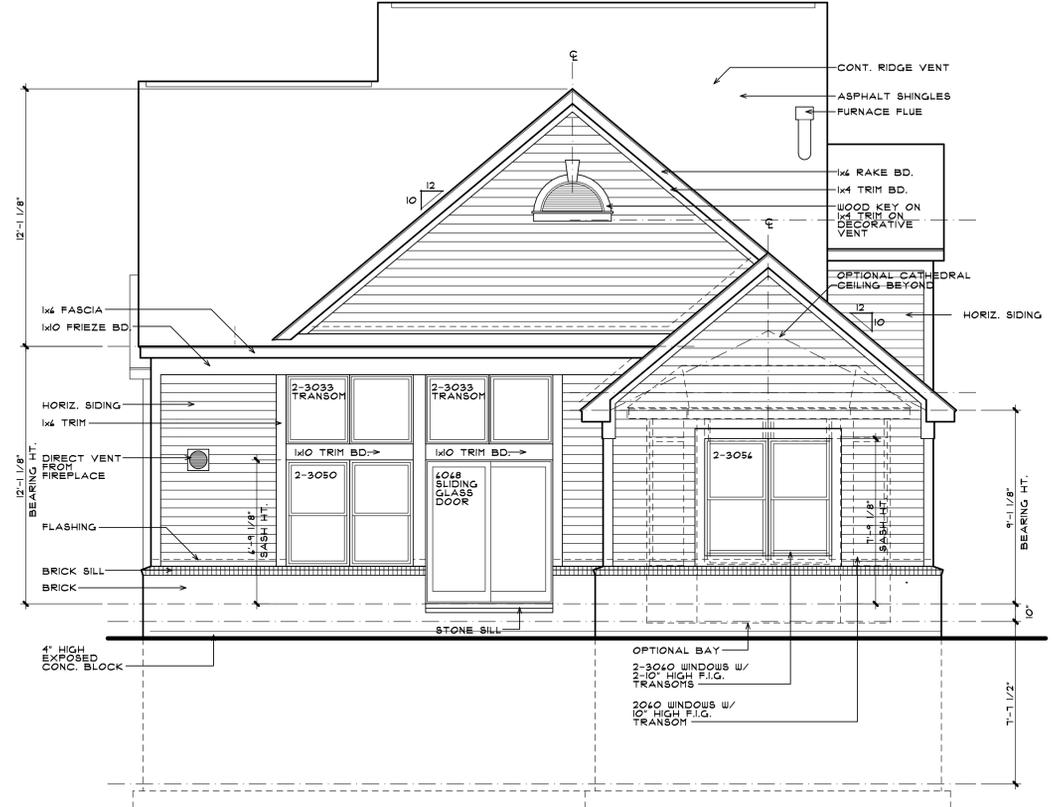
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PRELIMINARY
 BIDS
 PERMITS
 CONSTRUCTION
 REVISIONS

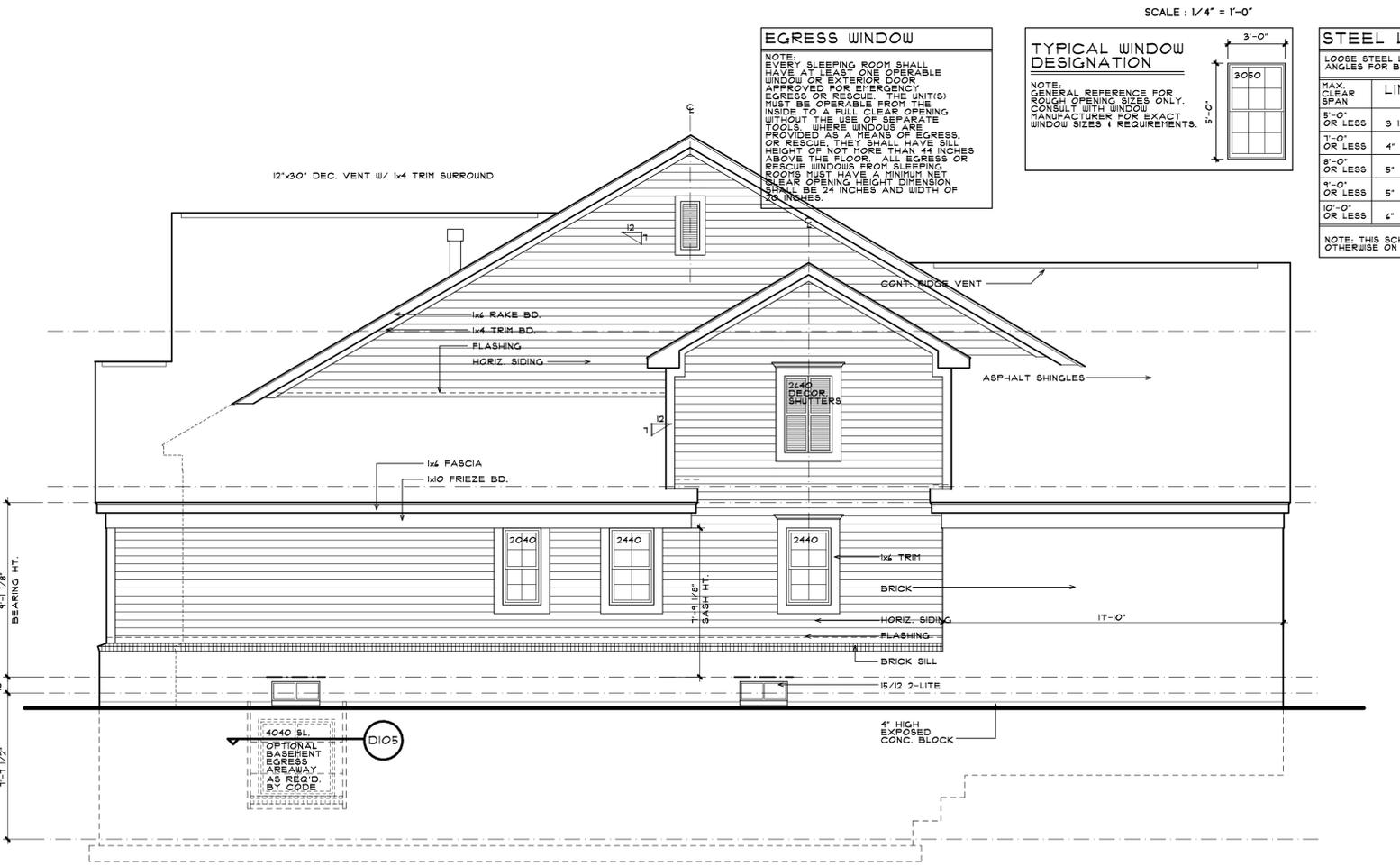
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 SHEET NUMBER



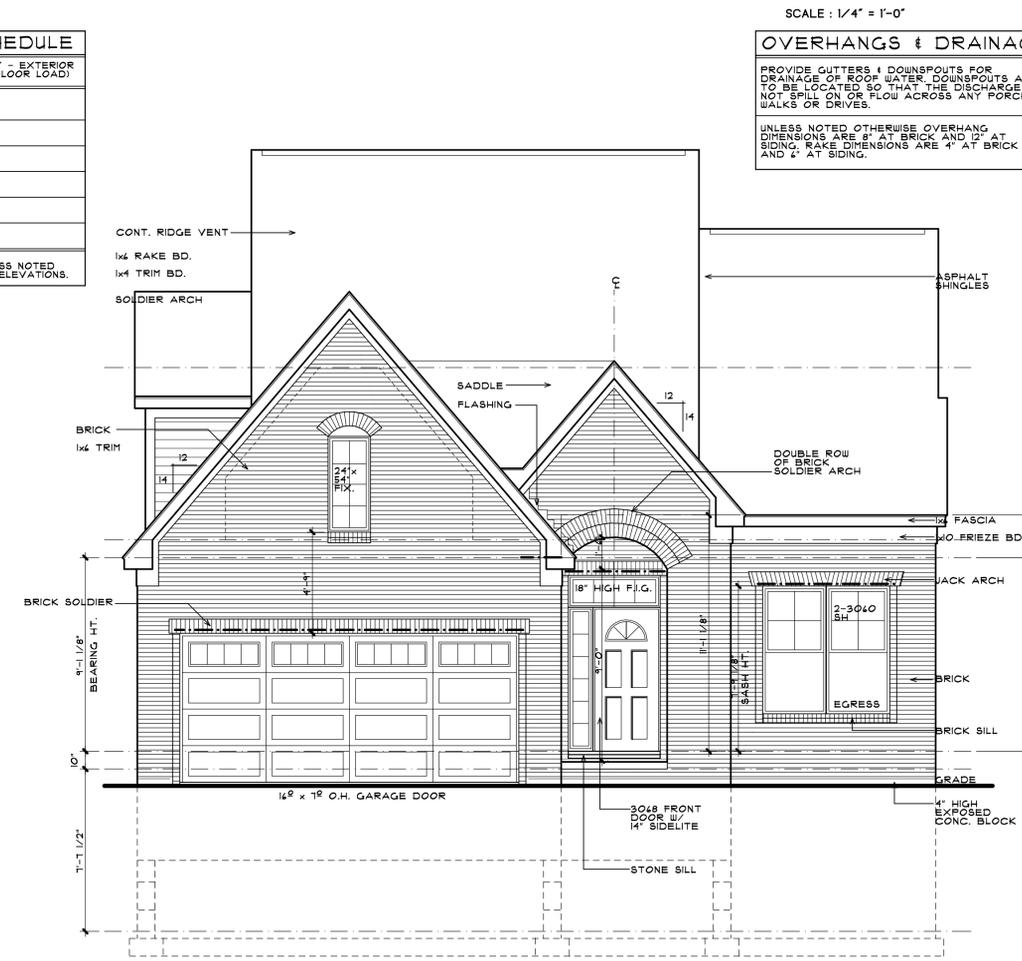
RIGHT SIDE ELEVATION - UNIT 'A'



REAR ELEVATION UNIT 'A'



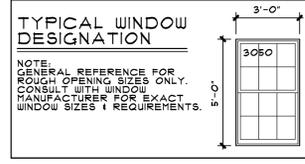
LEFT SIDE ELEVATION UNIT 'A'



FRONT ELEVATION UNIT 'A'

SCALE : 1/4" = 1'-0"

EGRESS WINDOW
 NOTE: EVERY SLEEPING ROOM SHALL HAVE AT LEAST ONE OPERABLE WINDOW OR EXTERIOR DOOR APPROVED FOR EGRESS. THE UNIT(S) MUST BE OPERABLE FROM THE INSIDE TO A FULL CLEAR OPENING WITHOUT THE USE OF SEPARATE TOOLS. WHERE WINDOWS ARE PROVIDED AS A MEANS OF EGRESS, OR RESCUE, THEY SHALL HAVE SILL HEIGHT OF NOT MORE THAN 44 INCHES ABOVE THE FLOOR. ALL EGRESS OR RESCUE WINDOWS FROM SLEEPING ROOMS MUST HAVE A MINIMUM NET CLEAR OPENING HEIGHT DIMENSION OF 20 INCHES AND WIDTH OF 20 INCHES.



STEEL LINTEL SCHEDULE
 LOOSE STEEL LINTELS FOR MASONRY - EXTERIOR ANGLES FOR BRICK OR STONE (NO FLOOR LOAD)

MAX. CLEAR SPAN	LINTEL SIZE
5'-0" OR LESS	3 1/2" x 3 1/2" x 5/16"
7'-0" OR LESS	4" x 3 1/2" x 5/16"
8'-0" OR LESS	5" x 3 1/2" x 5/16"
9'-0" OR LESS	5" x 3 1/2" x 3/8"
10'-0" OR LESS	4" x 3 1/2" x 3/8"

NOTE: THIS SCHEDULE APPLIES UNLESS NOTED OTHERWISE ON THE PLANS AND/OR ELEVATIONS.

OVERHANGS & DRAINAGE
 PROVIDE GUTTERS & DOWNSPOUTS FOR DRAINAGE OF ROOF WATER. DOWNSPOUTS ARE TO BE LOCATED SO THAT THE DISCHARGE WILL NOT SPILL ON OR FLOW ACROSS ANY PORCHES, WALKS OR DRIVES.
 UNLESS NOTED OTHERWISE OVERHANG DIMENSIONS ARE 8" AT BRICK AND 12" AT SIDING. RAKE DIMENSIONS ARE 4" AT BRICK AND 4" AT SIDING.

ALL INTERIOR PARTITION DIMENSIONS ARE 3/16" UNLESS NOTED OTHERWISE. BEFORE CONSTRUCTION OBTAIN WINDOW & DOOR ROUGH OPENING SIZES FROM WINDOW/DOOR SUPPLIER. TRUSS DESIGNER/FABRICATOR SEE SHEET #2 FOR ADDITIONAL NOTES & INFORMATION. SEE SHEET #3 & 4 FOR ADDITIONAL DETAILS REGARDING DRAWINGS & CONSTRUCTION. SEE SHEET #1 FOR ADDITIONAL NOTES REGARDING DRAWINGS & CONSTRUCTION. DO NOT SCALE THESE DRAWINGS. USE CALCULATED DIMENSIONS. IF VARIATIONS OCCUR CONTACT ARCHITECT FOR CLARIFICATION.

Alexander V. Bogaerts + Associates, P.C. • Architecture • Planning • Interior Design
 2445 Franklin Road
 Bloomfield Hills, MI 48302
 248-334-5000

CLIENT/PROJECT: BRIGGS PARK LLC
 BRIGGS PARK SINGLE FAMILY
 TROY, MI

SHEET TITLE: ELEVATIONS
 PRELIMINARY

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PRELIMINARY

DATE: _____

SHEET NUMBER: _____

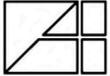


Streetscape Elevaton

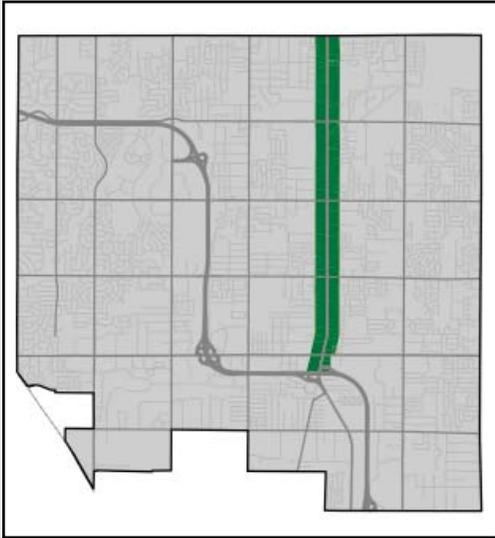
Briggs Park

Troy, MI

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Rochester Road: Green Corridor



- *Regional model for a green corridor*
- *A strong focus on access management*
- *Heightened emphasis on strong stormwater management techniques*
- *Retail catering to regional traffic*
- *Innovative site design techniques applied through PUD use to allow for redevelopment for shallow lots*

Rochester Road carries high volumes of traffic causing backups at intersections. The abutting development pattern from Big Beaver Road north to Long Lake Road is a continuous row of highway-oriented commercial uses. North of Long Lake Road, the land use pattern evolves, becoming a mix of commercial and office near the intersections and older single-family homes and multiple-family complexes in between.

If Rochester Road is to have a defined role and pleasing character in the City, it must undergo a significant transformation over time. Ultimately, the Rochester Road Corridor will become a regional showcase for effective stormwater management and enhancement of the natural environment, while encouraging a combination of high-quality land uses. Effective landscaping focused on

native plantings, and improved land use and access management along Rochester will create a green corridor that provides a high level of service for motorists, and which provides an effective natural buffer between high traffic volumes and people visiting adjacent properties. The creation of this green corridor would occur primarily in the right-of-way along road frontages and in the median of a future boulevard.

While the emphasis on innovative stormwater management is specifically called on for the Rochester Road Corridor, new low-impact techniques are to be encouraged elsewhere throughout the City of Troy. As noted in Chapter 7, innovative stormwater management is a priority for the community. Rochester Road will play an important role in this City-wide initiative by proving a regional showcase for such techniques.

New construction along the corridor may include detention and retention basins that work together from site-to-site with other features to create a continuous, linear landscape feature. By connecting properties, the basins create visual relief from traffic. **Low impact development methods will be used throughout the corridor to filter stormwater runoff.** Rochester Road will also be characterized by effective new signage, high-quality lighting, and effective, complementary site and architectural design.

Uses along Rochester Road will include a variety of mixed uses, established in a “pulsing” pattern where the most intense mixed-use or exclusively non-residential development will occur near the Neighborhood Nodes situated along its main intersections. Lower-impact uses, such as small scale retail or condominiums should be encouraged along the corridor frontage between these nodes.

